

Walk & Roll Anne Arundel!

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Information contained in this document is for planning purposes and should not be used for final design of any project. All results, recommendations, concept drawings, cost opinions, and commentary contained herein are based on limited data and information and on existing conditions that are subject to change. Further analysis and engineering design are necessary prior to implementing any of the recommendations contained herein.

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Executive Summary

Figure 1: *Walk & Roll Anne Arundel!* aims to provide comfortable places to travel for people of all ages and abilities.

Anne Arundel County, “The Best Place – For All!”

Anne Arundel County is known as the heart of Maryland. It is home to Annapolis, the state capital. Residents and visitors alike enjoy its plentiful shoreline, idyllic rural areas, and lively city and town centers. Throughout these various sceneries of Anne Arundel County, a safe, accessible, and enjoyable environment for walking and bicycling is key to a high quality of life.

Today, many parts of the County can be challenging to navigate on foot or by bicycle, as the network of sidewalks and bicycle infrastructure is incomplete and, in many places, not comfortable or appealing to people of all ages and abilities.

Walk & Roll Anne Arundel!, the County’s updated pedestrian and bicycle master plan, seeks to improve these transportation conditions. Anne Arundel County leaders, residents, and stakeholders can use *Walk & Roll Anne Arundel!* to advance active transportation infrastructure. The recommendations in this plan will enable more people to walk and bicycle safely, comfortably, and efficiently.

Goals of this Plan

This plan builds on recommendations established in *Move Anne Arundel!*, the County’s first Transportation Functional Master Plan.

Specifically, the goals and strategies in *Walk & Roll Anne Arundel!* are to:

- Prioritize a safe and comfortable – low-stress – walking and rolling network between essential destinations throughout the County.
- Ensure that vulnerable populations have access to active transportation infrastructure.
- Recommend infrastructure proven to reduce crashes, especially at intersections and along corridors with high rates of crashes resulting in death or serious injury.
- Identify policy recommendations to promote the construction of safe, accessible, and direct walking and rolling infrastructure.

Walk & Roll Anne Arundel! also works to address transportation challenges detailed in *Plan2040*, the County’s General Development Plan. This plan is organized by the nine Regional Planning Areas established by *Plan2040*.

Relationship to the City of Annapolis

This plan considers the relationship and connectivity of the proposed network to the network to the City of Annapolis. However, *Walk & Roll Anne Arundel!* does not evaluate or make recommendations within the City. The City has its own Bicycle Master Plan.

Shared-use paths



Figure 2: Pedestrians and bicyclists both use a shared-use path. Source: Toole Design Group.

Bicycle lanes



Figure 3: A bicyclist using a standard bicycle lane. Source: Toole Design Group.

Separated bicycle lanes

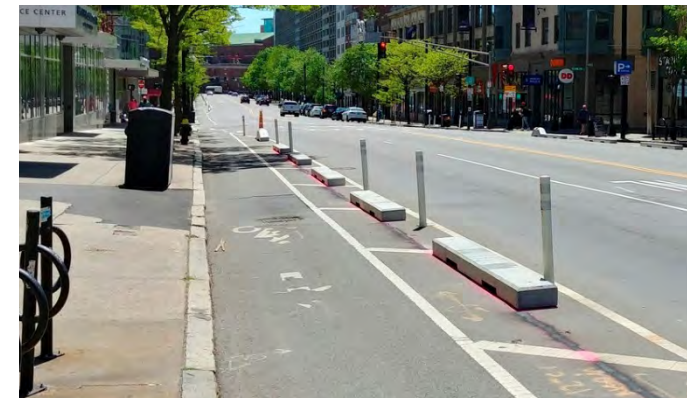


Figure 4: A separated bicycle lane located in Boston, MA. Source: Toole Design Group.

What is a Low-stress Network?

Anne Arundel County residents of all ages and abilities should feel comfortable traveling in a low-stress network that meets their needs in all phases of life: from getting to school, commuting to work, and participating in recreational activities.

In a low-stress network:

- **Sidewalks** are continuous and meet accessibility requirements. They are also characterized by wider sidewalks that are buffered from the roadway by either grass strips, street trees, parked cars, or other barriers.
- **Bicycle facilities** provide separation from fast-moving motor vehicle traffic.
- **Trails** are designed to accommodate the anticipated number of trail users.
- At **intersections and crossings**, people have a clear, well-marked place to cross without risk of a crash.
- **Multimodal transit opportunities** will seamlessly allow travelers to complete their trips using a combination of modes.

To the left are some of the low-stress facilities recommended in the plan.

Developing the *Walk & Roll Anne Arundel!* Walking and Bicycling Network

The proposed active transportation network for Anne Arundel County was developed through a combination of field work, technical analysis, and public engagement. After an in-depth public engagement process across all regions of the County, the project team incorporated the needs expressed by County residents into the development of plan recommendations. Through this process, the team was able to get a full, holistic view of the County's infrastructure.

The recommended network incorporates

- public comments,
- fieldwork observations,
- findings from a crash risk analysis,
- location and density of important destinations, and
- recommendations from previous plans.

Priority Improvement Areas

To create a connected network of low-stress walking and bicycling infrastructure, the project team proposed specific walking and bicycling infrastructure on 15 "Priority Improvement Areas" or PIAs. The PIAs were developed through a Level of Traffic Stress analysis (see Map 1) for each street in the County, as well as on the ground observations. Figure 5 displays the 15 PIAs across the County.

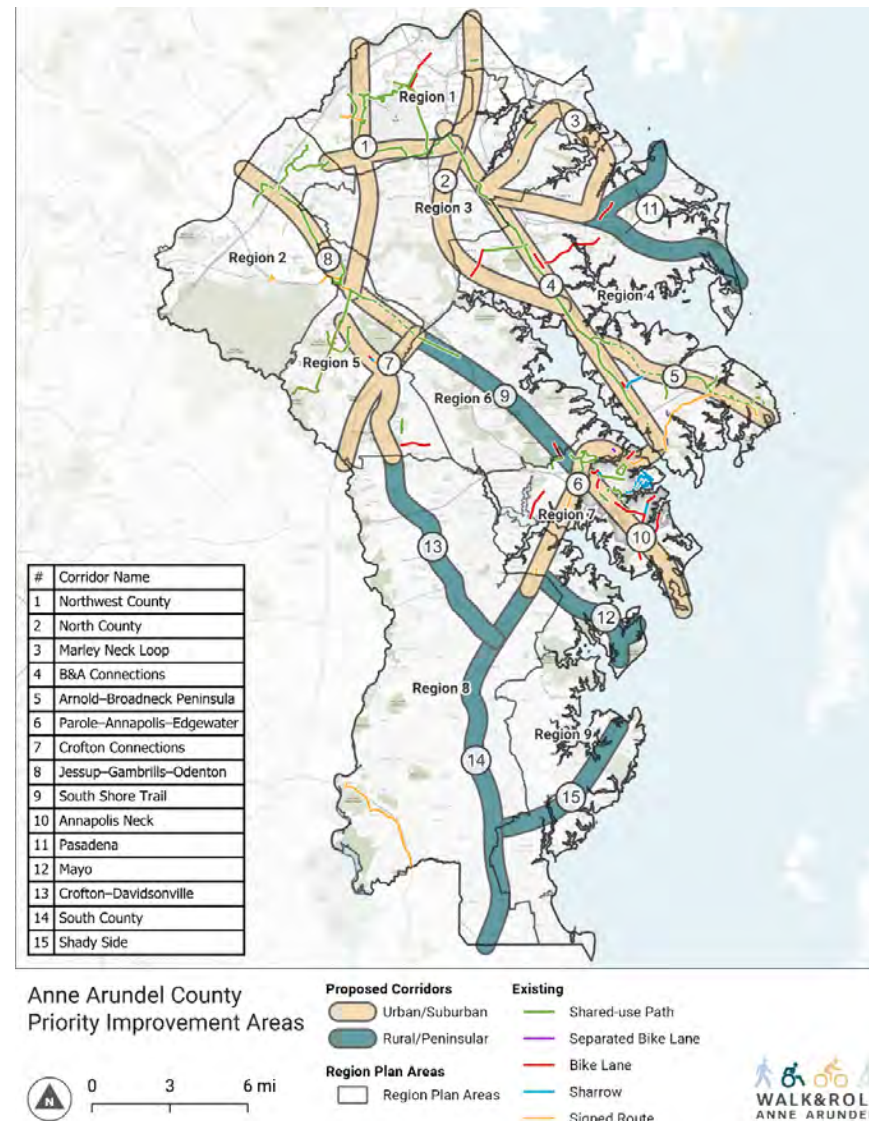
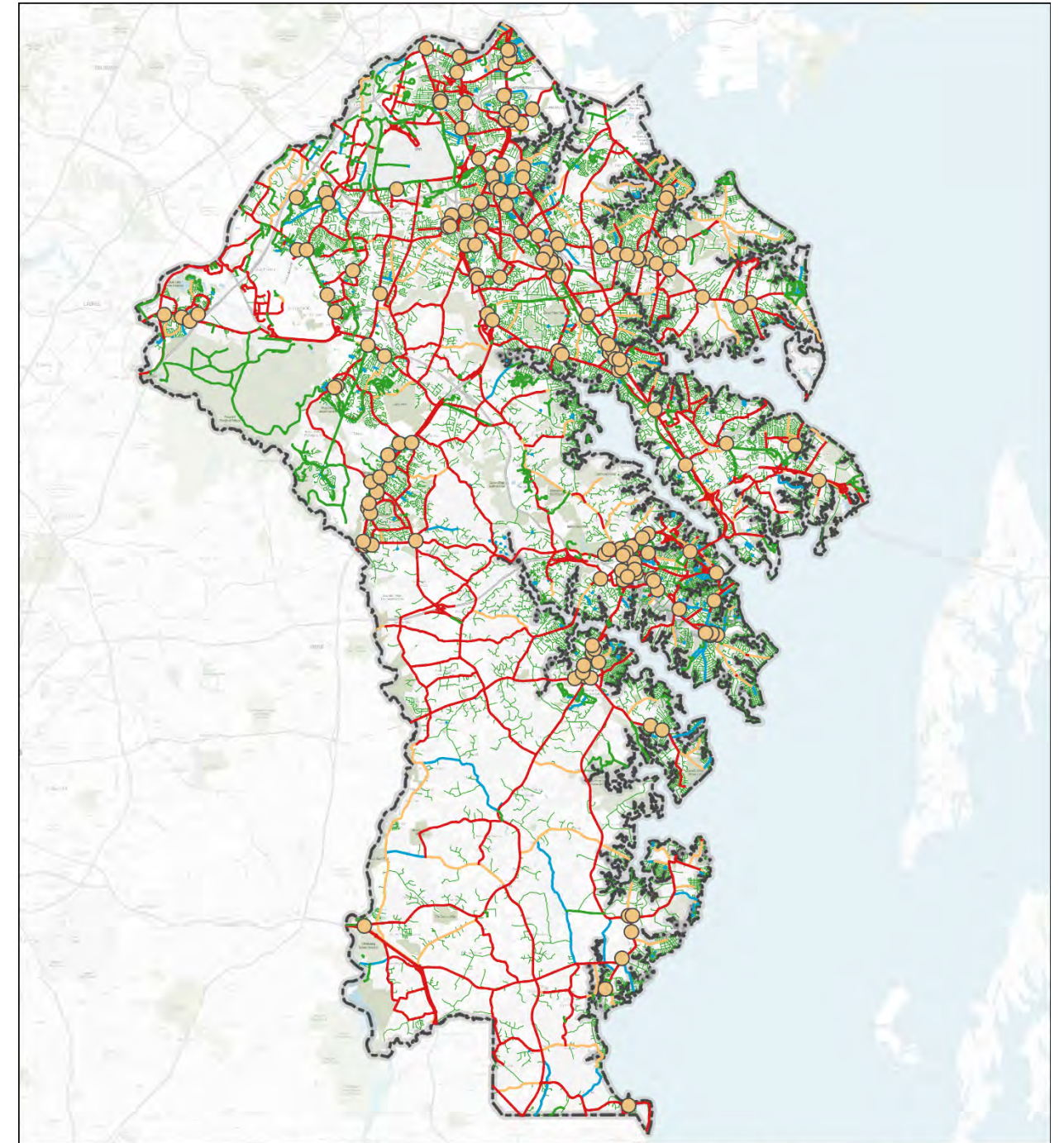
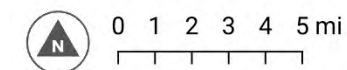


Figure 5: Priority Improvement Areas Studies for Walking and Bicycling Infrastructure Recommendations in *Walk & Roll Anne Arundel!*



Anne Arundel County

LTS and Shopping Centers



Level of Traffic Stress (LTS)

- LTS 1
- LTS 2
- LTS 3
- LTS 4

Shopping Centers
● Shopping Center/Mall



Map 1: Shopping centers and malls across Anne Arundel County with the corresponding Level of Traffic Stress (LTS) in the surrounding road network.

Network Recommendations

Using observations from fieldwork and a desktop review, the Project Team evaluated the condition of every main corridor within each PIA, as well as streets that connect to essential destinations such as major jobs centers, schools, transit, parks, and medical centers. Each corridor was given one of three preliminary recommendations:

- Prioritize a new low-stress facility on the route,
- Prioritize a low-stress facility on a parallel route, if the main corridor appeared to have significant constraints, such as buildings located close to the roadway, or
- Upgrade an existing facility on the route to make it a low-stress facility or one that is sufficient for the needs of all travelers, including people with disabilities.

The project team identified recommended bicycle facilities or shared-use paths for each corridor.

Map 2 depicts the overall network of walking and bicycling infrastructure recommendations. The table below summarizes the mileage of existing, programmed, and proposed facilities.

	Existing	Programmed	Proposed	Total
Bike Lanes	26.9	4.0	130.4	161.3
Protected Bike Lanes	0.5	-	22.4	22.9
Shared-use Paths	55.2	10.9	248.5	314.6
Low Stress Connections	-	-	3.6	3.6

Town Centers

This plan provides more detailed network recommendations for Anne Arundel County's Town Centers – **Odenton**, **Glen Burnie**, and **Parole**. These mixed-use developments feature a high concentration of residential areas as well as areas of employment, which makes it especially important to ensure that there is a safe active transportation network in these areas.

Walk and Roll Anne Arundel! provides recommendations to prioritize sidewalks, shared-use paths, separated bicycle lanes, standard bicycle lanes, and either improve existing crossings or add new infrastructure to make crossing safer within the Town Centers. These recommendations are shown in Maps 3, 4, and 5.

Pedestrian Safety Infrastructure

It is imperative to provide safe, continuous paths for residents and visitors of Anne Arundel County. The project team identified and recommended the implementation of infrastructure proven to reduce pedestrian crashes at signalized intersections and midblock locations.

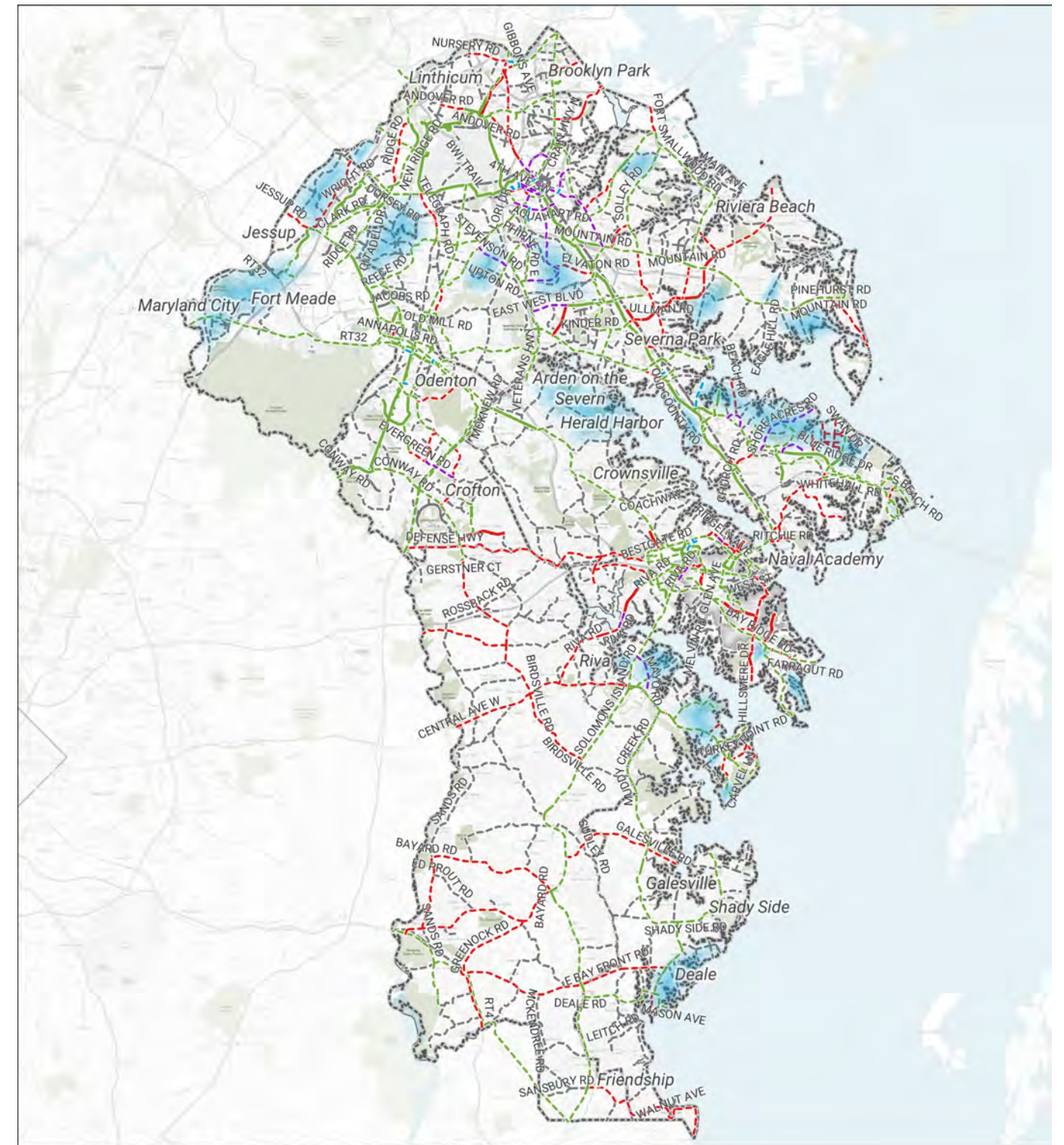
Following guidance from agencies like the Federal Highway Administration, the project team recommended the following elements of pedestrian safety infrastructure due to their proven ability to significantly reduce pedestrian crashes, shown in the table below.

Recommended Pedestrian Safety Infrastructure
Shared-use path
Pedestrian Hybrid Beacons (PHB)
Rectangular Rapid Flashing Beacons (RRFB)
Pedestrian refuge islands
Leading Pedestrian Intervals (LPI)
Road diets/lane reconfigurations
Raised crosswalks
High-visibility crosswalk enhancements
Low stress design elements for sidewalks

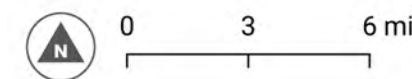
Performance Measures

Chapter 4 outlines specific performance measures to ensure that the County meets all of the *Walk & Roll Anne Arundel!* goals. Each indicator is measured in an appropriate time frame, with some being year by year, and others projected over the next 25 years. For long-range projects, there are also interim metrics for 10- to 15-year targets. The categories of performance measures include:

- Connectivity and Access
- Equity
- Safety
- Strategic Growth and Mode Shift
- Investment



Anne Arundel County Walk & Roll Network

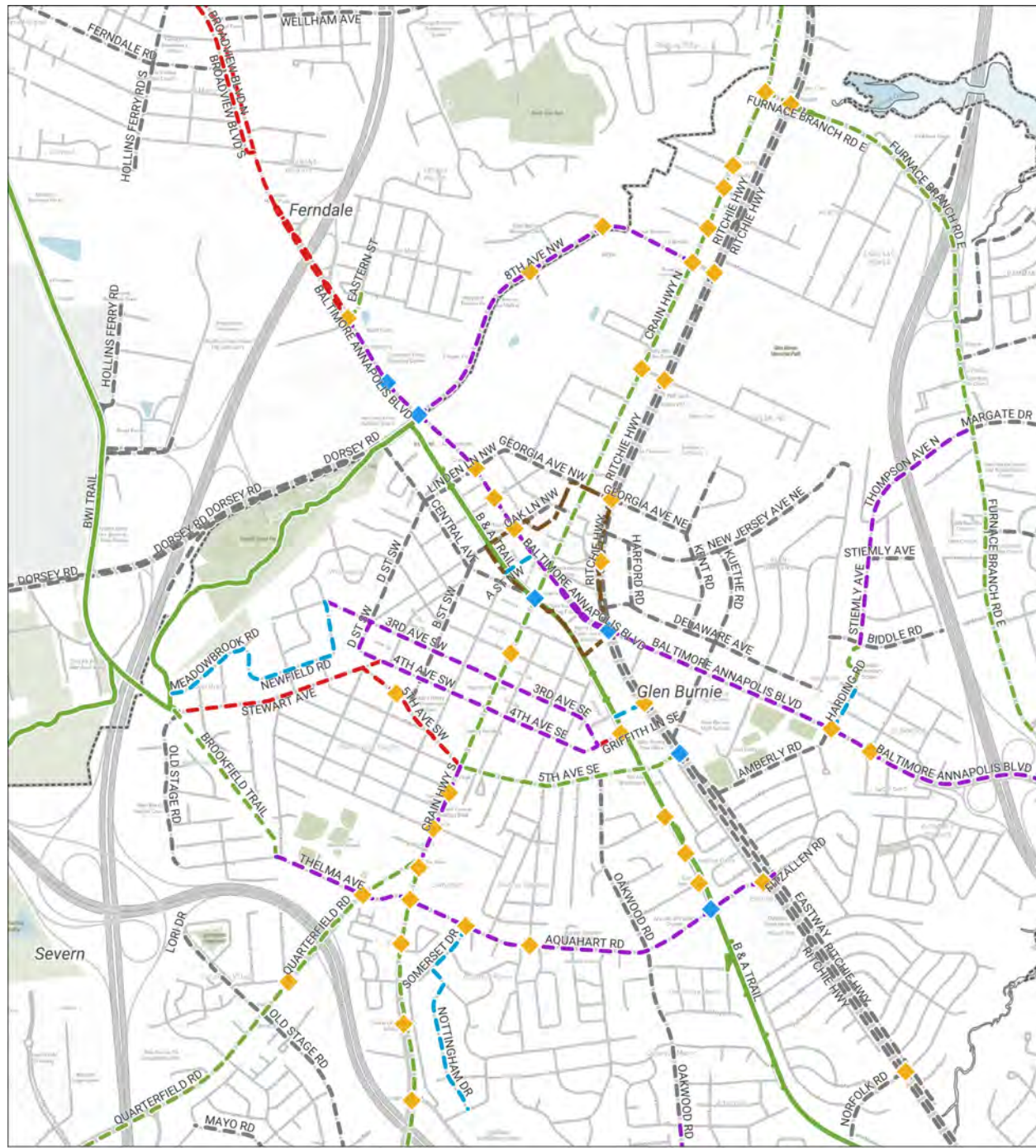


Facility Types

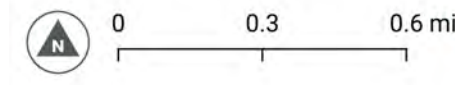
- | | |
|-----------------------|-----------------------|
| Proposed | Area for Future Study |
| Shared-use Path | Existing |
| Separated Bike Lane | Shared-use Path |
| Bike Lane | Separated Bike Lane |
| Low Stress Connection | Bike Lane |
| Secondary Network | |



Map 2: Walk & Roll Anne Arundel! Countywide Walking and Bicycling Infrastructure Recommendations

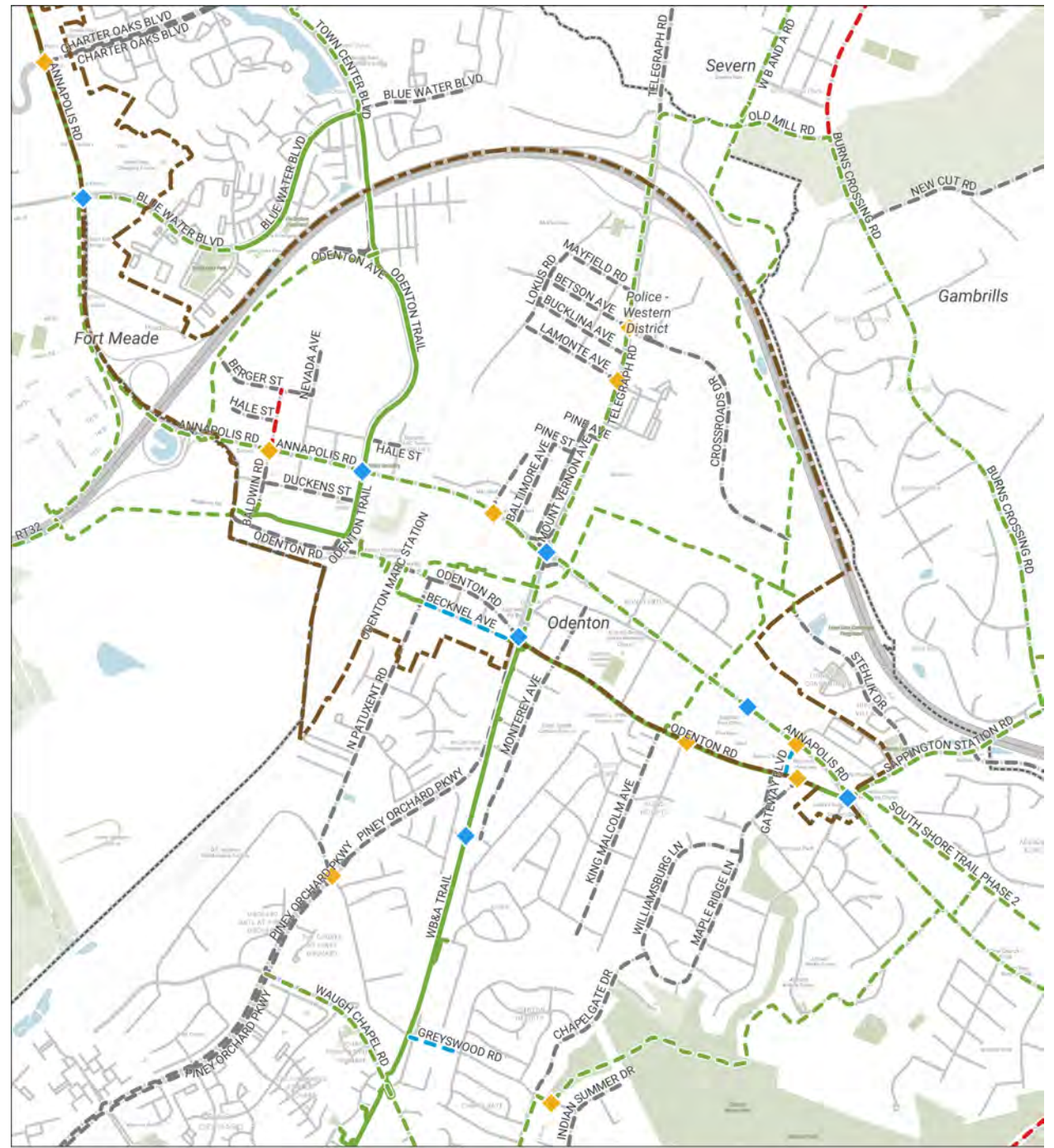


**Glen Burnie Town Center
Proposed Bike Network**

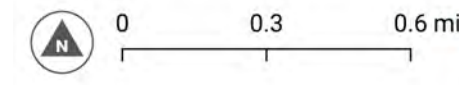


- | Facility Types | | Proposed Crossing Improvements | |
|---|---|---|--|
| --- Shared-use Path | --- Shared-use Path | ◆ Upgrade Pedestrian and Bike Crossing Elements | ◆ New Pedestrian and Bike Crossing |
| --- Separated Bike Lane | --- Bike Lane | Town Center Boundary | |
| --- Bike Lane | Town Center Boundary | | |
| --- Low Stress Connection | | | |
| --- Secondary Network | | | |

Map 3: The proposed bike network for Glen Burnie Town Center.

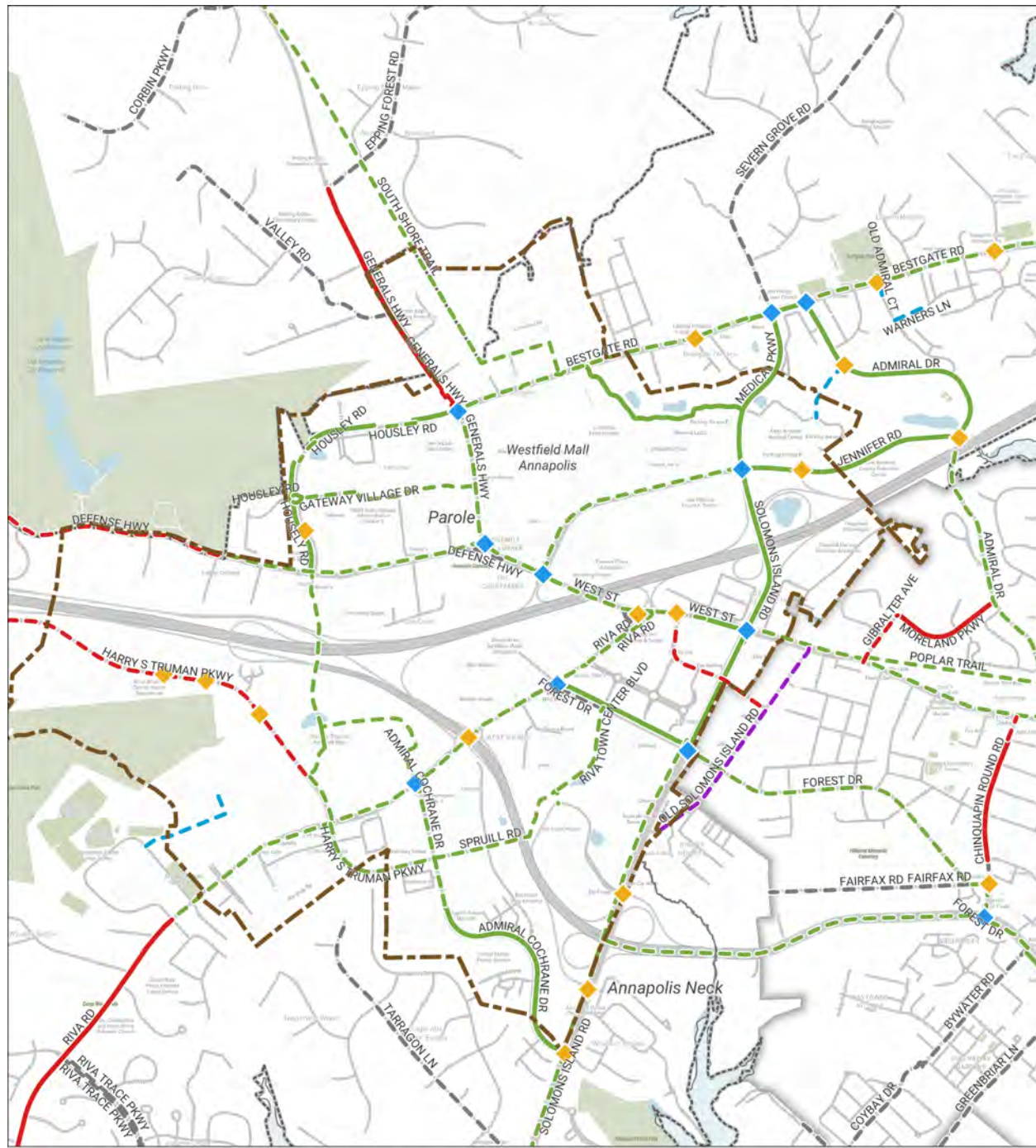


**Odenton Town Center
Proposed Bike Network**

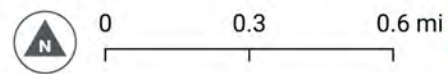


- | Facility Types | | Proposed Crossing Improvements | |
|---|---|---|--|
| --- Shared-use Path | --- Shared-use Path | ◆ Upgrade Pedestrian and Bike Crossing Elements | ◆ New Pedestrian and Bike Crossing |
| --- Separated Bike Lane | --- Bike Lane | Town Center Boundary | |
| --- Bike Lane | Town Center Boundary | | |
| --- Low Stress Connection | | | |
| --- Secondary Network | | | |

Map 4: The proposed bike network for Odenton Town Center.



Parole Town Center
Proposed Bike Network



- | Facility Types | | Existing | | Proposed Crossing Improvements | |
|----------------|-----------------------|----------|----------------------|--------------------------------|---|
| | Shared-use Path | | Shared-use Path | | Upgrade Pedestrian and Bike Crossing Elements |
| | Separated Bike Lane | | Bike Lane | | New Pedestrian and Bike Crossing |
| | Bike Lane | | Town Center Boundary | | |
| | Low Stress Connection | | | | |
| | Secondary Network | | | | |

Map 5: The proposed bike network for Parole Town Center.

Design Guidance

The details of street design are key to ensuring that people walking and rolling are safe, comfortable, and able to get to the places they need to go. Chapter 5 includes recommendations for the design of Anne Arundel County's streets based on a typology of the adjacent land use and the street's functional classification, from rural highways to suburban arterials to commercial corridors. Cross sections help to illustrate these concepts.

Prioritizing Improvements

Strategic planning to carefully fund and phase projects is crucial to successful implementation of the County's walking and rolling infrastructure. To prioritize limited available funding for transportation projects that will result in the most benefit to the County, each proposed project is scored according to its ability to support *Walk & Roll Anne Arundel!*'s established goals.

The County prioritized walking and rolling projects in census tracts with higher-than-average rates of vulnerable people, including low income, older residents, people of color, and households without a car. In addition, the County prioritized walking and rolling infrastructure along roadways with high rates of crashes, or high risk of crashes, involving pedestrians and bicyclists.

This plan recommends infrastructure proven to reduce pedestrian crashes at intersections and along corridors with high pedestrian crash rates, especially crashes resulting in death or serious injury. It also identifies policy recommendations to promote the construction of safe, accessible, and direct walking and rolling infrastructure.

Policy Recommendations

In order for the County to expand its walking and rolling network and provide state-of-the-practice multimodal infrastructure that keeps people safe and comfortable, the County should enact the following policies.

Policy Recommendations

The County should consider a significant increase in Capital Improvement Projects (CIP) funding for pedestrian and bicycle infrastructure.

The County should strengthen its coordination with the State to enhance its competitiveness for State and Federal funding.

The County should strengthen its planning, funding, and construction of Complete Streets projects. The County should codify its "Complete Streets Policy" to establish exact procedures and requirements that would consistently yield projects with walking and bicycling infrastructure.

Both the "Anne Arundel County Design Manual" and "Complete Streets Guidelines" should be updated to include specific preferred standards for pedestrian and bicycle projects enumerated in this Plan.

The County should strengthen partnerships with and requirements of private developers to provide low stress walking and bicycling facilities as part of their private developments.

The County should provide incentives to encourage people to commute via walking and bicycling, and encourage businesses and other entities to do the same.

The County should commit to achieving the League of American Bicyclist's recognition of Silver, then Gold, in the Bicycle Friendly Community program.

Anne Arundel County Public Schools should focus efforts on improving safety for students to walk and bicycle to school and encouraging students to do so.

The County should improve facilities to assist people with disabilities, so that they may easily, safely, and comfortably reach their destinations via active transportation infrastructure.



Chapter 1: Introduction

Figure 6: Safe and enjoyable bicycling is a key element of *Walk & Roll Anne Arundell*

Anne Arundel County, “The Best Place – For All!”

Anne Arundel County is the most central county of Maryland, with over a half million people calling it home. The County is centrally located between Baltimore and Washington, DC, and it is home to Annapolis, the State capital. With bountiful shoreline, scenic rural areas, and vibrant city and town centers, residents of Anne Arundel County enjoy urban amenities, beautiful scenic areas, and a high quality of life.

Key to each of these different experiences is a safe, accessible, and enjoyable environment for walking and bicycling. Today, many parts of Anne Arundel County can be challenging to navigate on foot or by bicycle, due to an incomplete and, in many places, uncomfortable network of sidewalks and bicycle infrastructure. This is especially difficult in lower income areas which have lower car ownership and limited transit options.

People often must walk or bicycle next to, or in many cases with, fast moving car and truck traffic, which is uncomfortable and dangerous.

The County first adopted a Pedestrian and Bicycle Master plan in 2003, which was updated in 2013. *Walk & Roll Anne Arundel!* serves as the next update to the plan and unlike the last iteration, will encompass the entire County. *Walk & Roll Anne Arundel!* provides a framework for improving multimodal transportation conditions Countywide, and a prioritized list of proposed projects, both of which County leaders, residents, and stakeholders can use to advance safe and accessible active transportation infrastructure that enables people to move around the County and get where they need to go.

What is active transportation?

Active transportation, also known as non-motorized transportation, is defined as a self-propelled, human-powered mode of transportation, including walking, biking, small-wheeled transport (skates, skateboards, push scooters, and hand carts), electric-assist devices such as ebikes and e-scooters, and wheelchair travel.

Strong mobility options provide access to opportunities that are critical for individual, economic, social, and community success.

There are health benefits to walkable and bikeable neighborhoods. For example, people are more likely to walk or bicycle if they can do so to meet their daily needs. Studies show that walking reduces the risk of high cholesterol, increased blood pressure, diabetes and obesity, as well as improved mental health. For senior citizens, walkable neighborhoods are linked with greater physical activity,¹ which can slow down the aging process, and allow people to live independently for a longer time.² Places where driving is not the only travel option are also safer, as higher rates of driving mean a greater risk of car crashes.³ Streets designed for walking and bicycling often have lower speeds, which reduces the risk of injury for everyone who uses streets in the community, regardless of travel mode.

More safe ways to walk and bicycle also allow more people to access public transportation, expanding the number of places to which people can travel without a car. Active transportation modes are recognized as the connectors between origin, public transportation, and destination in a multimodal network, and as such are frequently referred to as the “first and last mile” transportation connections.

Walking and bicycling infrastructure has economic benefits as well. A survey from the National Association of Homebuilders found that prospective homebuyers rank trails as one of their most desired amenities.⁴ Likewise, a study from the National Association of Realtors noted that 60% of households and 67% of families with children are willing to spend more on a home to live in a walkable neighborhood.⁵ More businesses are seeking walkable and bikeable locations, and a study from the Urban Land Institute found that they will pay higher rents to be there.

What is a walking and rolling network?

Walk & Roll Anne Arundel! presents a vision for a comprehensive network of infrastructure, strategies and policies that, once built, will allow people to access essential destinations throughout Anne Arundel County. *Walk & Roll Anne Arundel!* defines and prioritizes the following facility types in the bicycle and “shared-use” network:

- **Shared-Use Paths:** A bikeway physically separated from motor vehicle traffic by an open space or barrier and either within the highway right-of-way or within an independent right-of-way. Shared-use paths may also be used by pedestrians, skaters, wheelchair users, joggers, and other non-motorized users, and motorized users only as permitted in this Code and in State Code, and may be designed for two-way travel.⁶ Trails and sidepaths are types of shared-use paths in the County.
- **Separated Bicycle Lanes:** An exclusive facility for bicyclists, also called a “cycle track” or “protected bicycle lane”, that is located within, or directly adjacent to, the roadway and that is physically separated from motor vehicle traffic with a vertical element.⁷
- **Buffered Bicycle Lanes:** Buffered bicycle lanes are a combination of a conventional bike lane with a designated buffer space. This designated buffer space provides lateral separation between the bike lane and adjacent motor vehicle travel lanes or parking space.⁸
- **Standard Bicycle Lanes:** A portion of the roadway designated for use by bicyclists through the use of pavement markings and signage. Bike lanes are typically located on the right side of vehicular travel lanes and generally run in the same direction of motor vehicle traffic.⁹
- **Sidewalks:** Paved paths for pedestrians on the side of a street. In Anne Arundel County, it is legal to ride a bicycle on a sidewalk.

Goals of this Plan

The goals of *Walk & Roll Anne Arundel!* are founded on the County’s priorities to create safe, comfortable, and connected ways to walk or bicycle, reduce crashes, and ensure that people who are least likely to drive are able to travel to their destinations.

Specifically, the goals established in this Plan – and the strategies used to prioritize active transportation projects – are to:

- Prioritize a safe and comfortable – low-stress – walking and rolling network between essential destinations throughout the County. Essential destinations are defined as:
 - » Job centers
 - » Shopping centers
 - » Schools (K-12, community colleges, colleges, and universities)
 - » Hospitals and health care clinics
 - » Libraries
 - » Parks
 - » Residential areas
 - » Transit stops and stations.
- Ensure that vulnerable people have access to active transportation infrastructure by prioritizing projects in census tracts with higher than average rates of vulnerable people, including people with low incomes, older residents, people of color, and households without a car.
- Recommend infrastructure proven to reduce pedestrian crashes at intersections and along corridors with high pedestrian crash rates, especially crashes resulting in death or serious injury.
- Identify policy recommendations to promote the construction of safe, accessible, and direct walking and rolling infrastructure.

1 <https://www.nar.realtor/sites/default/files/documents/2017%20Analysis%20and%20slides.pdf>

2 <https://urbanland.uli.org/news/walkable-downtowns-drawing-companies-talent/>

3 https://nhts.ornl.gov/assets/2017_nhts_summary_travel_trends.pdf

4 <https://www.pewresearch.org/fact-tank/2018/03/01/millennials-overtake-baby-boomers/>

5 <https://www.brookings.edu/blog/the-avenue/2020/07/30/how-more-than-half-of-americans-are-millennials-or-younger/>

6 <https://www.aacounty.org/departments/transportation/forms-and-publications/multimodal-transportation-review-guidelines.pdf>

7 <https://www.aacounty.org/departments/transportation/forms-and-publications/multimodal-transportation-review-guideline.s.pdf>

8 <https://www.aacounty.org/departments/transportation/forms-and-publications/multimodal-transportation-review-guideline.s.pdf>

9 <https://www.aacounty.org/departments/transportation/forms-and-publications/multimodal-transportation-review-guideline.s.pdf>

These goals build on the goals of *Move Anne Arundel!*, the County’s first Transportation Functional Master Plan adopted in 2019, of which the following relate to active transportation:

- Establish a multimodal transportation system that is safe, and
- Provide reliable transportation choices throughout the County.

Move Anne Arundel! sets targets for 2040, including increasing non-single occupant mode share to 20% for all commute trips. Target mode share to Town Centers are shown in Table 1.

Table 1: *Move Anne Arundel!* Mode Share Targets for Commuter Trips to Town Centers

	Odenton		Glen Burnie		Parole	
	2017	2040	2017	2040	2017	2040
Drove Alone	79%	71%	82%	74%	79%	71%
Walk, Bike, Transit, Carpool, Work from home	21%	29%	18%	26%	21%	29%

National Trends

The ways U.S. residents get around are continuing to change as more people shift from driving to other modes of transportation. More Americans are walking, for example, in 2017, 18% of Americans said they walk for social or recreational activities, and 13% for work or work-related activities, compared to 7% and 4%, respectively, in 1995.¹⁰ This shift is especially pronounced among Millennials (adults born between 1981 and 1996) and Generation Z (those born in 1997 or later), the two generations comprising a population majority in the United States today,¹¹ totaling over 166 million people.¹²

Due to a combination of lower incomes, increases in the cost of gas and car ownership, and changing habits, younger people are driving less than previous generations at the same age. In 2017, only 25% of 16-year-olds had driver’s licenses, compared to 46% of 16-year-olds in 1983.

10 https://nhts.ornl.gov/assets/2017_nhts_summary_travel_trends.pdf

11 <https://www.pewresearch.org/fact-tank/2018/03/01/millennials-overtake-baby-boomers/>

12 <https://www.brookings.edu/blog/the-avenue/2020/07/30/now-more-than-half-of-americans-are-millennials-or-younger/>

13 <https://data.census.gov/cedsci/table?q=commuting&g=0400000US24&tid=ACST5Y2020.S0801>

14 <https://data.census.gov/cedsci/table?q=commuting%20anne%20arundel&tid=ACST5Y2010.S0801>

15 <https://data.census.gov/cedsci/table?q=commuting%20anne%20arundel&tid=ACST5Y2010.S0801>

16 https://baltometro.org/sites/default/files/bmc_documents/general/community/demo-and-socio-forecasting/Round-9A_Jurisdictional.pdf

Local Trends

In recent years, Anne Arundel County has continued to develop beyond suburban land use patterns, and now includes major centers of employment. Sixty percent of Anne Arundel County residents both live and work within the County. With an average commute time of 31 minutes, it is slightly higher than the national average of 26 minutes, but below the average of 33 minutes for the state of Maryland.¹³ This average has remained relatively stable over the last ten years.

The commuting patterns of Anne Arundel County residents have changed slightly over the course of the last ten years. An estimated 78% of Anne Arundel County residents drive alone to work, which is a 3% decrease from 2010.¹⁴ However, there are fewer people reporting walking to work (a decrease from 2% to 1.8%) and bicycling to work (a decrease from 0.3% to 0.2%).¹⁵ People walking and bicycling to work are more likely to be seen at higher rates in areas where people live closer to their jobs, such as the County’s Town Centers. With the economic growth being seen around these areas, the County can expect an increased demand for active transportation options.

Regional Growth and Congestion

With a population of just under 600,000 and trending upward, Anne Arundel County is seeing increased development as it works towards achieving the goals of *Plan2040*, the County’s comprehensive plan for growth and development. The population is projected to grow 14% by 2045,¹⁶ but employment numbers are expected to increase 28.4% in this same time period. With this level of growth, and the network of existing roadways with high traffic volumes, it is anticipated that the levels of congestion will increase in the future. Much of the growth and congestion within the County is seen around the three Town Centers that have been identified as focal points within this plan: Glen Burnie Town Center, Parole Town Center, and Odenton Town Center.

These areas are critical places for the County to improve multimodal conditions, to provide travel choices while

limiting additional strain on the road network. Creating compact and mixed-use communities connected by walkable and bikeable streets will allow people to travel without a car safely and comfortably.

Cities and counties across the United States are taking this approach to tackle congestion, improve public health, promote economic development, or protect the environment. People For Bikes, a national organization that promotes safer bicycling, has produced a Bicycle Network Analysis (BNA) that measures how well a city’s bicycle network connects people with the places they want to go, focusing on low-stress bicycle routes. The BNA shows how Anne Arundel County compares to other counties in Maryland, as well as peer counties around the nation with a similar population, geography, or demographics. Each city receives a score from 0 to 100. The score rates access to different types of destinations from a low-stress bicycle facility, including: public recreation facilities, shopping areas, transit, and core services like health care, jobs, and education. A lower score means a city has poor access to these destinations, while higher scores indicate better access.

Table 2: Bicycle Access with Anne Arundel County.

Bicycle Access within Anne Arundel County ¹⁷				
	Crofton	Glen Burnie	Gambrills	Odenton
Population	29,350	74,950	7,085	43,690
Core Services	4	10	6	15
Education and Employment	22	18	21	26
Recreation	54	20	33	39
Retail	18	21	16	24
Transit	0	3	0	0

The table above breaks down the Bicycle Access scores within Anne Arundel County by the six scoring categories. Countywide, the scores are consistently low in 5 out of 6 categories, with recreation being the only outlier.¹⁸ Crofton has a significantly higher score for access to recreation. This could be due to a variety of factors, such as the convenient locations of Crofton Park and other recreation opportunities within the community. One strikingly low

17 <https://bna.peopleforbikes.org/#/places/5e3a2c5f-8038-4656-9e22-97bcb8e29299/502514b2-45fb-4535-975b-4e67c4452b8a//>

18 The scores as explained by People for Bikes: “Points are assigned on a scale of 0 to 100 for each destination type based on the number of destinations available on the low-stress network as well as the ratio of low-stress destinations to all destinations within biking distance. The scoring places higher value on the first few low-stress destinations by assigning points on a stepped scale. Beyond the first few low-stress destinations, points are prorated up to 100 based on the ratio of low-stress to high-stress connections to those destinations. For example, a census block with low-stress access to only one park out of five nearby parks would receive 30 points. A census block with low-stress access to two parks out five would receive 50 points (30 for the first park, 20 for the second). A census block with low-stress access to four parks out of five would receive 85 points (30 for the first, 20 for the second, 20 for the third, and 15 out of the remaining 30 points for connecting one of the remaining two parks).”

19 <https://www.walkscore.com/score>

score across the County was access to transit, with 3 being the highest score. This breakdown gives the County a starting point when it comes to increasing bicycle access throughout Anne Arundel County.

Walk Score provides another method to assess infrastructure and walkability in the County’s Town Centers. It provides an index that rates the walkability of cities and areas within on a scale from 0 to 100. The majority of Anne Arundel County received ratings that classified them as “car-dependent,” with the Town Centers receiving the follow ratings:

- Odenton – 26, car-dependent
- Glen Burnie – 36, car-dependent
- Parole – 54, somewhat walkable¹⁹

These indicators show that the County has room for improvement. Fortunately, the tools to make communities more friendly to walking and bicycling are available. For more information on the scoring methodology, see footnote 18 below.

County Plan Review

The project team reviewed the following plans previously prepared by the County:

- *2003 Pedestrian and Bicycle Master Plan*
- *2013 Pedestrian and Bicycle Master Plan*
- *Plan2040 – The County’s 2020 General Development Plan*
- *Green Infrastructure Plan*
- *Land Preservation, Parks, and Recreation Plan*
- *Vision Zero Anne Arundel County*
- *Move Anne Arundel! – The County’s 2019 Transportation Functional Master Plan*
- *Anne Arundel County Design Manual*
- *Anne Arundel County Orange Notice*
- *Anne Arundel County Code: Bicycle, Pedestrian, and Transit Infrastructure*
- *Corridor Growth Management Plan*

- Major Intersections and Important Facilities Transportation Study
- Central Maryland Regional Transit Plan
- Odenton Town Center Master Plan
- Parole Town Center Master Plan
- Plans for local areas and streets throughout the County

The project team also reviewed:

- The County’s existing “Complete Streets Policy” and Guidance
- The initial comments, input, and documents for the Regions 2, 4, and 7 Plans
- Projects funded in the County’s Capital Improvement Program
- The Anne Arundel Trail Network
- The Baltimore Metropolitan Council’s Maximize 2045 – The Baltimore region’s regional transportation plan
- 2040 Maryland Bicycle and Pedestrian Master Plan
- The Maryland State Highway Administration’s “Complete Streets Policy”
- Context Sensitive Solutions Manual by the Maryland State Highway Administration
- State of Maryland’s Bicycle Policy and Design Guidelines
- AASHTO Guidelines for the Development of Bicycle Facilities
- NACTO’s Street Design and Urban Bikeway Design Guide
- FHWA’s Bikeway Selection Guide

Previous Plans/Performance Measure/Goals

The 2003 Anne Arundel County Pedestrian and Bicycle Master Plan and 2013 Pedestrian and Bicycle Master Plan established policy recommendations and corresponding infrastructure goals for bicycle and pedestrian achievement and advancement.

Table 3: 2003 Pedestrian and Bicycle Master Plan Infrastructure Goals.

Type of Improvement	During Years 1-5	During Years 6-10	During Years 11-15	Total Years
Bike Lanes/Shoulders	20 miles (4 miles/year)	+75 miles (+15 miles/year)	+120 miles (+24 miles/year)	+215 miles
Pedestrian Safe Intersections	40 intersections (8/year)	+80 intersections (+12/year)	+80 intersections (+12/year)	+180 intersections

2003 Anne Arundel County Pedestrian and Bicycle Master Plan

The infrastructure goals (Table 3) and policy recommendations (Table 4) were set in the 2003 Anne Arundel County Pedestrian and Bicycle Master Plan. Table 4 lists the policy recommendations as well as their completion status.

Infrastructure Goals

For more detail related to the status of the infrastructure goals established in 2003, please reference the table of Existing and Proposed Bicycle Facilities in Chapter 3.

Table 4: 2003 Pedestrian and Bicycle Master Plan Policy Recommendations.

Recommendation	Status
Amend the Zoning Ordinance, Subdivision Regulations, and Design Manual to improve conditions for walking and bicycling in the County	Incomplete
Put general requirements to address bicycle and pedestrian needs in the Zoning and Subdivision Regulations and place detailed design provisions in appropriate sections of the Design Manual	Incomplete
Move detailed site layout design requirements relating to pedestrian and bicycle facilities into the Design Manual	Incomplete
Adopt bicycle parking requirements	Incomplete
Revise standard road sections to better accommodate the needs of bicyclists and pedestrians	Incomplete
Add a standard section for off-road trails into the Design Manual	Incomplete
Investigate the potential of modifying utility easement agreements to allow for shared-use utility and trail development	Incomplete
Expand the Design Manual’s section on buildings to address pedestrian access	Incomplete
Expand the Design Manual’s treatment of intersection design to accommodate the needs of all users	Incomplete
Revise project reviewer checklists to incorporate pedestrian and bicycle elements	Incomplete
Incorporate the Neighborhood Traffic Control Program in the CIP	Incomplete
Continue the tradition of funding sidewalks and bikeways through the Capital budget; look for more opportunities to address bicycle and pedestrian concerns during “regular” roadway improvement projects and maintenance schedules.	Complete

2013 Pedestrian and Bicycle Master Plan

Walk & Roll Anne Arundel! builds on the recommendations made in the County’s previous plans, notably the 2003 Pedestrian and Bicycle Master Plan, the 2013 Pedestrian and Bicycle Master Plan, and Move Anne Arundel! The proposed projects and policies adopted in those plans remain valid. A list of sidewalk projects proposed in the 2013 Pedestrian and Bicycle Master Plan is available in the appendix. Policy recommendations were set in the 2013 Anne Arundel County Pedestrian and Bicycle Master Plan, which are listed in Table 5 with their completion status.

Table 5: 2013 Pedestrian and Bicycle Master Plan Policy Recommendations.

Recommendation	Status
Design Manual	
Adopt a set of design guidelines for pedestrian and bicycle facilities for inclusion in the Design Manual.	Incomplete
Require a minimum clear width sidewalk of 5-feet for all County sidewalks.	Complete
Update the Standard Details for roadway typical sections to reflect the inclusion of different pedestrian and bicycle facility types (e.g., shared-use roadway, on-road bicycle lane, and shared-use path).	Incomplete
Update the Design Manual to refer to a “Complete Streets Policy” and Design Criteria for guidance designing roadway improvements to be safe efficient routes for travel by all modes.	Incomplete
County Code – Subdivision and Development Regulations (Article 17)	
Update the General Provisions (Article 17 §2-102) to include a provision for the consideration of all modes of travel to include accessible pedestrian and bicycle facilities as viable transportation alternatives.	Complete
Update the Site Development Plan outlined in Article 17 §4-202 to include a more robust description of pedestrian and bicycle connectivity, including at the preliminary plan stage.	Incomplete
Incorporate pedestrian and bicycle facilities into Title 5, which discusses the need to include Adequate Public Facilities in accordance with “General Development Plan growth objectives” to ensure connection to the existing pedestrian and bicycle system and to connect the planned facilities in the appropriate width and with the proper grades and cross slopes.	Incomplete
Update Article 17 Subtitle 4 which discusses “Adequate Road Facilities” to include specific reference to pedestrian and bicycle facilities.	Incomplete
Include the establishment of a “Pedestrian/Bicycle Fee in Lieu of Construction” type program within Title 5 to create a County fund for use in implementing pedestrian and bicycle facilities.	Complete

Recommendation	Status
Provide a strong clear discussion of the need to provide right-of-way and construction of Complete Streets with accessible pedestrian and bicycle facilities compliant with the Pedestrian and Bicycle Master Plan in Title 6, Article 17 §6-103 through a site development plan.	Incomplete
Amend Article 17 §11-209 to explicitly state that pedestrian and bicycle facilities fall within the eligible capital improvements which can be paid for through the use of impact fees.	Incomplete
County Code – Zoning (Article 18)	
Amend Title 3 to include a requirement for bicycle parking as well as establishing the number of required bicycle parking spaces.	Incomplete
Include bicycle parking requirements for additional zoning districts.	Incomplete
Amend Section Q, Recreational Facilities, to include the provision of bicycle parking.	Incomplete
Include a list of approved and/or recommended bicycle parking types in the Appendices of the Design Manual.	Incomplete
Complete Streets Policy	
Develop and adopt a “Complete Streets Policy” for Anne Arundel County to guide designers, developers, and government officials in the implementation of roadways which include comfortable facilities for motorists, pedestrians, bicycles, and transit vehicles.	Complete
Integrate the “Complete Streets Policy” into the County Code, Subdivision and Development Regulations, Design Manual, and Landscape Manual where appropriate to ensure that roadway projects and developments completed in the County provide the equitable inclusion of facilities for all modes at all stages of the planning, design, and construction processes.	Incomplete
Administrative	
Create a Pedestrian and Bicycle Coordinator permanent staff position within the department where it will be most effective. The overarching responsibility of the Pedestrian and Bicycle Coordinator should be to oversee the planning, prioritization, coordination and implementation of pedestrian and bicycle projects throughout the County.	Complete
Facility Maintenance	
Include any roadway with a bicycle facility (shared-use roadway, shoulder, or bicycle lane) on the County’s Street Sweeping list.	Incomplete
Streamline facility maintenance requests from citizens through an online form available on the County website.	Complete
The Pedestrian and Bicycle Coordinator should complete field reviews of pedestrian and bicycle facilities in high use areas and in areas where issues have been reported by citizens.	Ongoing
Develop a maintenance process for shared-use facilities which addresses issues such as maintenance responsibility, hours of operation, funding, snow removal, staffing, and equipment needs.	Incomplete

Plan2040

Plan2040 is the County's General Development Plan, guiding the vision for how the county should grow and evolve over the next 20 years. Adopted in 2020, it was the result of extensive community outreach with dozens of public forums and thousands of individual comments, and has three main themes: Green, Smart, and Equitable. Plan2040 reflects ongoing trends that are reshaping the county, such as increased commuting to Washington, DC and Howard County and fewer trips to Baltimore City, recurring concerns about infrastructure supporting continued population and job growth, and an effort to address racial and socioeconomic inequity and provide economic and social opportunities to all County residents.

The Plan identifies several challenges that may be addressed, including making the area more resilient to climate change, investing in more transportation choices, encouraging transit-oriented development, and providing better access to the waterfront and park system. It identifies areas that will experience limited development, such as agricultural areas and the peninsulas, as well as targeted locations for redevelopment and revitalization, all of which will require further investments in pedestrian and bicycle infrastructure.

The County adopted these specific goals through Plan2040 to resolve transportation-related challenges:

- GOAL NE3: Expand, enhance, and continue to protect the County's greenways, open space, and rural areas, including the Priority Preservation Area (PPA);
- GOAL BE4: Support quality of life and economic vitality in County Peninsula Policy Areas, while preserving environmentally sensitive areas.
- GOAL BE7: Promote vibrant, high-quality development in Town Centers that provides opportunities to live, work, learn, and play without daily use of a car;
- GOAL BE9: Provide a high-quality mix of employment, residential, commercial and service uses near existing or funded transit stations.
- GOAL BE10: Relieve traffic congestion and improve mobility options and safety in the

Critical Corridor Policy Areas;

- GOAL BE15: Provide a well-maintained multimodal transportation network that is safe, efficient, environmentally sensitive, and provides practical and reliable transportation choices and connections for all users;
- GOAL BE16: Increase the County's resilience to future changes in climate and reduce emissions of greenhouse gases;
- GOAL HC8: Provide a diverse range of accessible public recreational facilities to serve the needs of all County residents; and
- GOAL HE1: Promote economic development that supports smart growth and provides opportunities for all County residents.

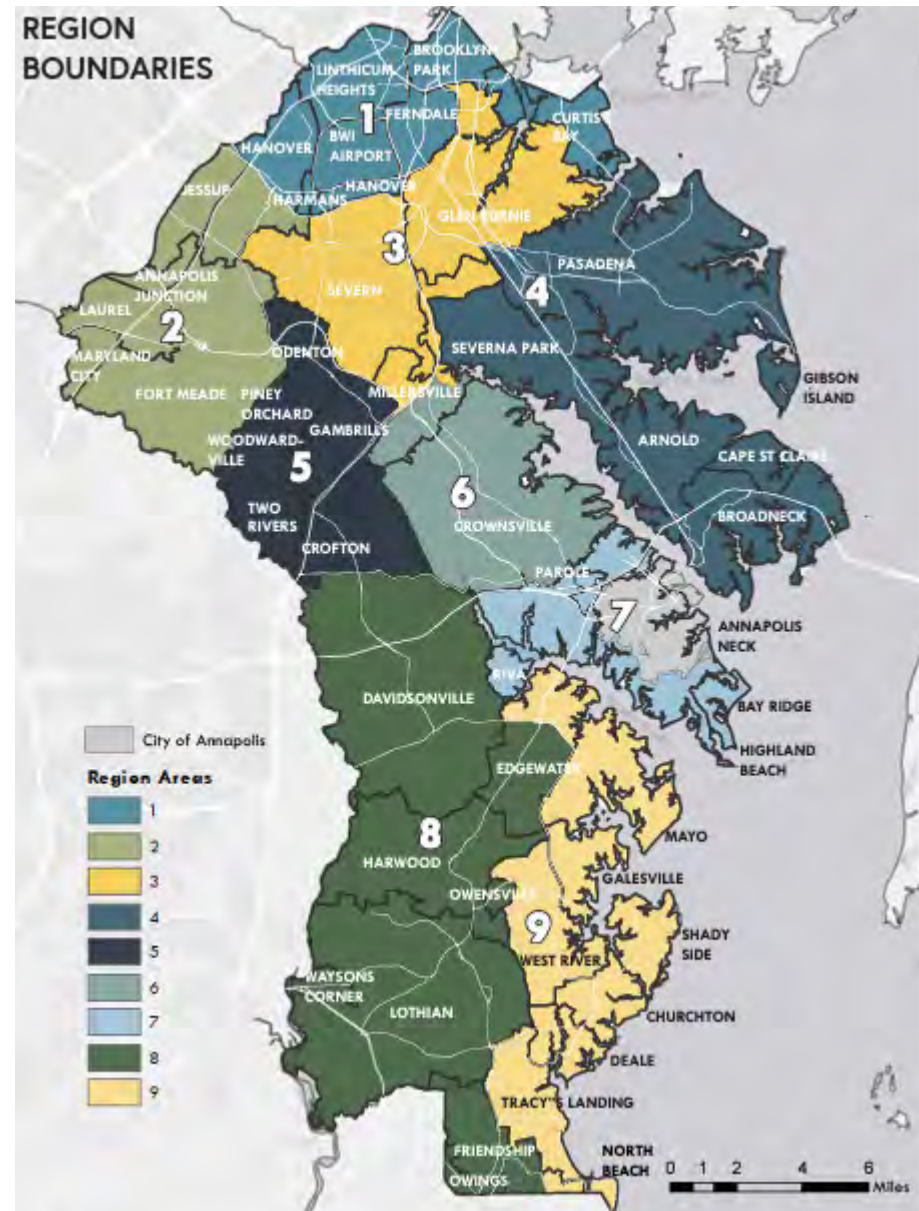


Figure 7: Anne Arundel County's Nine Regional Planning Areas

Plan2040 established nine Regional Planning Areas to organize community members' shared vision for future development. In addition to a 20-year vision, each of the nine Region Plans, currently in development, will provide recommendations for land use, zoning, environmental protection, transportation, public facilities, and community design. The bicycle infrastructure and shared-use path recommendations issued in *Walk & Roll Anne Arundel!* are presented for each Regional Planning Area.

Through Plan2040, the County also adopted Development Policy Areas. As shown in Figure 9, these Development Policy Areas broadly identify areas in the County where development and redevelopment are encouraged, as well as areas where preservation of rural or suburban character and natural features are prioritized. They provide a framework for implementing the goals, policies, and implementing strategies in Plan2040. They also provide guidance for investments in public facilities and services as appropriate for the type of planned development in each area.

In addition, Plan2040 established 13 Planned Land Use Designations, which specify the land uses to guide future development and redevelopment. Figure 10 shows the 13 designations.



Figure 8: Bicyclists and pedestrians of all ages and abilities utilize a shared-use path.

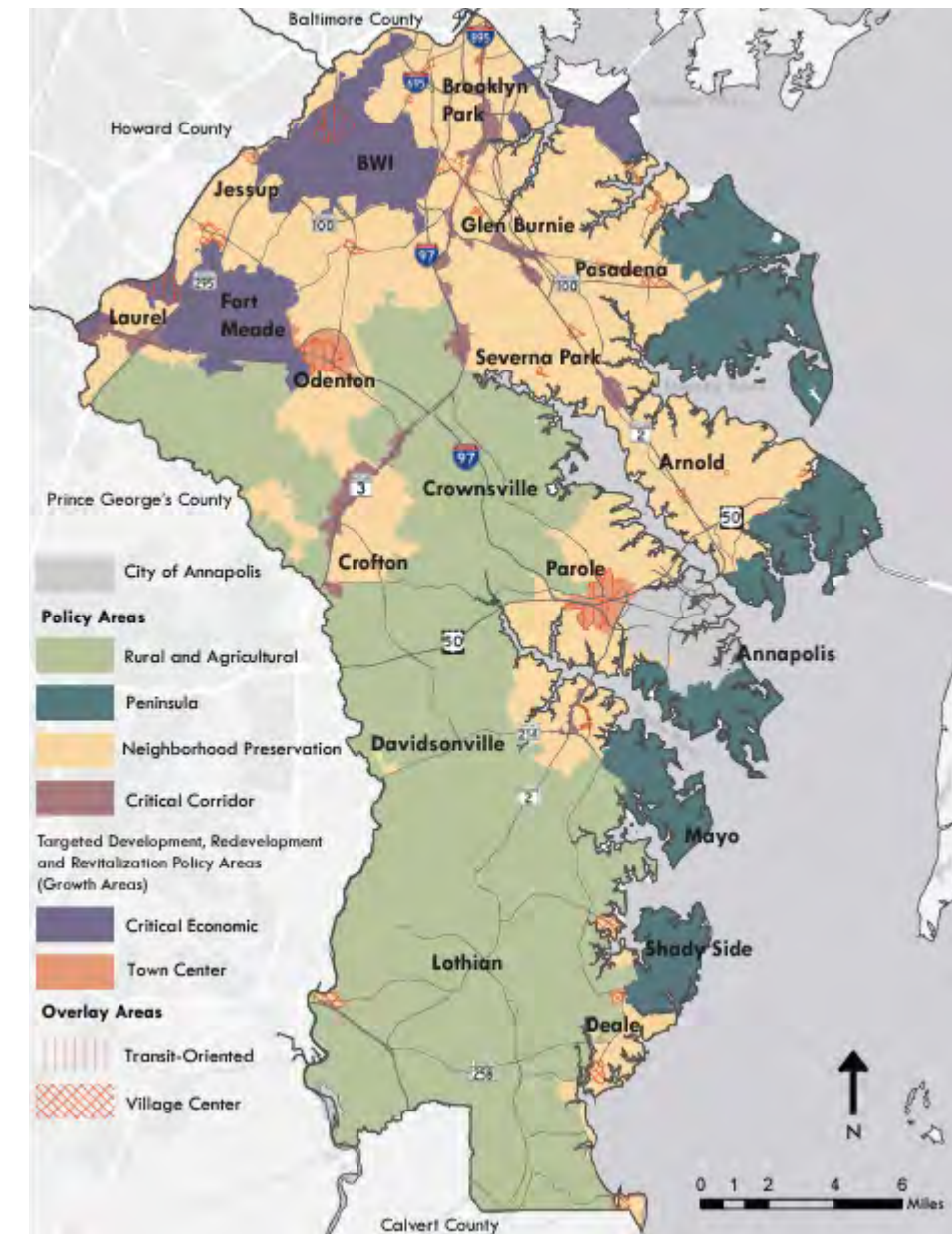


Figure 9: Anne Arundel County's Development Policy Areas

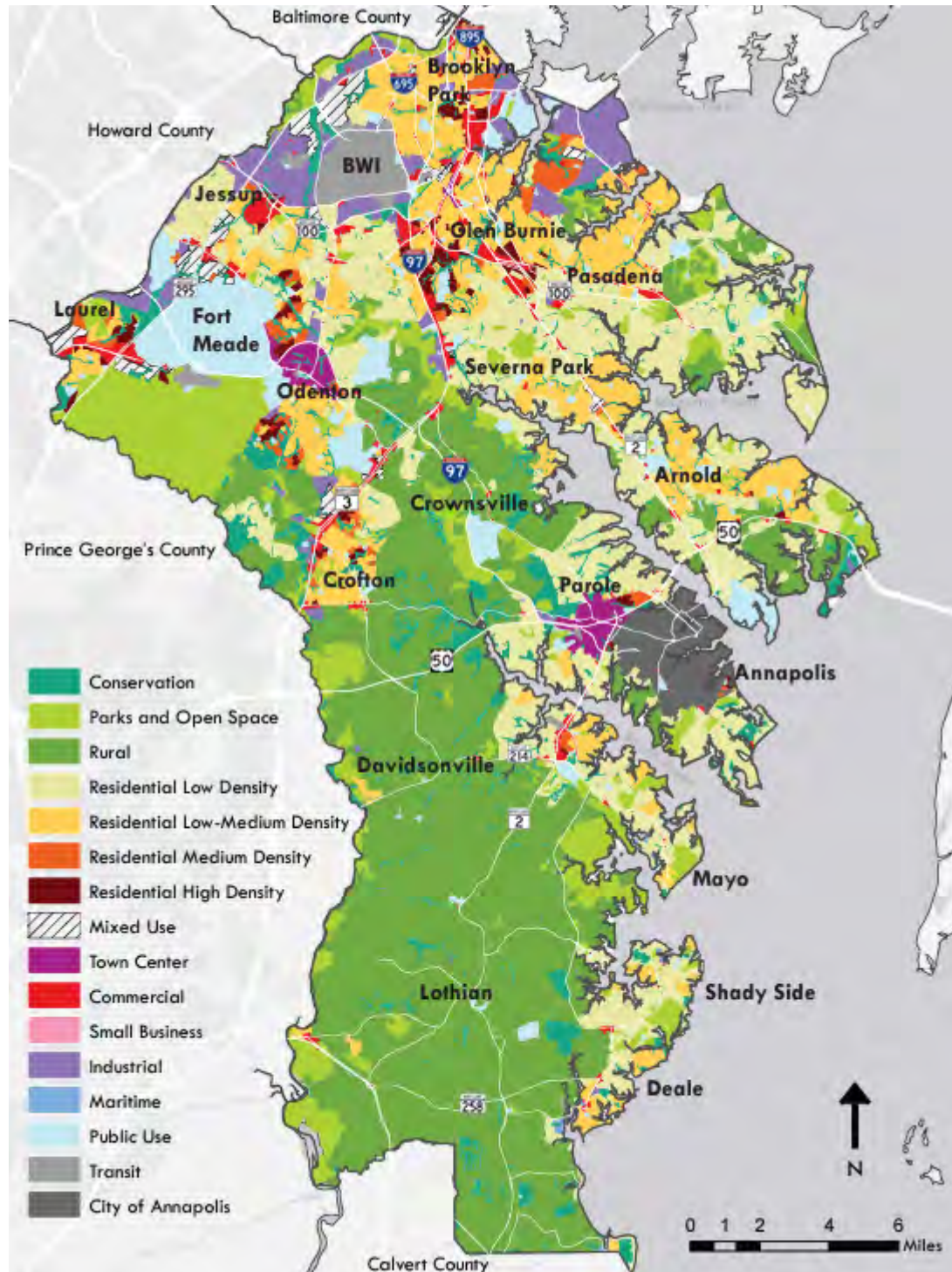


Figure 10: Anne Arundel County's 13 Planned Land Use Designations as adopted in Plan2040

Other Factors Shaping Plan Development

The *Walk & Roll Anne Arundel!* Plan is informed and guided by extensive outreach to residents and incorporates findings from a comprehensive analysis of pertinent transportation system data. Specifically, the team gathered feedback via in-person public meetings, virtual public meetings, and an online survey, as described in Chapter 2.

The project team also conducted the following analyses to inform the prioritization of walking and rolling infrastructure based on relevant trends:

- Hot spots of pedestrian and bicyclist crashes, including crashes resulting in death or serious injury of a person walking or bicycling;
 - Identification of roadways with characteristics that are conducive to higher rates of crashes, including higher numbers of travel lanes, higher levels of daily traffic, and higher traffic speeds;
 - Density of job centers and residential areas;
 - Locations of schools, medical centers, shopping centers, parks, and transit stations or stops; and
 - Locations of existing or planned walking and bicycling facilities, or those that are currently under construction.
- Using this information the project team identified the most appropriate routes that will connect people to the destinations to which they travel on a regular basis to meet their daily needs.



Figure 11: Pedestrians lack adequate crossing facilities on Daele Churchton Road.

Anne Arundel Trail Network

The project team also sought insight from local advocacy groups as well. One such effort that informed the development of the network is the Anne Arundel Trail Network (AATN). Bike AAA, a local bicycle advocacy group, developed the AATN with the vision of creating a world-class network of multi-use trails that equitably connect residential, employment, education, recreation, cultural and natural areas within the county and to the greater Washington and Baltimore trail networks. While the AATN is not an official County product, it has been endorsed by County officials as a valued source to guide the development of our bicycle and pedestrian network.

Approval Process

The approval process for *Walk and Roll Anne Arundel!* involves the approval of the Planning Advisory Board followed by the County Council Adoption.

Chapter 2: Public Engagement Process and Feedback



Figure 12: A public meeting for Walk & Roll Anne Arundell in April 2022.

The *Walk & Roll Anne Arundel!* study process was centered around listening to the public, to learn about their experiences, challenges, and suggestions for potential improvements. To diversify outreach methods and gather comments from a broad range of people, the project team utilized the following four outreach methods:

- An online, interactive webmap and survey
- In person public meetings
- Presentations to and conversations with selected local committees
- Virtual public meetings

Online Webmap and Survey

The online webmap and survey invited members of the public to document their experiences, preferences, challenges, and concerns related to walking and rolling throughout Anne Arundel County. Participants were invited to take an eight-question survey and were given the option to share demographic data. They were then directed to an interactive map of the County, where they could use point and line drawing tools to comment on areas that they believe need walking and bicycling improvements. Using a crowdsourcing technique, people could also comment on proposed ideas that were presented by others on the webmap. The map also displayed existing bike infrastructure for reference.

The online webmap received over 800 unique comments related to the County's existing and proposed bicycle network. These comments are documented in

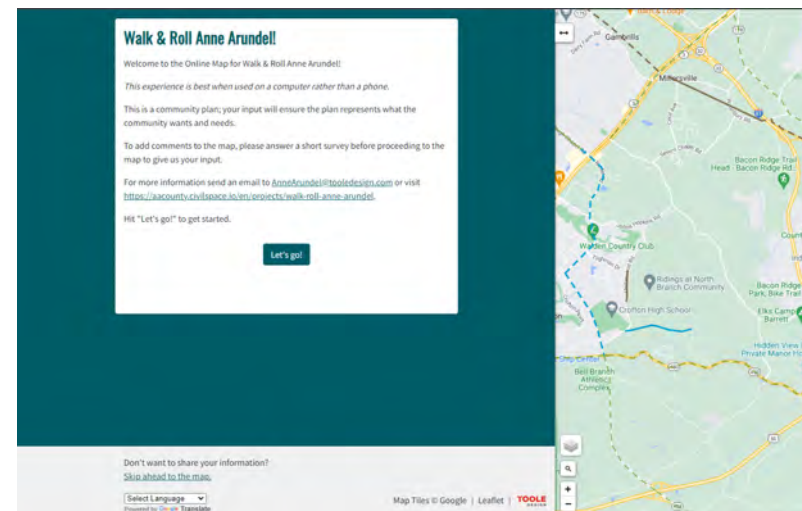


Figure 13: Online interactive webmap.

Appendix H: Web Survey Public Comments. It also solicited information on suggested improvements that community members wanted to see within the County.

Comments and concerns expressed on the webmap centered around trail connectivity, the need for protected or buffered bicycle facilities, and a desire for direct neighborhood connections.

Many of the comments followed similar themes, such as a desire for improved connections to the County's existing trails network and a desire for improved connections to the WB&A Trail. Additionally, many commenters were looking for upgrades to existing infrastructure, such as adding additional protection or buffers to existing bicycle lanes. There were over 140 comments mentioning a lack of shoulder on high-volume roadways, creating unsafe and/or uncomfortable conditions for bicyclists and pedestrians. Commenters coming from primarily residential areas noted a desire for increased direct connections to nearby neighborhoods. While they may live geographically close to friends, or their children's friends, it was noted that it is not currently safe for them to walk or bike there.

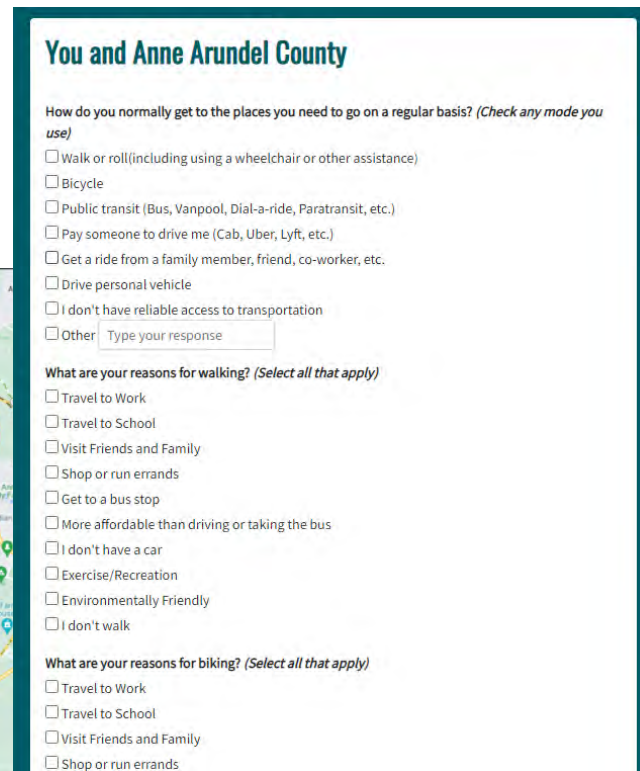


Figure 14: First Set of Online Survey Questions.

Survey

The survey presented with the map consisted of a range of questions focused on community members' travel habits, motivating and deterring factors, and desires surrounding walking and biking within the County. The survey also collected basic demographic data. The survey received over 800 unique responses and the webmap received over 700 in the period in which it was publicly available.

Demographic data show that the majority – about 74% - of respondents were white. Respondents between ages 35-64 years of age represented around 66% of all responses. Additionally, 59% of respondents reported not having school-aged children. Approximately 8% of respondents reported having a disability. While it provided important and useful insights, it should be noted that this was not a statistically representative survey of the County.

Most survey respondents – 96.5% - reported using a personal vehicle on a regular basis. The remaining portion of respondents were almost evenly split between bicycling and walking or rolling. However, almost all respondents listed exercise or recreation as their primary reason for walking or bicycling. Two other common reasons were for visiting friends and family and to be environmentally friendly. Very few respondents reported relying on public transit to meet transportation needs on a regular basis.

When asked about level of comfort cycling, 61% of respondents stated that they are interested in bicycling in the County, but there is something preventing them from doing so. 26% of respondents reported riding their bicycles sometimes, but wish it felt more comfortable,

and 5.6% reported being comfortable traveling most places by bicycle. The number one thing keeping people from walking or bicycling more is a lack of safe, comfortable, and appealing infrastructure. Almost 96% of respondents said that having more sidewalks, bicycle lanes, or trails in the community would encourage them to walk or bike more. Respondents also stated that they do not feel safe walking or bicycling due to dangerous traffic or dangerous intersections. Other changes that would encourage community members to walk or bike more include more accessible infrastructure, better lighting of sidewalks, trails, and roads, and better wayfinding.

The responses on the survey provided important perspectives about gaps in safe infrastructure across the County, with many respondents being deterred from walking and bicycling due to a lack of safe infrastructure. For further information and a list of specific public comments, see Appendix I.



Figure 15: *Walk & Roll Anne Arundel!* survey respondents' reported comfort level with bicycling.

What would encourage you to walk or bicycle more?

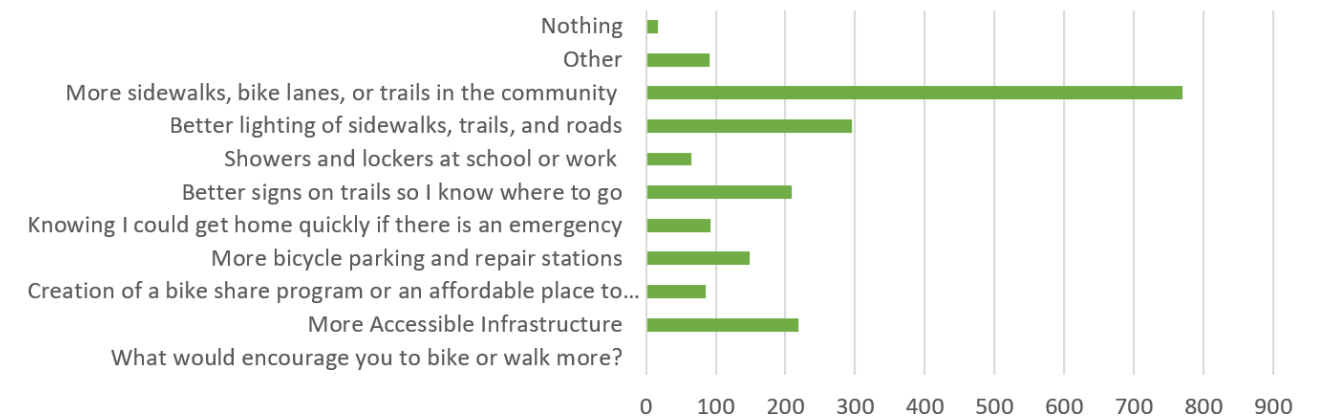


Figure 16: *Walk & Roll Anne Arundel!* survey responses on infrastructure and services needed from them to walk or bicycling more.

Public Meetings

In the early stages of plan development, three in-person and one virtual public meeting were held to accommodate residents living in various locations across the County. In-person meetings were held in Annapolis on April 19th, 2022, Edgewater on April 26th, 2022, and Linthicum Heights on May 3rd, 2022, with 4, 15 and 15 attendees, respectively. Between the two in-person meetings, 88 written comments were collected. The format of the in-person meetings featured a presentation and interactive component where attendees were able to place sticky notes to indicate their habits, thoughts, and concerns about travel throughout the County. Participants were also given the opportunity to share what was important to them when walking, rolling, driving, or using transit. The virtual meeting, held on May 17th, 2022, included 15 confirmed attendees who provided 15 written comments during the meeting.

When expressing thoughts and opinions on existing conditions Countywide and more specifically on the proposed 15 corridors, meeting attendees shared their desire for increased connectivity. For example, at the meeting in Edgewater many attendees from South County mentioned that they live on and/or in the vicinity of peninsulas, leaving them with one road to access many of their nearby neighbors and friends. Ideally, they'd be able to walk, bicycle, or otherwise roll there. However, these roadways are often high speed and lack infrastructure, rendering them unsafe for pedestrians and bicyclists. At the Linthicum Heights meeting, attendees expressed similar concerns surrounding a lack of safe, direct, bike and pedestrian facilities. Alternatively, at the virtual meeting many attendees were more recreational bicyclists and were looking for improvements in recreational routes.

The public meetings also gave the project team an opportunity to engage in conversations relating to attendees' priorities when getting from place to place, whether it be walking, bicycling, rolling, or driving. For bicyclists, many attendees reported wanting direct and convenient routes, physical separation and protection between bicycles and vehicles, and bicycle storage at their destinations. For those walking and rolling, sidewalks and safe crossings were by far the main priority. Many transit riders were looking for safe first-last mile connections to and from their destinations. Additionally, secure bike parking and storage at transit stops is important. Drivers voiced concern about being held up by slower bicyclists, stop and go traffic, and motorists not paying attention to driving, speeding, or engaging in other risky behaviors.

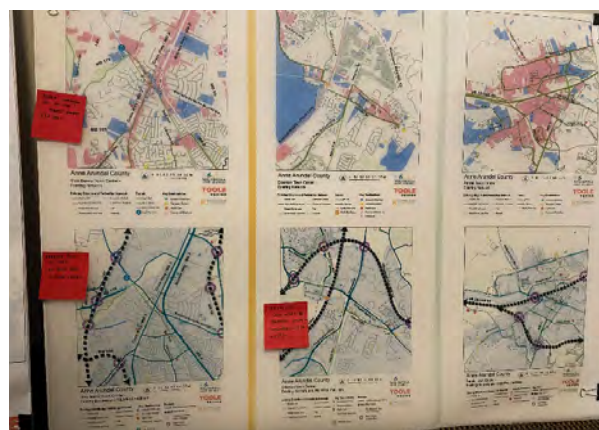
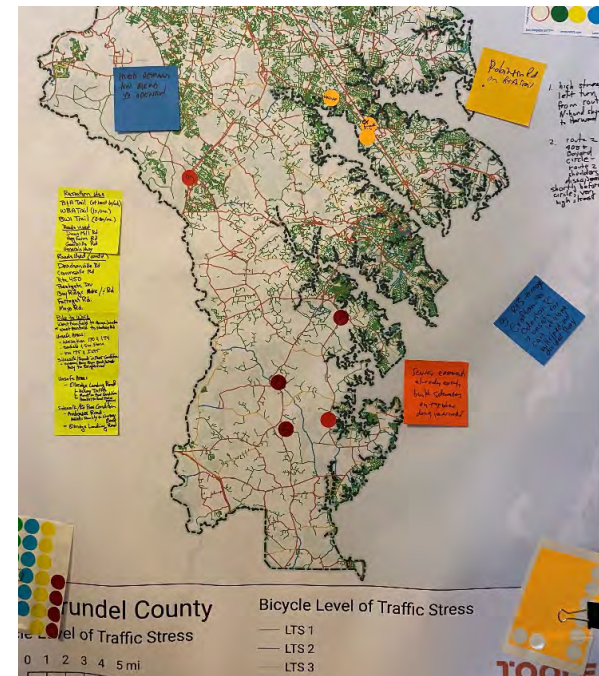
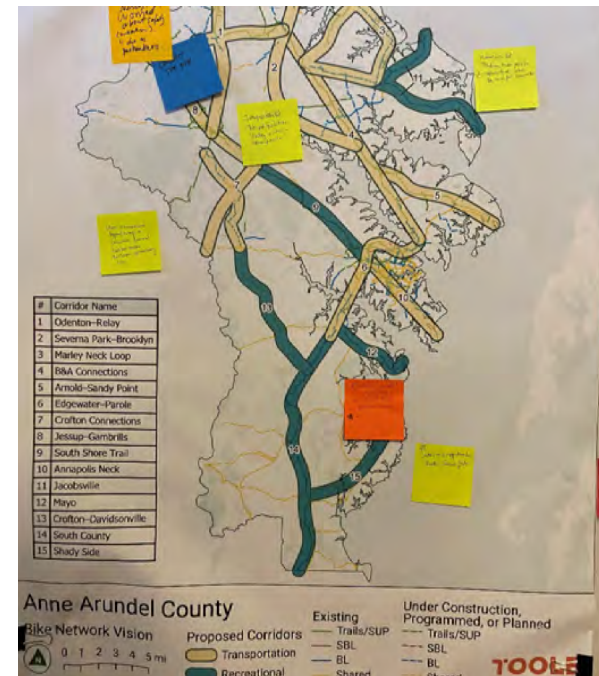


Figure 17: Example comments given at the April 2022 meetings.

Fieldwork

The project team conducted fieldwork along key corridors and in town centers to build on information gathered in the public engagement process. The goals of this fieldwork were to collect current data, including lane numbers and posted speeds, presence of pedestrian and bicycle facilities, photos of the existing conditions, and observations of existing infrastructure, such as curb ramps and crosswalks. The project team visited the following locations:

- Severn
 - » Telegraph Rd
 - » WB&A Rd
 - » Quarterfield Rd
 - » Donaldson Ave
- Hanover/Laurel
 - » Ridge Rd
 - » Arundel Mills Blvd
 - » MD 176 (Dorsey Rd)
 - » MD 175 (Annapolis Rd)
- Brooklyn Park/Linthicum
 - » Church Ave
 - » MD 648 (Baltimore Annapolis Blvd)
 - » MD 2 (Ritchie Hwy)
- Pasadena/Glen Burnie
 - » Fort Smallwood Rd
 - » Edwin Raynor Rd
 - » MD 177 (Mountain Rd)
- Severna Park/Millersville
 - » MD 648 (Baltimore Annapolis Blvd)
 - » Benfield Rd
- Odenton Town Center
 - » Reece Rd
 - » Town Center Blvd
 - » MD 175 (Annapolis Rd)
 - » Piney Orchard Pkwy
- Parole Town Center
 - » Generals Hwy
 - » Bestgate Rd
- Glen Burnie Town Center
 - » MD 3 (Crain Hwy)
 - » Quarterfield Rd
- Crofton/Crownsville
 - » Waugh Chapel Rd
 - » Riedel Rd

- » MD 3 (Crain Highway)
- » Generals Hwy
- » MD 3 (Crain Hwy)
- Arnold
 - » MD 450 (MD 450 (Defense Highway))
 - » Arnold Rd
 - » Church Rd
 - » MD 2 (Ritchie Highway)
 - » Jones Station Rd
 - » College Pkwy
- Mayo
 - » Mayo Rd
 - » MD 214 (Central Ave)
- South County
 - » MD 2 (Ritchie Highway)

By obtaining field observations of Anne Arundel County, many important observations were made about the overall state of walking, bicycling, and rolling in Anne Arundel. Some of the overarching takeaways include:

- Many observations were made of people riding in unsafe conditions; this included wrong way travel, travel on multi-lane roads, or on shoulders.



Figure 18: A person riding a skateboard on a shoulder on Donaldson Avenue in Severn, observed during the field visit.

- In many commercial corridors where bicycle lanes have been recommended, multiple and frequent curb cuts presented significant challenges to on-street bicycle lanes, as well as a constrained right-of-way.
- In addition to the three town centers, prioritizing walking and rolling infrastructure in large commercial/mixed-use zones (Arundel Mills, Waugh Chapel, etc.) is important, since these developments are conducive to walking and bicycling trips.



Figure 19: Several curb cuts exist on main corridors, such as here in Odenton.

- The most well-used facilities observed were protected shared-use paths, either trails or sidepaths.
- Some of the streets with lower traffic volumes and fewer travel lanes already provide a comfortable walking and bicycling experience and may need relatively small changes (e.g., Riedel Road in Crofton).
- Although they may have an adjacent facility such as the B&A Trail along MD 2 (Ritchie Highway), major roads such as MD 2 (Ritchie Highway) and MD 3 (Crain Highway) present significant barriers to crossing on foot and bicycle. With the exception of the B&A Trail that parallels much of MD 2 (Ritchie Highway), they also have large intersections that are major barriers and lack parallel routes that could otherwise be feasible alternatives for bicycling or walking. Signalized crossing opportunities are rare, increasing the likelihood that pedestrians will cross mid-block in commercial areas or to get to/from bus stops.
- It appears to be common practice to install one diagonal ramp on each curb rather than two ramps oriented to each crosswalk. Diagonal ramps create challenges for people with disabilities or people pushing strollers or shopping carts since they direct people away from the crosswalk.

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Chapter 3: Walking and Bicycling Network

Figure 20: The WB&A Trail near the Patuxent River

A goal of this Plan is to identify a low-stress multimodal network that allows and encourages people in Anne Arundel County to walk, bicycle, or otherwise move with their own power on safe, comfortable, and connected routes.

The following key inputs were used in the development of the network recommendations:

- Public comments received through in-person meetings, virtual meetings, and an online survey and interactive map.
- Observations and data collected through in-person fieldwork.
- Findings from a crash risk analysis.
- Location and density of important destinations throughout the County, including job centers, shopping centers, schools, medical facilities, transit stops, libraries, and parks.
- Recommendations from previous plans and studies. These recommendations were modified where necessary to create a low-stress network.
- Feedback from community members and leaders.

The project team evaluated each corridor in the study network and, for ones that needed enhanced multimodal facilities, applied one of three preliminary recommendations:

- Prioritize a new low-stress facility on the route
- Prioritize a low-stress facility on a parallel route, if the main corridor appeared to have significant constraints, such as buildings located close to the roadway
- Upgrade an existing facility on the route to make it a low-stress facility or one that is sufficient for the needs of all travelers, including people with disabilities.

To guide bicycle and shared-use path facility type selection and design, the project team primarily referenced the following resources:

1. FHWA's *Bikeway Selection Guide*
2. NACTO's *Urban Street Design Guide*
3. NACTO's *Urban Bikeway Design Guide*
4. AASHTO *Guide for the Development of Bicycle Facilities* (existing and forthcoming updated version)
5. Context Sensitive Solutions Manual by the Maryland State Highway Administration
6. ADA Transition Plans: A Guide to Best Management Practices by the National Cooperative Highway Research Program

7. Designing for All Ages & Abilities: Contextual Guidance for High-Comfort Bicycle Facilities by the National Association of City Transportation Officials

The project team identified recommended bicycle facilities or shared-use paths for each corridor. As noted, observations from fieldwork, existing design of streets, proximity to essential destinations, results of the data analyses described above, and feedback from community members informed the development of network recommendations. Recommendations are also founded on the desire to create a low-stress network throughout Anne Arundel County.

The Network

The vision for *Walk & Roll Anne Arundel!* is a connected network of walking and bicycling facilities throughout the County. Numerous barriers exist in the County's transportation network today, from interstates and four to seven lane highways like MD 3 (Crain Highway) to large bodies of water like the South River. Gaps in sidewalk, bicycle lane, and shared-use path networks also serve as barriers. These barriers can make it feel unsafe and inconvenient to walk or bicycle. Where gaps in the bicycle/pedestrian network exist, travelers are left without a safe facility to continue their journey. Where unnavigable roadway and waterway barriers exist, people must walk or roll along long, circuitous routes, or they are prevented from taking trips altogether.

When completed, the walk and roll network of low-stress connections to essential destinations will provide safe, comfortable access for anyone walking, bicycling, or using a wheelchair in Anne Arundel County. The network will fill in gaps between existing bicycle and pedestrian facilities, as well as create new routes where they do not exist today. This will be accomplished by upgrading the County's existing shared-use paths that do not meet *Walk & Roll Anne Arundel!* standards, growing the nascent separated bicycle lane network.

This update did not do a countywide sidewalk network map or gap analysis, except within the Town Centers due to the higher likelihood of pedestrian activity. Proposed pedestrian network improvements were derived from either a combination of the previous Pedestrian and Bicycle Master Plans, new recommendations from the project team, the current and previous Town Center Master Plans, the Parole Mobility Study, or other County studies performed in these areas.

Level of Traffic Stress Analysis

The project team conducted a Level of Traffic stress analysis for each street in the County, measuring how stressful the walking or bicycling experience is based on traffic speed, number of travel lanes, and separation between pedestrian and/or bicycle facilities and motor vehicle traffic. This analysis found that most streets – 74% – in the County are already low-stress routes, many of which are residential streets that carry relatively few vehicles per day. Larger streets, such as collector roads and major arterials, are less comfortable and appealing given high motor vehicle speeds and volumes. Figure 21 shows different roadway conditions and the associated level of stress for people walking and rolling.

Some routes highlighted in the Level of Traffic Stress analysis may be represented as high-stress if there is a combination of standard and substandard walking and bicycling facilities. The Level of Traffic Stress analysis calculates a higher stress level if there are gaps in a

low-stress facility. Improvements to high-stress routes are crucial to the active transportation network and will determine whether people view active transportation solely for recreation or very short trips, or whether they will walk or bicycle to jobs, shopping centers, schools, or other essential destinations.

Many of the streets in the walking and rolling network are large thoroughfares that provide important connections in Anne Arundel County, allowing people to travel between neighborhoods, to access daily needs, and to commute to work or school. These streets also typically have large rights-of-way, high speeds, and high traffic volumes. To complete the low-stress network, the County will need to improve the walking or rolling experience on these corridors by constructing shared-use paths or separated bicycle lanes and sidewalks.

In order to create an active transportation network within the confines of the existing right-of-way and make

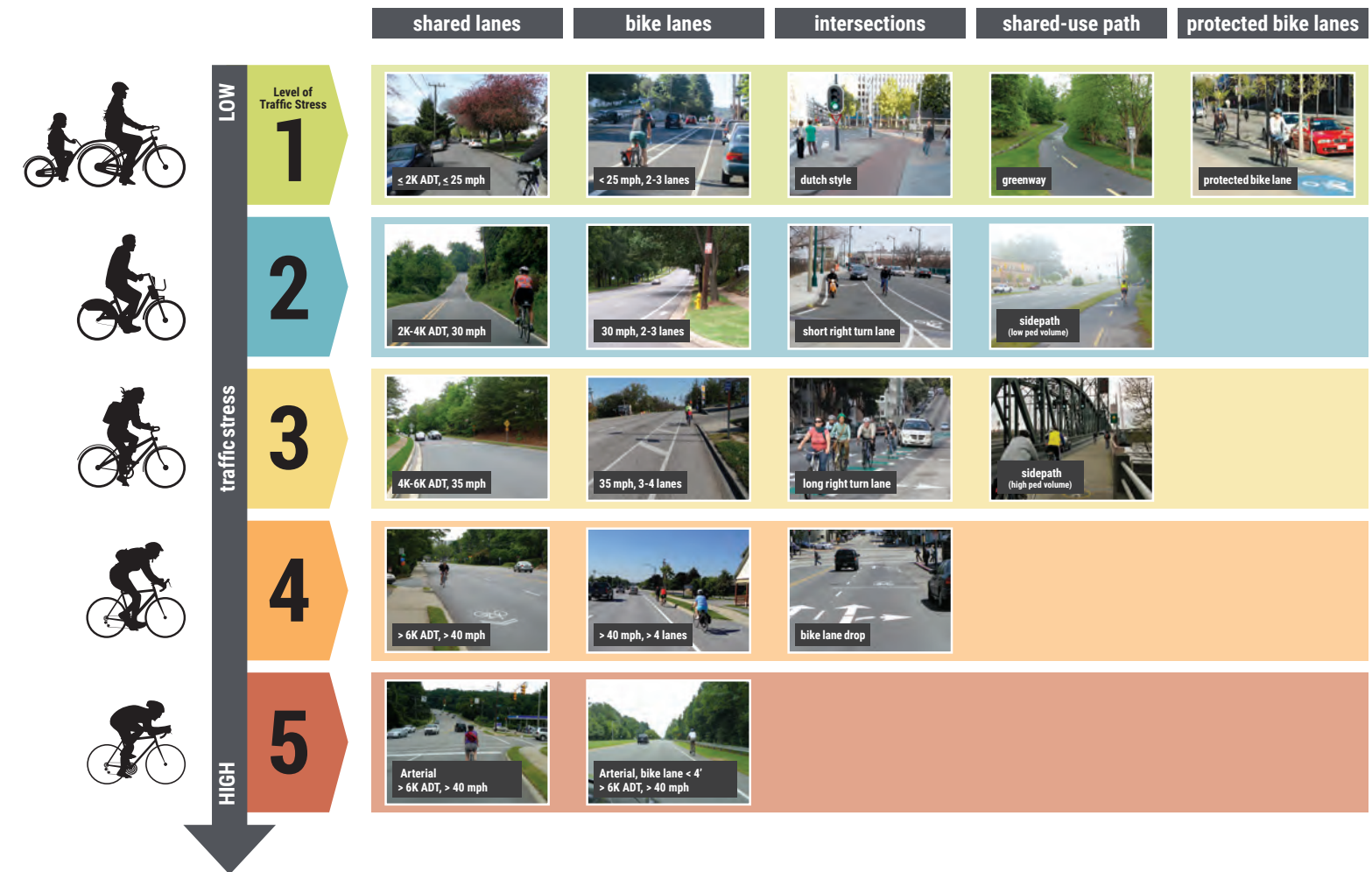


Figure 21: Levels of Traffic Stress are determined by these roadway characteristics and bicycle infrastructure facility types on those roadways.

roadways more efficient and safe for drivers, bicyclists, and pedestrians, corridors were evaluated for their potential to implement lane reconfigurations, or “road diets.” In some cases, the number of travel lanes can be reduced, and in others the width of travel lanes can be narrowed to a minimum of 10’, or 11’ on routes with buses or large truck traffic.

The Level of Traffic Stress analysis conducted as a part of this study is modified from the methodology first developed by Peter Furth in 2017 and from the methodology used by the Maryland Department of Transportation (MDOT).

The differences between MDOT’s methodology and the one employed in this study are described in Appendix J. Since more narrow neighborhood streets are generally more comfortable for walking and bicycling, with traffic usually traveling at slower speeds, the network does not include specific recommendations at these locations. The County should build upon its traffic calming program to provide resources for neighborhoods to install infrastructure meant to manage speeds.

Table 6: Low-stress network travelers and infrastructure components.

Low-Stress Networks are for all travelers, including:	Facilities in Low-Stress Networks include:
<ul style="list-style-type: none"> • Children • Older people • People of all genders • People from all backgrounds • People of all income levels • People with disabilities • Transit riders • Bikeshare riders • Confident bicyclists 	<ul style="list-style-type: none"> • Well-maintained sidewalks with buffers • Buffered and separated bicycle lanes • Shared-use paths • Trails • Bicycle boulevards • High-visibility and raised crosswalks • Median refuge islands • ADA-accessible curb ramps with detectable warning surfaces • ADA-accessible curb extensions • Audible pedestrian signals • Bicycle signals • Wayfinding • Designated parking sites for bikes and micromobility devices

Town Centers

A countywide sidewalk analysis was completed as part of the 2013 Pedestrian and Bicycle Master Plan. The approach to pedestrian infrastructure in this plan was to focus on areas of the county most in need of pedestrian improvements due to safety issues.

In the three Town Centers of Glen Burnie, Parole, and Odenton, *Walk & Roll Anne Arundel!* provides recommendations for locations to prioritize sidewalks, shared-use paths, separated bicycle lanes, standard bicycle lanes, and either improve existing crossings or add new infrastructure to make crossings safer for pedestrians and bicyclists. More detail on the three Town Centers is included later in this chapter. Appendix B displays the previous sidewalk recommendations made for both in the Town Centers and throughout the County.

The Town Centers have high numbers of people walking relative to other parts of the County. To this end, the project team specifically developed sidewalk and crossing recommendations in each of the three Town Centers to improve pedestrian safety and access. While walking is prevalent throughout the County, this Plan does not provide Countywide sidewalk or safe crossing recommendations at specific locations outside of the Town Centers. Pedestrians may use shared-use paths recommended throughout the County.

Note that many of the top scoring projects are in the County’s three Town Centers.

Bicycling Infrastructure Recommendations

Anne Arundel County has made progress constructing safe facilities that connect to essential destinations for people who travel via bicycle, wheelchair, scooter, and other self-powered travel on wheel. A wide variety of people were observed bicycling during fieldwork, from individuals traveling to work, to families bicycling to weekend breakfast, to people recreating. Despite the County’s progress, many people who bicycle do so in uncomfortable or unsafe conditions. Large portions of main roadways throughout the County do not have bicycle lanes, separated bicycle lanes, or shared-use paths.

To establish a low-stress bicycling network, the County should pursue construction of separated bicycle lanes, separated shared-use paths or sidepaths, and off-street trails. The selection of bicycle infrastructure type for *Walk & Roll Anne Arundel!* should be informed by the process of deciding what infrastructure will provide the safest and most comfortable experience for a bicyclist or other bikeway traveler of any age or ability. FHWA created the Bikeway Selection Guide to help transportation designers evaluate trade-offs and make those decisions.²⁰ It helps answer the question, “What type of bikeway should be chosen on this particular street or in this plan given real-world context, constraints, and opportunities?” The recommendations in *Walk & Roll Anne Arundel!* are consistent with the guide and the County should refer to this guide when designing bicycle facilities. The guide itself references existing resources from FHWA, AASHTO, NACTO, ITE, and others.

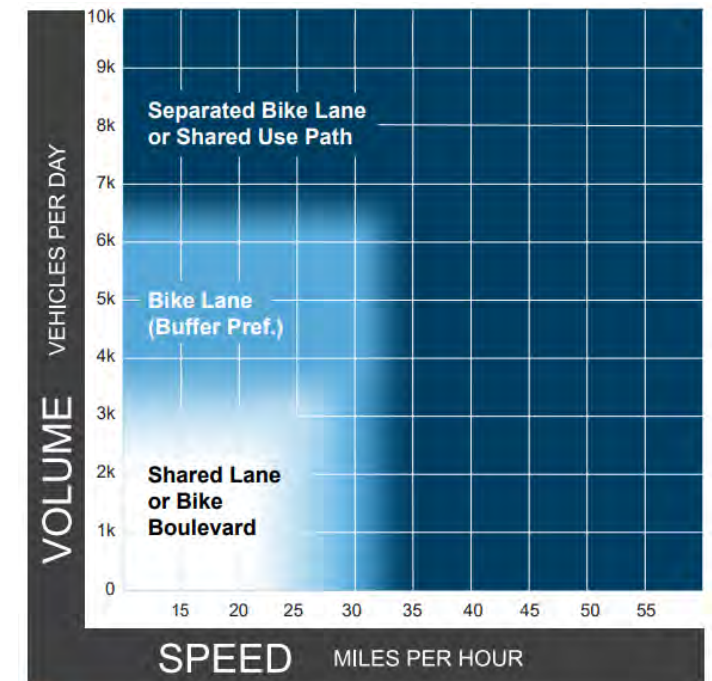
The guide focuses bikeway selection primarily on the traffic volume and design speed of a roadway within various land use contexts. Land use often influences the availability of right-of-way, travel behavior, property access, traffic speeds, roadway operations, safety, and goals of the community.

Detailed guidance to inform the County’s decisions to select specific bikeway types is available in the guide. Highlights of the guide are offered here.

Figure 22 demonstrates the preferred bikeway type for urban, urban core, suburban, and rural town contexts, again as informed by the average daily traffic and design speed for a given roadway.

Bicycle Facility Guidance on Rural Roadways

Figure 23 focuses on shoulder widths for bikeways in rural settings. 10’ shoulders should be provided on roadways with more than 8,000 vehicles per day traveling at speeds of 40 mph. Forthcoming updated guidance from AASHTO is likely to recommend 3’ buffers on rural roadways, which may be provided within the 10’ shoulder.



Notes
 1 Chart assumes operating speeds are similar to posted speeds. If they differ, use operating speed rather than posted speed.
 2 Advisory bike lanes may be an option where traffic volume is <3K ADT.
 3 See page 32 for a discussion of alternatives if the preferred bikeway type is not feasible.

Figure 22: Preferred Bikeway Type for Urban, Urban Core, Suburban, and Rural Town Contexts.

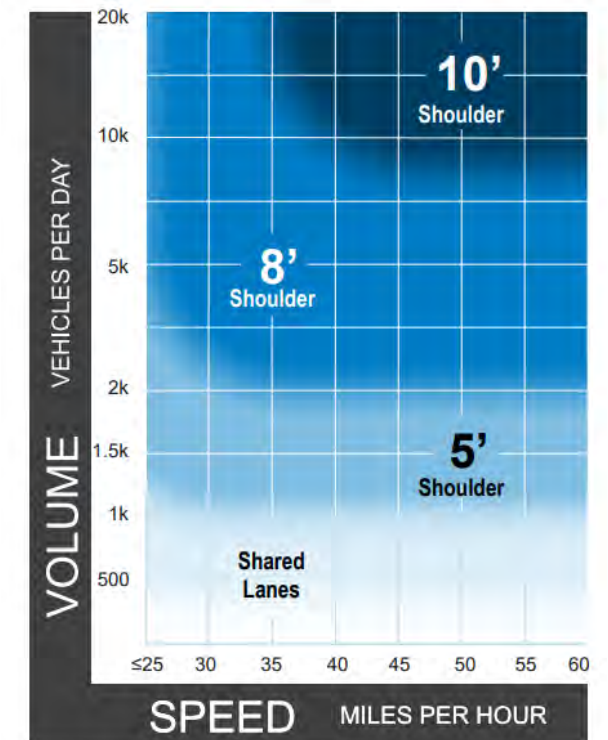


Figure 23: Preferred Shoulder Widths for Rural Roadways.

²⁰ Bikeway Selection Guide (dot.gov)



Figure 24: Trade-offs between standard bicycle lanes and buffered bicycle lanes on a 2-lane roadway.



Figure 25: Trade-offs between buffered bicycle lanes and separated bicycle lanes on a 5-6 lane roadway.

Bicycle Facility Guidance on 2-lane Roadways

For suburban and urban streets, the requirement for physical separation of bikeways depends on roadway characteristics. For example, standard bicycle lanes (without a buffer) may be sufficient for riders on roadways with relatively low design speeds and traffic volumes (where the level of traffic stress = 2). See Figure 24. Separated bicycle lanes with a buffer improve the travel experience for bicyclists and will likely encourage more “interested but concerned” bicyclists to ride.

Bicycle facility selection on 4-lane roadways

Standard bicycle lanes with widths of 6’ can be comfortable for confident bicyclists. However, interested but concerned bicyclists will not be as willing to travel on roadways with this type of facility. In order to provide a safe, inviting bicycling route for interested but concerned bicyclists, a 6’ bicycle lane with a buffer of 3’ (2’ minimum) should be provided. On roadways with infrequent driveways, vertical separation in the buffer should be provided.

Bicycle Facility Selection on 5-6 Lane Roadways

For 5-6 lane roadways, separated bicycle lanes are preferred to optimize comfort and increase ridership. See Figure 25. However, when constraints are present, such as limited ROW or prohibitive environmental terrain, buffered bicycle lanes may be provided.



Figure 26: A separated bicycle lane. Taken by Toole Design staff.

When to Choose a Shared-use path or Separated Bicycle Facility

For the average adult bicyclist, a separated bicycle lane, such as the one pictured in Figure 26, is preferred over a shared-use path in order to provide physical separation between pedestrians and typically faster-moving bicyclists. Shared-use paths may be used where space is constrained and the project is in land use contexts where both walking and/or bicycling volumes are relatively low, and expected to remain low. A SUP may be located on one or both sides of the street, depending on space availability, pedestrian and bicyclist connectivity needs, and volume of people walking or bicycling. Where density is expected to increase, right-of-way should be preserved to provide separated bicycling facilities adjacent to sidewalks.

Furthermore, the Bikeway Selection Guide promotes separated shared-use paths as options for 2-lane roadways, as well as 5-6 lane roadways. This physical

2-Lane Roadway (Shared Use Path Option)



5-6 Lane Roadway (Shared Use Path Option)



Figure 27: A Shared-use path may be used on any street. More protection becomes increasingly necessary with higher travel lanes, traffic speeds, and traffic volumes.

separation and protection becomes increasingly necessary as travel lanes increase; however, a SUP may be applied to any roadway.

FHWA provides a Shared-use path Level of Service Calculator that helps to calculate anticipated volumes of people using shared-use path. It is intended to demonstrate where a SUP is expected to provide enough space due to higher numbers of people walking or bicycling in an area. Separated facilities for bicyclists and pedestrians should be considered when SUP level of service is expected to be at or below a level “C” during peak hours; pedestrians are expected to comprise 30% or more of people using active transportation facilities during peak volumes; higher numbers of children, older adults, or individuals with disabilities are likely to be present; and where bicyclists need to travel at faster speeds.²¹

Pedestrian Safety Infrastructure Recommendations

It is imperative to provide safe, continuous paths for residents and visitors of Anne Arundel County. As crashes – including those that result in the death or serious injury of a person walking – continue to rise across the United States, it is increasingly important to provide safe pedestrian infrastructure and proactively work to reduce crashes.

The County should fully design, fund, and build infrastructure proven to reduce pedestrian crashes, fatalities, and serious injuries, following guidance from agencies like the Federal Highway Administration.

Effective Pedestrian Safety Infrastructure

Safe Transportation for Every Pedestrian:

FHWA provides guidance on how and where to construct seven specific types of infrastructure in order to reduce these crashes. This guidance is found in the Safe Transportation for Every Pedestrian (STEP) Program. STEP recommends the following seven infrastructure designs, all of which are proven to reduce pedestrian crashes:

1. Pedestrian Hybrid Beacons (PHB)

Pedestrian Hybrid Beacons (Figure 28) are effective designs that alert drivers to the presence of a person crossing a street at a midblock location. When activated, they require drivers to fully stop while a pedestrian is in the crosswalk. They are often paired with raised pedestrian islands, advance stop or yield signs, and high-visibility crosswalk markings. They are most effective on streets with a higher number of travel lanes, higher traffic speeds, and greater volumes of traffic. FHWA provides a chart to help agencies determine when to design a PHB versus a Rectangular Rapid Flashing Beacon depending on the roadway characteristics. Studies have shown that PHBs can reduce pedestrian crashes by 55%.²²

21 FHWA. Shared-use path Level of Service Calculator—A User’s Guide. FHWA-HRT-05-138. Federal Highway Administration, U.S. Department of Transportation, Washington DC, 2006.

22 https://safety.fhwa.dot.gov/ped_bike/step/resources/docs/fhwasa18064.pdf

2. Rectangular Rapid Flashing Beacons (RRFB)

Rectangular Rapid Flashing Beacons (Figure 29) are also pedestrian-activated signals that alert drivers to their presence in a crosswalk at a midblock location. Compared with PHBs, they are more appropriate on streets with fewer travel lanes, lower traffic speeds, and lower volumes of traffic. They are often paired with a pedestrian refuge island, advance stop or yield signs or markings, and crosswalk visibility enhancements. Pairing an RRFB with these features leads to a greater reduction in pedestrian crashes. FHWA provides a chart to help agencies determine when to design an RRFB versus a PHB depending on the roadway characteristics. Research shows that RRFBs can lead motorists to yield as high as 98% of the time and reduce pedestrian crashes by 47%.²³

3. Pedestrian refuge islands

As infrastructure that both physically protects pedestrians crossing a roadway and increases their visibility to drivers, pedestrian refuge islands are effective safety infrastructure. Located in the middle of a street (or between outer travel lanes on roadways with multiple lanes of travel), they allow pedestrians to focus on one direction of traffic at a time. They also provide refuge to pedestrians while they wait for traffic to clear. They may be paired with curb extensions, PHBs, RRFBs, LPs, and crosswalk visibility enhancements. Refuge islands can reduce pedestrian crashes by 32%.²⁴

4. Leading Pedestrian Intervals (LPI)

Leading Pedestrian Intervals gives pedestrians a 3- to 7-second lead time, typically, at signalized intersections. This reduces the likelihood of a vehicle hitting a pedestrian while turning right or left. They may be paired with right turn on red restrictions, accessible pedestrian signals, and parallel vehicular green extension intervals. LPIs can reduce pedestrian crashes by 13%.²⁵

5. Road diets/lane reconfigurations

Road diets, or lane reconfigurations, reduce the number of travel lanes on a street and add pedestrian and bicycle safety infrastructure, including pedestrian refuge islands, curb extensions, wider sidewalks and/or sidewalks with buffers, and bicycle infrastructure and/or transit lanes. Typically, four travel lanes are reduced to two travel

lanes and a center turn lane. FHWA provides a Road Diet Informational Guide to assist communities design lane reconfigurations. Road diets can reduce total crashes by 19% in urban areas and 47% in suburban areas.²⁶

6. Raised crosswalks

Raised crosswalks are ramped speed tables that rise to the height of the sidewalk and fully cover the width of a roadway. They are often used at midblock crossing locations. They make a pedestrian more visible to drivers. They may be paired with crosswalk visibility enhancements and approach ramps to reduce vehicle speeds and increase yielding rates by motorists. Raised crosswalks can reduce pedestrian crashes by 45%.²⁷



Figure 28: A Pedestrian Hybrid Beacon. Picture taken by Toole Design Staff.



Figure 29: A Rectangular Rapid Flashing Beacon. Picture taken by Toole Design Staff.



Figure 30: A pedestrian refuge island. Picture taken by Toole Design Staff.



Figure 31: A bulb out is a feature of a high-visibility crosswalk. Picture taken by Toole Design Staff.

7. High-visibility crosswalk enhancements

Improve crosswalks with treatments that make pedestrians more visible to drivers. Employ designs such as curb extensions, high-visibility and retroreflective crosswalk markings, pedestrian scale lighting, parking restrictions, and in-street signs alerting drivers to the presence of a crosswalk. These measures may reduce crashes by 23-48%.²⁸

To inform the selection of these seven countermeasures, FHWA provides Figure 32 to recommend designs according to roadway speed, volume, and configuration. Detailed information about each of these countermeasures is available on FHWA's website at Safe Transportation for Every Pedestrian (STEP - Safety | Federal Highway Administration (dot.gov)).



Figure 32: Pedestrians crossing midblock without a crosswalk on MD 648 in Severna Park. Source: Google StreetView.

Designing for a Safe, Comfortable Walking Experience

The County's Vision Zero goals include protecting vulnerable road users, including bicyclists and pedestrians, who bear an unproportional share of deaths and serious injuries occurring on our roads. In accordance with the Vision Zero goals, the County should design transportation facilities to provide a safe and comfortable walking experience. The design of these facilities should relate to the surrounding land use and overall context of the infrastructure project.

In order to provide this infrastructure and experience, the County should employ the following, in addition to the STEP countermeasures described previously and shown in Figure 33:

1. Design sidewalks with sufficient sidewalk widths. Widths should be 8-14' with 4-8' buffers in central business districts, 6-8' with 2-8' buffers in commercial areas, and 5-7' with 2-6' buffers in residential areas. In rural areas, the County should follow AASHTO's A Policy on Geometric Design of Highways and Streets for sidewalk width.
2. Provide and maintain high visibility crosswalks at all crossing locations, such as by striping longitudinal crosswalk markings instead of transverse crosswalk markings.
3. Provide frequent crosswalks throughout corridors. In general, crosswalks should be spaced at approximately the same spacing as the street grid in the area.
4. Reduce the time pedestrians must wait to cross with signals; provide enough time during the signal phase for a pedestrian to cross the street, including those with disabilities.

²³ https://safety.fhwa.dot.gov/ped_bike/step/docs/techSheet_RRFB_2018.pdf

²⁴ https://safety.fhwa.dot.gov/ped_bike/step/docs/techSheet_PedRefugeIsland2018.pdf

²⁵ https://safety.fhwa.dot.gov/ped_bike/step/resources/docs/fhwasa19040.pdf

²⁶ https://safety.fhwa.dot.gov/ped_bike/step/docs/techSheet_RoadDiet2018.pdf

²⁷ https://safety.fhwa.dot.gov/ped_bike/step/docs/techSheet_RaisedCW2018.pdf

²⁸ https://safety.fhwa.dot.gov/ped_bike/step/docs/techSheet_VizEnhancemt2018.pdf

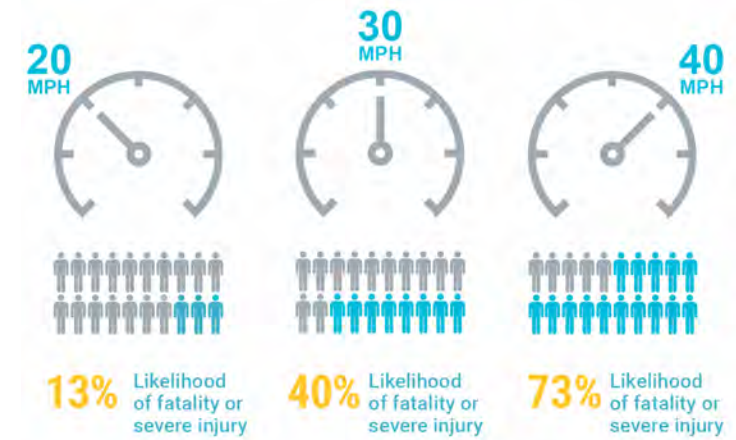
5. Shorten crossing distances with features like curb extensions, pedestrian refuge islands, the realignment of crosswalks at offset or diagonal intersections, the reduction of wide travel or parking lane widths, or the reduction of the number of vehicle lanes.
6. Restrict passive left turns and right turn on red.
7. Illuminate crossings with pedestrian-scale lighting.
8. Increase visibility of pedestrians by restricting parked vehicles, fences, buildings, hedges, and walls.
9. Design and construct smaller turning radii at intersections.
10. Reduce travel lane width to slow traffic speeds.
11. Incorporate traffic calming measures to slow the speed of traffic, including raised pedestrian crosswalks, chicanes, bulb-outs, street trees, and other elements.

Pedestrians are exponentially less likely to survive a crash as vehicle speeds increase (Figure 34); lowering the speed of moving vehicles is crucial to protecting the lives of people

who walk for transportation. It is therefore paramount to implement recommendations that slow traffic speeds, including the STEP recommendations, designing smaller turning radii, reducing travel lane widths, and incorporating traffic calming measures into roadway design.

While the County follows ADA requirements, walking and bicycling facilities should be upgraded to ensure that people with disabilities are able to easily and comfortably travel. The County should design all facilities to make it easy for people with disabilities to cross or move along the street, such as by designing parallel curb ramps instead of diagonal curb ramps, detectable warning surfaces, audible pedestrian signals, curb cuts, and sufficient space for people in a wheelchair to pass by others on a sidewalk or bicycle facility. For further guidance, refer to FHWA's Designing Sidewalks and Trails for Access: Best Practices Design Guide.

For more details, see FHWA's Achieving Multimodal Networks and the NACTO Urban Street Design Guide.



Source: Tefft, Brian C. *Impact speed and a pedestrian's risk of severe injury or death. Accident Analysis & Prevention. 50. 2013*

Figure 34: The County should install infrastructure that reduces vehicular speeds. A pedestrian's risk of death and serious injury exponentially decreases as vehicular speeds decrease.

Roadway Configuration	Posted Speed Limit and AADT								
	Vehicle AADT <9,000			Vehicle AADT 9,000–15,000			Vehicle AADT >15,000		
	≤30 mph	35 mph	≥40 mph	≤30 mph	35 mph	≥40 mph	≤30 mph	35 mph	≥40 mph
2 lanes (1 lane in each direction)	1 2 4 5 6	1 5 6 7 9	1 5 6 7 9	1 4 5 6 7 9	1 5 6 7 9	1 5 6 7 9	1 4 5 6 7 9	1 5 6 7 9	1 5 6 7 9
3 lanes with raised median (1 lane in each direction)	1 2 3 4 5	1 5 6 7 9	1 5 6 7 9	1 3 4 5	1 5 6 7 9	1 5 6 7 9	1 3 4 5	1 5 6 7 9	1 5 6 7 9
3 lanes w/o raised median (1 lane in each direction with a two-way left-turn lane)	1 2 3 4 5 6 7	1 5 6 7 9	1 5 6 7 9	1 3 4 5 6 7	1 5 6 7 9	1 5 6 7 9	1 3 4 5 6 7	1 5 6 7 9	1 5 6 7 9
4+ lanes with raised median (2 or more lanes in each direction)	1 3 7 8 9	1 5 6 7 8 9	1 5 6 7 8 9	1 3 7 8 9	1 5 6 7 8 9	1 5 6 7 8 9	1 3 7 8 9	1 5 6 7 8 9	1 5 6 7 8 9
4+ lanes w/o raised median (2 or more lanes in each direction)	1 3 7 8 9	1 5 6 7 8 9	1 5 6 7 8 9	1 3 7 8 9	1 5 6 7 8 9	1 5 6 7 8 9	1 3 7 8 9	1 5 6 7 8 9	1 5 6 7 8 9

Given the set of conditions in a cell,
 # Signifies that the countermeasure is a candidate treatment at a marked uncontrolled crossing location.
 ● Signifies that the countermeasure should always be considered, but not mandated or required, based upon engineering judgment at a marked uncontrolled crossing location.
 ○ Signifies that crosswalk visibility enhancements should always occur in conjunction with other identified countermeasures.*

The absence of a number signifies that the countermeasure is generally not an appropriate treatment, but exceptions may be considered following engineering judgment.

*It should be noted that the PHB and RRFB are not both installed at the same crossing location.

This table was developed using information from: Zegeer, C.V., J.R. Stewart, H.H. Huang, P.A. Lagerwey, J. Feaganes, and B.J. Campbell. (2005). *Safety effects of marked versus unmarked crosswalks at uncontrolled locations: Final report and recommended guidelines*. FHWA, No. FHWA-HRT-04-100, Washington, D.C.; FHWA, *Manual on Uniform Traffic Control Devices*, 2009 Edition, (revised 2012), Chapter 4F, Pedestrian Hybrid Beacons, FHWA, Washington, D.C.; FHWA, *Crash Modification Factors (CMF) Clearinghouse*, <http://www.cmfclearinghouse.org/>; FHWA, *Pedestrian Safety Guide and Countermeasure Selection System (PEDSAFE)*, <http://www.pedbikesafe.org/PEDSAFE/>; Zegeer, C., R. Srinivasan, B. Lorr, D. Carier, S. Smith, C. Sundstrom, N.J. Thirsk, J. Zegeer, C. Lyon, E. Ferguson, and R. Van Houten. (2017). *NCHRP Report 841: Development of Crash Modification Factors for Uncontrolled Pedestrian Crossing Treatments*. Transportation Research Board, Washington, D.C.; Thomas, Thirsk, and Zegeer. (2016). *NCHRP Synthesis 498: Application of Pedestrian Crossing Treatments for Streets and Highways*. Transportation Research Board, Washington, D.C.; and personal interviews with selected pedestrian safety practitioners.

Figure 33: Table produced by FHWA outlining roadway characteristics that should guide the placement of STEP pedestrian safety infrastructure.



Figure 35: An intersection lacking crosswalks in Odenton. Taken by Toole Design Group.

Implementation of Pedestrian Safety Infrastructure

The design and construction of safe pedestrian infrastructure will take significant effort and resources from the County and State. While this is not an exhaustive list of locations where pedestrian safety infrastructure should be prioritized throughout the County, it provides suggestions for implementation.

High pedestrian crash locations

The crash analysis conducted for *Walk & Roll Anne Arundel!* found that among areas with a high risk of pedestrian crashes, two are town centers (Glen Burnie and Parole) while others are busy commercial corridors with many destinations, such as Laurel-Fort Meade Road.

Areas with a high risk of bicycle crashes are more diffuse, including arterial and collector roads in more residential areas, such as Harmans Road in Hanover and Catherine Avenue in Pasadena. Aligning with historical crash data in *Move Anne Arundel!*, the top pedestrian crash corridors are primarily state roads like MD 3 (Crain Highway North) in Crofton, while the top bicyclist crash corridors are county-owned roads like Catherine Avenue in Pasadena or Harmans Road in Severn. One possible explanation is that state roads are home to a significant amount of commercial activity that attracts people on foot, while local roads that are more residential in character may be common locations for recreational bicyclists.

High risk pedestrian, bicyclist, and motorist crash corridors are documented in Tables 7-9. Intersections with the highest number of crashes are shown in Table 10.

Table 7: Top High Risk Pedestrian Crash Corridors

Corridor	Location
MD 648 (Baltimore-Annapolis Boulevard)	Glen Burnie Town Center
MD 3 (Crain Highway North)	Glen Burnie Town Center
MD 2 (Ritchie Highway)	Glen Burnie
Forest Drive (County)	Parole Town Center
MD 607 (Hog Neck Road)	Pasadena
MD 198 (Laurel-Fort Meade Road)	Laurel
MD 710 (East Ordnance Road)	Glen Burnie
MD 270 (East Furnace Branch Road)	Glen Burnie

Table 8: Top High Risk Bicyclist Crash Corridors

Corridor	Location
MD 177 (Mountain Road)	Pasadena
MD 214 (Central Avenue) (County)	Glen Burnie
Doris Avenue (County)	Brooklyn Park
Catherine Avenue (County)	Pasadena
Harmans Road (County)	Severn

Table 9: Top High Risk Motorist Crash Corridors

Corridor	Location
MD 3 (Crain Highway South)	Crofton
MD 175 (Annapolis Road)	Odenton Town Center
MD 3 (Crain Highway North)	Gambrills
MD 170 (Telegraph Road)	Severn
MD 424 (Davidsonville Road)	Crofton
MD 173 (Fort Smallwood Road)	Pasadena
MD 177 (Mountain Road)	Pasadena

Table 10: Anne Arundel County Intersections with high pedestrian crash rates.

Street	Cross Street
Quarterfield Road	Parke West Drive
Clark Road	Linda Place
Piney Orchard Parkway	Stream Valley Drive
Crain Highway South	Crainmont South
8th Avenue northwest	Grayburn Drive
Fort Smallwood Road	Hillcreek Road
Robert Crain Highway	Chapel Lake Drive
MD 198 (Laurel Fort Meade Rd)	Old Annapolis Road
MD 198 (Laurel Fort Meade Rd)	Russett Green West
MD 2 (Ritchie Highway)	4th Avenue
Robert Crain Highway	Johns Hopkins Road
Quarterfield Road	Landmark Drive
Baltimore Annapolis Road	Ferndale Road
Crain Highway South	MD 214 (Central Avenue)
MD 2 (Ritchie Highway)	MDOT MVA driveway (6617 Ritchie Hwy)
Padfield Boulevard	2nd Ave Southwest
MD 2 (Ritchie Highway)	Fitzallen Road
Aquahart Road	Somerset Drive



Figure 36: An unprotected crosswalk to Severna Park Middle School.
Source: Toole Design Group.

Pedestrian Safety Improvement Recommendations

The County should implement infrastructure proven to reduce pedestrian crashes at signalized intersections and midblock locations. For example, the County could prioritize the installation of safety infrastructure at the following high-crash locations:

- MD 3 (Crain Highway) and Chapel Point Road/Bel Alton Newton Road:
 - Pedestrian Hybrid Beacon
 - Pedestrian refuge islands
 - New crosswalks with visibility enhancements
- Aquahart Road and Somerset Drive
 - Rectangular Rapid Flashing Beacon
 - Pedestrian refuge island
 - New crosswalk with visibility enhancements
- Mitchells Chance Road/Stepneys Lane
 - Road Diet – reduce the number of travel lanes from four to two with a center turn lane and 6' bicycle lanes.

Network Recommendations: Anne Arundel County's Future Walking and Rolling Network

The project team proposed specific walking and bicycling infrastructure for the County's roadways. As depicted in Map 7, the initial network of proposed infrastructure was organized within 15 Priority Improvement Areas (Map 7). The PIAs reflect areas of the County where it is important to create continuous connections for people to travel to important destinations in their communities. Walking and rolling recommendations for each of the 15 PIAs are found in the appendix.

The following sections depict the network of walking and bicycling infrastructure recommendations. Green, purple, and blue dashed lines represent the "primary recommended network." Grey dashed lines represent the "secondary network." Public and private roadway and active transportation projects on the secondary network, while not called out specifically for infrastructure improvements in this plan, play an important role in providing access to the primary network and should always be considered for pedestrian and bicycling infrastructure projects using a context-sensitive approach with regard to land use. Within the secondary network, "Areas for Future Study" are identified in blue shade and represent areas to be the first analyzed to make connections to the primary network. In reference to this planning process, previous studies, and changing development to meet the public's demand for increased bicycling and pedestrian infrastructure, these areas were determined to be "areas for future study."

The following facility types and phases are depicted in the network maps:

Facility Types

- Shared-use Path: an 8' or wider path or trail that both bicyclists and pedestrians can use.
- Separated Bike Lane: on-road bicycle facility that is separated from motor vehicle lanes with a vertical element, such as barriers or flex posts.
- Bike Lane: on-road bicycle facility marked with striping.
- Low-Stress Connection: low volume and low speed roads that bicyclists can comfortably share with motor vehicles; may be marked with sharrows or signed.

- Areas for Future Study: neighborhoods in Anne Arundel County where more low-stress connections may be recommended with further study.
- Secondary Network: collector and arterial roads that may benefit from a bike lane or wider shoulder; appropriate for more confident bicyclists.
- Key Trail Intersections: trail crossings where improvements for safety and comfort are recommended.
- Sidewalk: concrete or asphalt path on the side of the road for pedestrians.
- Potential Multimodal Connection: proposed roads or paths that aim to reconnect dead end roads and restore the street grid in Town Center areas.

Phases

- Existing: Biking and walking infrastructure that is currently in place.
- Proposed: Biking and walking infrastructure that is unbuilt but being recommended in this plan.

Network Recommendations by Regional Planning Areas

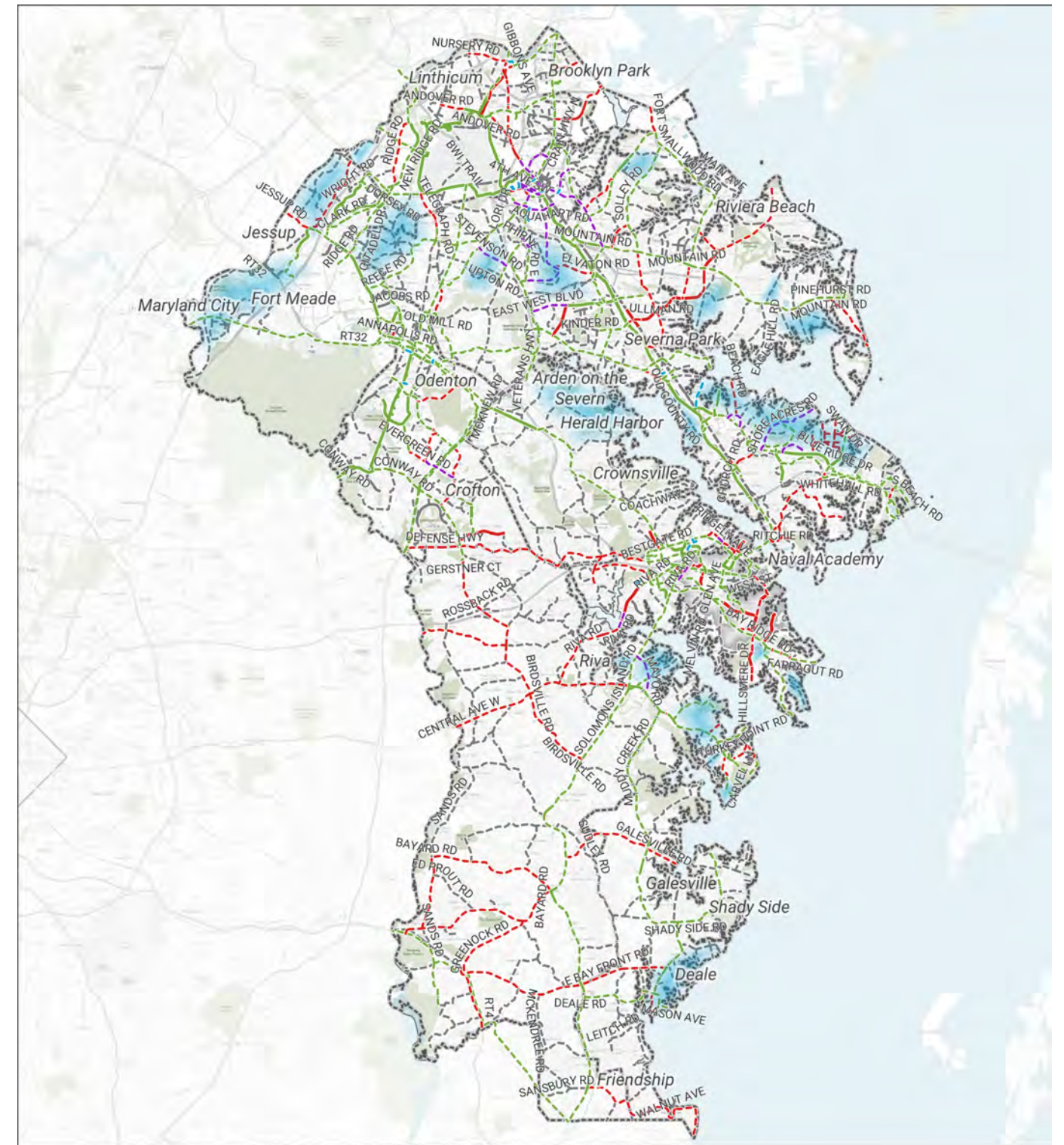
Plan2040 is the County's General Development Plan (GDP) that establishes goals, policies, and strategies for development throughout the County. To be consistent with *Plan2040*, *Walk & Roll Anne Arundel!* provides active transportation facility recommendations for each of the County's nine Regional Planning Areas as designated in *Plan2040*.

The bicycle infrastructure and shared-use path recommendations issued in *Walk & Roll Anne Arundel!* are presented for each Regional Planning Area (RPA). The following pages include a map of the proposed network and a table of the priority projects within the RPA. Several infrastructure projects that are critical to completing a low-stress network are described in a bulleted list. These projects also connect Anne Arundel residents to essential destinations.

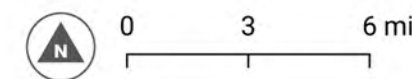
In all, *Walk & Roll Anne Arundel!* recommends about 23 miles of separated bicycle lanes, almost 100 miles of new bicycle lanes, and about 200 miles of shared-use paths. Table 11 shows the existing and proposed mileage of each facility type in the County.

Table 11: Existing, Programmed, and Proposed Facilities

	Existing	Programmed	Proposed	Total
Bike Lanes	26.9	4.0	130.4	161.3
Protected Bike Lanes	0.5	-	22.4	22.9
Shared-use Paths	55.2	10.9	248.5	314.6
Low Stress Connections	-	-	3.6	3.6



Anne Arundel County Walk & Roll Network

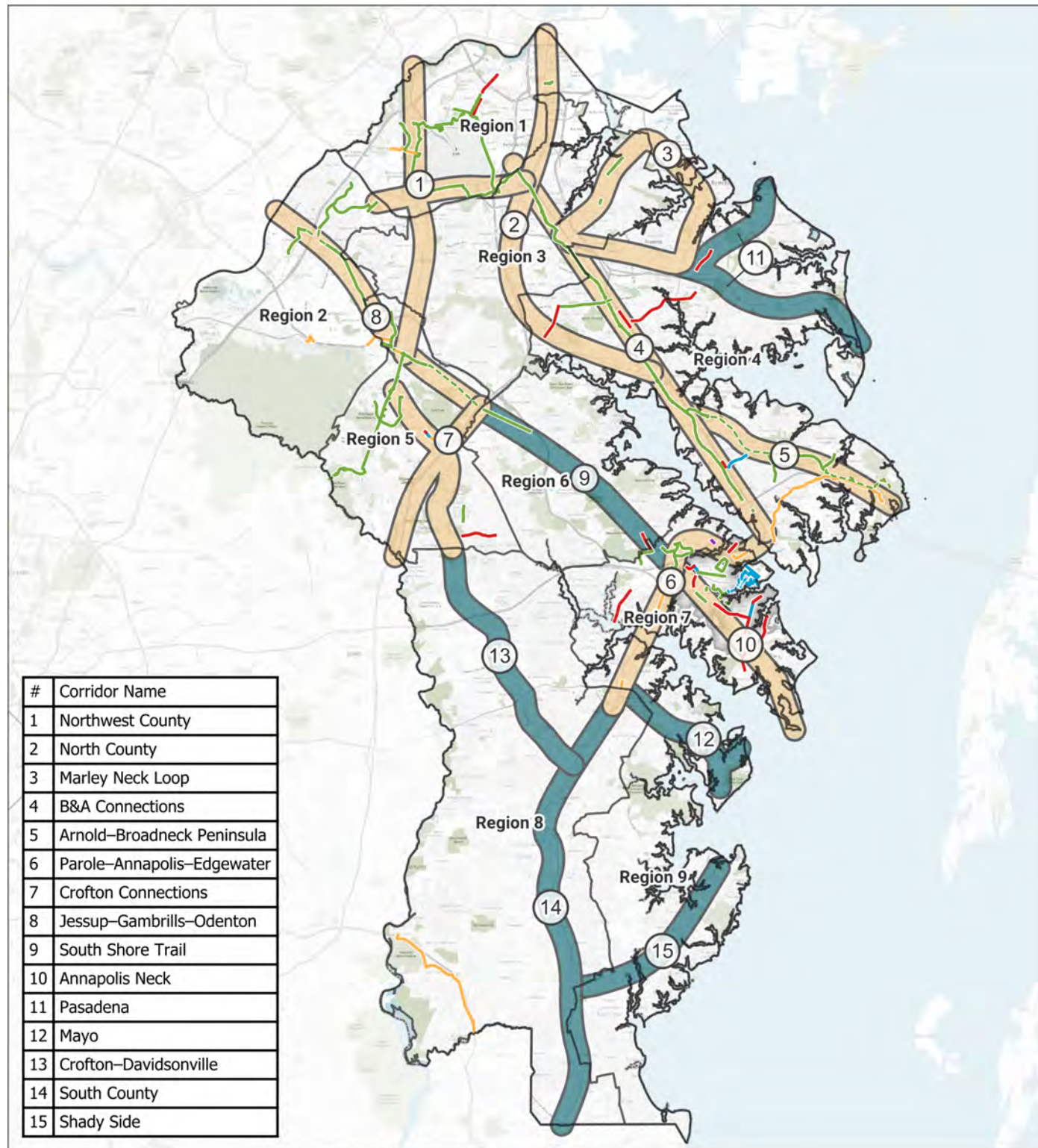


Facility Types

- Proposed**
- Shared-use Path
 - Separated Bike Lane
 - Bike Lane
 - Low Stress Connection
 - Secondary Network
- Existing**
- Shared-use Path
 - Separated Bike Lane
 - Bike Lane
- Area for Future Study



Map 6: Walk & Roll Anne Arundel! Countywide Walking and Bicycling Infrastructure Recommendations



Anne Arundel County
Priority Improvement Areas

- Proposed Corridors**
- Urban/Suburban
 - Rural/Peninsular
- Existing**
- Shared-use Path
 - Separated Bike Lane
 - Bike Lane
 - Sharrow
 - Signed Route
- Region Plan Areas**
- Region Plan Areas



Map 7: Priority Improvement Areas Studied for Walking and Bicycling Infrastructure Recommendations in *Walk & Roll Anne Arundel!*

Communities in RPA 1 include Brooklyn Park, Linthicum Heights, Ferndale, Hanover, Harmans, BWI Airport, and Curtis Bay.

Proposed projects that would significantly increase access to essential destinations for community members include:

- BWI Trail to Arundel Mills Trail Connector**

This project will connect the BWI Trail to the Arundel Mills Trail. A shared-use path is recommended to provide access on Stoney Run Road, New Ridge Road, Ridge Road, and Arundel Mills Boulevard. This connection will not only provide important access between existing trails, it will help people reach essential destinations, such as Arundel Mills Mall, a major shopping and jobs center. This project exists in both RPA 1 and 2.
- Baybrook Connector**

The Baybrook Connector will provide a separated walking and bicycling path for the neighborhoods of Cherry Hill, Brooklyn, Brooklyn Park, and Pumphrey. The SUP within Anne Arundel County is proposed on Belle Grove Road between Gibbons Avenue and the City of Baltimore line at the intersection with Potee Street, Hanover Street, and South Hanover Street. Travelers could reach the Nursery Road Light Rail Station from the western end of the project on signed residential streets. Local community members and planners are developing design for a bicycling facility to continue on into Baltimore from the eastern end of the project.
- MD 2 (Governor Ritchie Highway)**

MD 2 (Ritchie Highway) runs from the northern County line in Brooklyn Park to the Naval Academy Bridge in Annapolis. *Walk & Roll Anne Arundel!* proposes a shared-use path in RPA 1 from the Northern County line to where Route 2 meets MD 3 (Crain Highway) in Glen Burnie. A SUP would help people reach jobs (such as retail shopping centers along the corridor) and bus stops. Sidewalks exist on portions of this segment. ROW, utility relocation, and environmental challenges will exist on parts of the corridor. Significant constraints exist at the I-695 bridge and underpass at I-695 ramps, which necessitates appropriate phasing for segments of this project. This segment of Route 2 exists on the boundary of RPA 1 and 2.

The RPA 1 projects that scored the highest in this Plan’s prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
Ritchie Hwy	Shared-use Path	Northern County Line	MD 3 (Crain Hwy)	\$18,980,762	1.4
8th Ave NW	Separated Bike Lane	8th Ave NE, MD 3 (Crain Hwy N)	MD 648 (Baltimore Annapolis Blvd)	\$557,025	0.9
MD 648 (Baltimore Annapolis Blvd)	Separated Bike Lane	Nursery Rd	MD 177 (Mountain Rd)	\$3,619,476	7.4
Belle Grove Rd (Baybrook Connector)	Shared-use Path	Gibbons Ave	Pottee St, S Hanover St, Jack St Intersection	\$963,226	0.9
MD 713 (Ridge Rd) and New Ridge Rd	Shared-use Path	MD 175 (Annapolis Rd) & Ridge Rd	Stoney Run Rd & Ridge Rd	\$12,400,000	5
WB & A Rd	Shared-use Path	BWI Trail	Old Mill Rd	\$8,423,415	4.2
Ridge Rd	Shared-use Path	New Ridge Rd	Teague Rd	\$3,400,000	0.9
Church St	Bike Lane	Ritchie Hwy	County Line	\$439,499	0.9

*Projects are not in order of priority and need further study by the County

Regional Planning Area 1



Map 8: Walk & Roll Anne Arundel! Countywide Walking and Bicycling Infrastructure Recommendations.

Communities in RPA 2 include Jessup, parts of Hanover, Annapolis Junction, Fort Meade, parts of Laurel, and Maryland City.

Proposed projects that ranked highly in the Plan’s prioritization process include:

- Reece Road**

Walk & Roll Anne Arundel! proposes a SUP on Reece Road between Annapolis Road and where Donaldson Avenue and Telegraph Road meet.
- Annapolis Road**

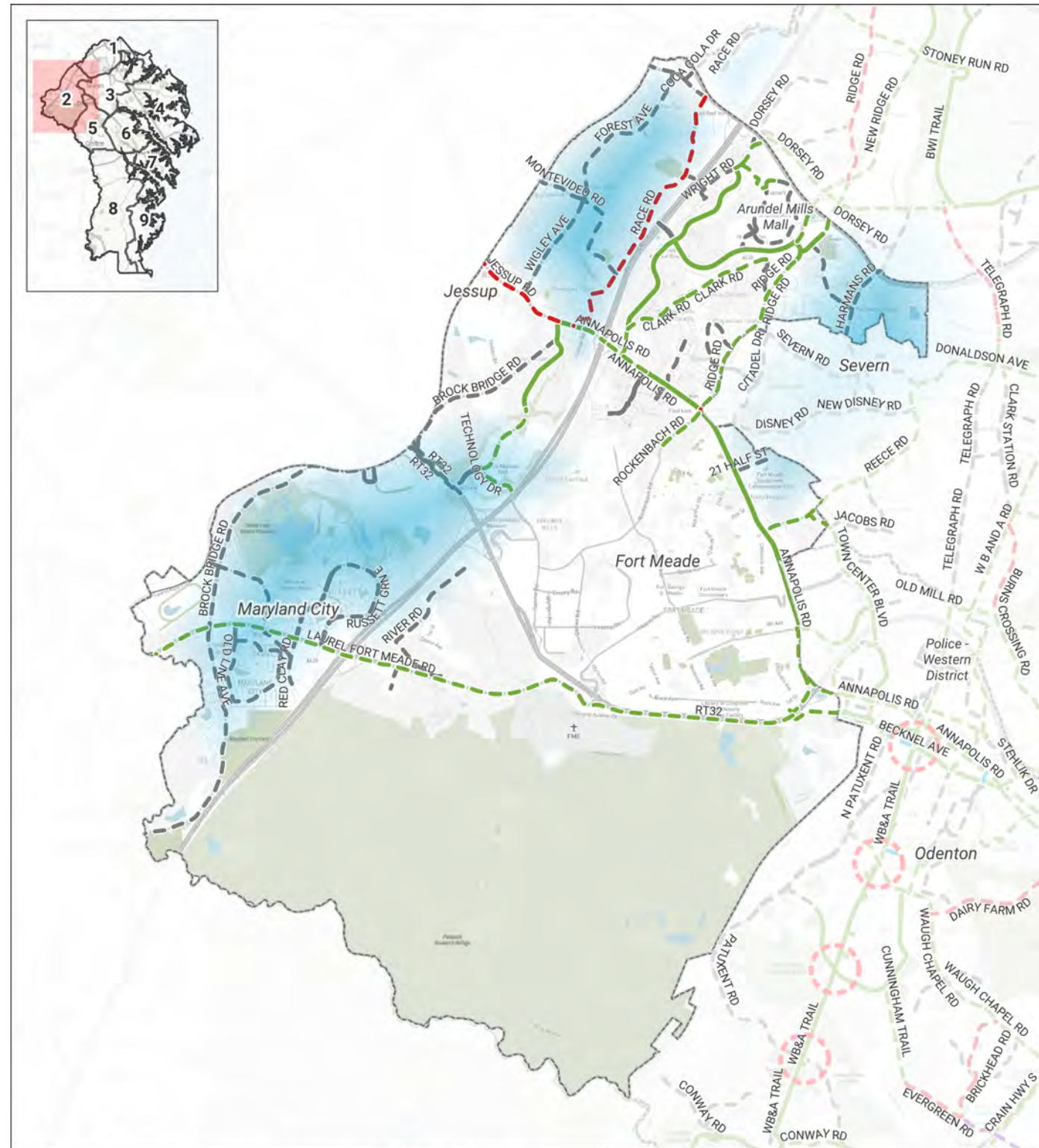
A proposed SUP on Annapolis Road between Baltimore Washington Parkway and south of Rae Road would connect to existing sidewalks that run (on mostly one side of the street) from south of Rae Road to Walker Drive. A SUP also exists on one side of Annapolis Road from Rockenbach Road to Blue Water Boulevard. A SUP on the northern end of Annapolis Road would help connect Jessup to Hanover, Severn, and Odenton, as well as Fort George G. Meade.
- Clark Road**

A facility on Clark Road would help connect community members in Jessup, the western part of Hanover, and Fort Meade to the commercial and residential areas of Hanover, including Arundel Mills Mall, grocery stores, and other important destinations.

The top prioritized projects in RPA 2 include:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
Reece Rd	Shared-use Path or separated bike lane	Donaldson Ave, Telegraph Rd	MD 175 (Annapolis Rd)	\$6,454,305	3.1
MD 175 (Annapolis Rd)	Shared-use Path	Baltimore Washington Pkwy	South of Rae Rd	\$911,625	0.4
Clark Rd	Shared-use Path	Milestone Pkwy	Watts Ave	\$3,008,363	1.5
National Business Pkwy	Shared-use Path	Technology Dr	North of Sentinel Way, at the existing SUP on National Business Pkwy	\$1,604,460	0.5
Ridge Rd	Shared-use Path	New Ridge Rd	Teague Rd	\$3,400,000	0.9
Jessup Rd	Bike Lane	Western County Line	Milestone Pkwy	\$1,256,878	1.6
MD 198 (Laurel Fort Meade Rd)	Shared-use Path	Western County Line	Mapes Road	\$11,167,406	4.6
MD 32 (Patuxent Fwy)	Shared-use Path	East of Mapes Road	MD 175 (Annapolis Rd)	\$6,381,375	2.4

*Projects are not in order of priority and need further study by the County



Anne Arundel Region 2 Proposed Bike Network

Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

Key Trail Intersections



Map 9: Walk and Roll Anne Arundel! Region 1 proposed bicycle network.

RPA 3 includes Glen Burnie, Severn, and Millersville.

Walking and rolling projects that will provide significant increases in access to destinations include:

- MD 648 (Baltimore Annapolis Boulevard)**
A separated bicycle lane is proposed on MD 648 (Baltimore Annapolis Boulevard) on a long segment, from Nursery Road to MD 177 (Mountain Road). Sufficient space exists within much of this project area; however, there will be constraints due to ROW and environmental issues, and potentially utility relocation needs. The project connects to jobs, transit, schools, and other important destinations.
- Aquahart Road and Fitzallen Road**
A separated bicycle lane is proposed on Aquahart Road and Fitzallen Road between MD 3 (Crain Highway) (which intersects with Aquahart Road) and Upton Road (intersects with Fitzallen Road). Sidewalks exist on this corridor and there will be ROW and some environmental constraints. However, the project will connect to residential areas, transit, the B&A Trail, Corkran Middle School, jobs, services (such as Anne Arundel County WIC), health care centers, retail centers (such as the Harundale Plaza Mall) the Glen Burnie Library, and more.
- MD 3 (Crain Highway) North and South**
A proposed SUP would provide walking and bicycling access to several jobs, educational, transit, retail centers, and more, from MD 3 (Crain Highway North) and MD 2 (Ritchie Highway) and MD 3 (Crain Highway South) and Veterans Highway.

The RPA 3 projects that scored the highest in this Plan’s prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
Ritchie Hwy	Shared-use Path	Northern County Line	MD 3 (Crain Hwy)	\$18,980,762	1.4
MD 648 (Baltimore Annapolis Blvd)	Separated Bike Lane	Nursery Rd	MD 177 (Mountain Rd)	\$3,619,476	7.4
Aquahart Rd and Fitzallen Rd	Separated Bike Lane	Aquahart Rd, MD 3 (Crain Hwy S), Thelma Ave	Fitzallen Rd, Upton Rd	\$1,084,245	1.1
MD 3 (Crain Hwy N & S)	Shared-use Path	MD 3 (Crain Hwy N) & Ritchie Hwy	MD 3 (Crain Hwy S) & Veterans Hwy	\$14,808,072	4.6
5th Ave SE	Shared-use Path	5th Ave SW, MD 3 (Crain Hwy S)	Ritchie Hwy	\$898,066	0.9
Quarterfield Rd	Shared-use Path	MD 3 (Crain Hwy S)	Old Stage Rd	\$1,203,345	0.6
8th Ave NW	Separated Bike Lane	8th Ave NE, MD 3 (Crain Hwy N)	MD 648 (Baltimore Annapolis Blvd)	\$557,025	0.9
Reece Rd	Shared-use Path	Donaldson Ave, Telegraph Rd	MD 175 (Annapolis Rd)	\$6,454,305	3.1
Griffith Ln	Separated Bike Lane and Bike Lane	Crain Highway	Greenway SE	\$495,994	0.4
Marley Neck Blvd	Shared-use path	MD 648 (Baltimore Annapolis Blvd), Marley Station Rd	Fort Smallwood Rd	\$9,025,088	4.5

*Projects are not in order of priority and need further study by the County

Regional Planning Area 3



Anne Arundel Region 3 Proposed Bike Network

Facility Types

Proposed

Shared-use Path

Separated Bike Lane

Bike Lane

Low Stress Connection

Secondary Network

Areas for Future Study

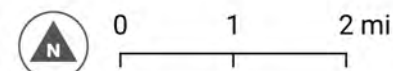
Key Trail Intersections

Existing

Shared-use Path

Separated Bike Lane

Bike Lane



Map 10: Walk and Roll Anne Arundel! Region 2 proposed bicycle network.

RPA 4 includes Pasadena, Severna Park, Arnold, Cape St. Clare, Broadneck.

Priority active transportation investments in RPA 4 include:

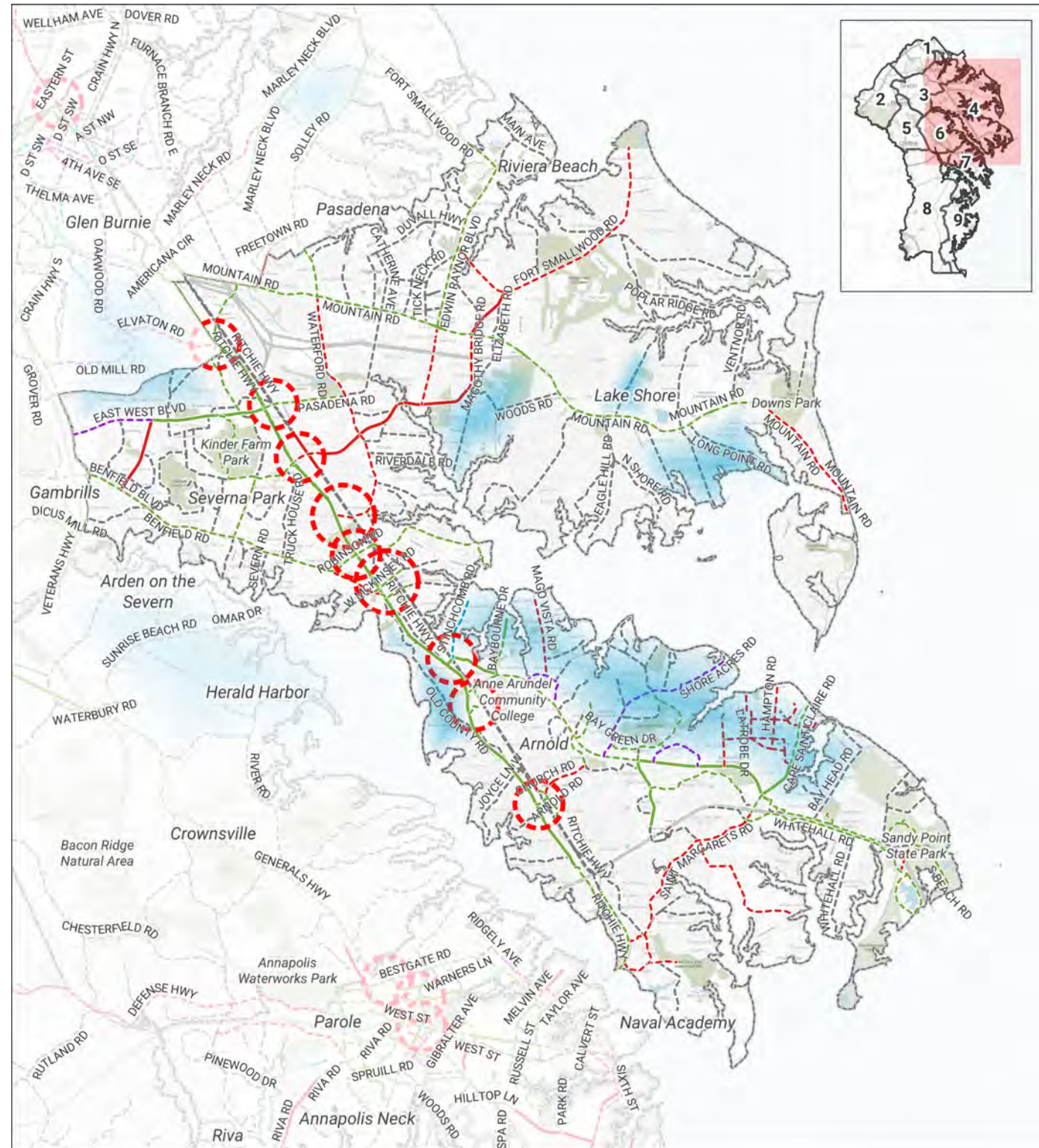
- MD 2 (Governor Ritchie Highway)**
 A proposed SUP or separated bicycle lane along the southern portion of MD 2 (Governor Ritchie Highway) would provide access from the southern trailhead of the B&A Trail to Annapolis, where Route 2 meets the Naval Academy Bridge.
- Jumpers Hole Road**
 SUP from MD 648 (Baltimore Annapolis Blvd)/MD 177 (Mountain Rd) to Benfield Road
- MD 177 (Mountain Road)**
 A proposed SUP from MD 2 (Ritchie Highway) to Pinehurst Rd.

The RPA 4 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
Route 2 (Ritchie Hwy)	Shared-use Path or separated bike lane	Near Boulters Way	Naval Academy Bridge	\$5,469,750	1.7
Jumpers Hole Rd	Shared-use Path	MD 648 (Baltimore Annapolis Blvd), MD 177 (Mountain Rd)	Benfield Rd	\$4,612,823	2.3
MD 177 (Mountain Rd)	Shared-use Path	Ritchie Hwy	Pinehurst Rd	\$7,612,069	3.7
Riggs Ave/McKinsey Rd	Shared-use Path	Riggs Ave & Lennox Ave	McKinsey Rd & Leelyn Dr	\$1,203,345	0.6
Robinson Rd	Shared-use Path	Benfield Rd	Asbury Dr & Leelyn Dr	\$1,403,903	0.7
MD 648 (Evergreen Rd)	Shared-use path	Evergreen Rd & Benfield Rd	Rt 2 Cyprus Creek	\$1,604,460	1.1
Bay Green Dr	Shared-use Path	Shore Acres Rd	College Pkwy	\$3,208,920	1.6
Benfield Rd	Shared-use Path	Veterans Hwy	Robinson Rd	\$4,612,823	2.3
College Pkwy	Shared-use Path	B&A Trail	Peninsula Farm Rd	\$2,279,063	0.5
Earleigh Heights Rd	Shared-use Path	Kinder Rd & Wembly Way	W Earleigh Heights Rd & Route 2	\$2,206,133	1.1
Fort Smallwood Rd	Shared-use Path	Edwin Raynor Blvd	Fort Smallwood Park	\$1,417,263	4.2

*Projects are not in order of priority and need further study by the County

Regional Planning Area 4



Anne Arundel Region 4 Proposed Bike Network

Facility Types

Proposed

Shared-use Path

Separated Bike Lane

Bike Lane

Low Stress Connection

Secondary Network

Areas for Future Study

Key Trail Intersections

Existing

Shared-use Path

Separated Bike Lane

Bike Lane

Map 11: Walk and Roll Anne Arundel Region 3 proposed bicycle network.

RPA 5 includes Odenton, Piney Orchard, Woodwardville, Gambrills, Two Rivers, and Crofton.

Projects that would provide significant improvements for people traveling on foot, bicycle, and wheelchair include:

• MD 3 (Crain Highway South)

A proposed SUP on MD 3 (Crain Highway) between North of Millersville Road to the Western County Line would provide walking and bicycling connections for communities in Crofton, Millersville, and Gambrills. It is recommended that a SUP exists on both sides of MD 3 (Crain Highway S) where the dual carriageway exists.

• MD 175 (Annapolis Road)

A SUP on Annapolis Road in Odenton, between Telegraph Road and Crain Highway, would connect people to Odenton Town Center – and its many jobs, schools, shopping, restaurant, residential, and transit centers – to Gambrills and Millersville. The SUP would also be in short proximity to the WB&A Trailhead in Odenton.

• Telegraph Road SUP between Old Mill Road and MD 175 (Annapolis Road)

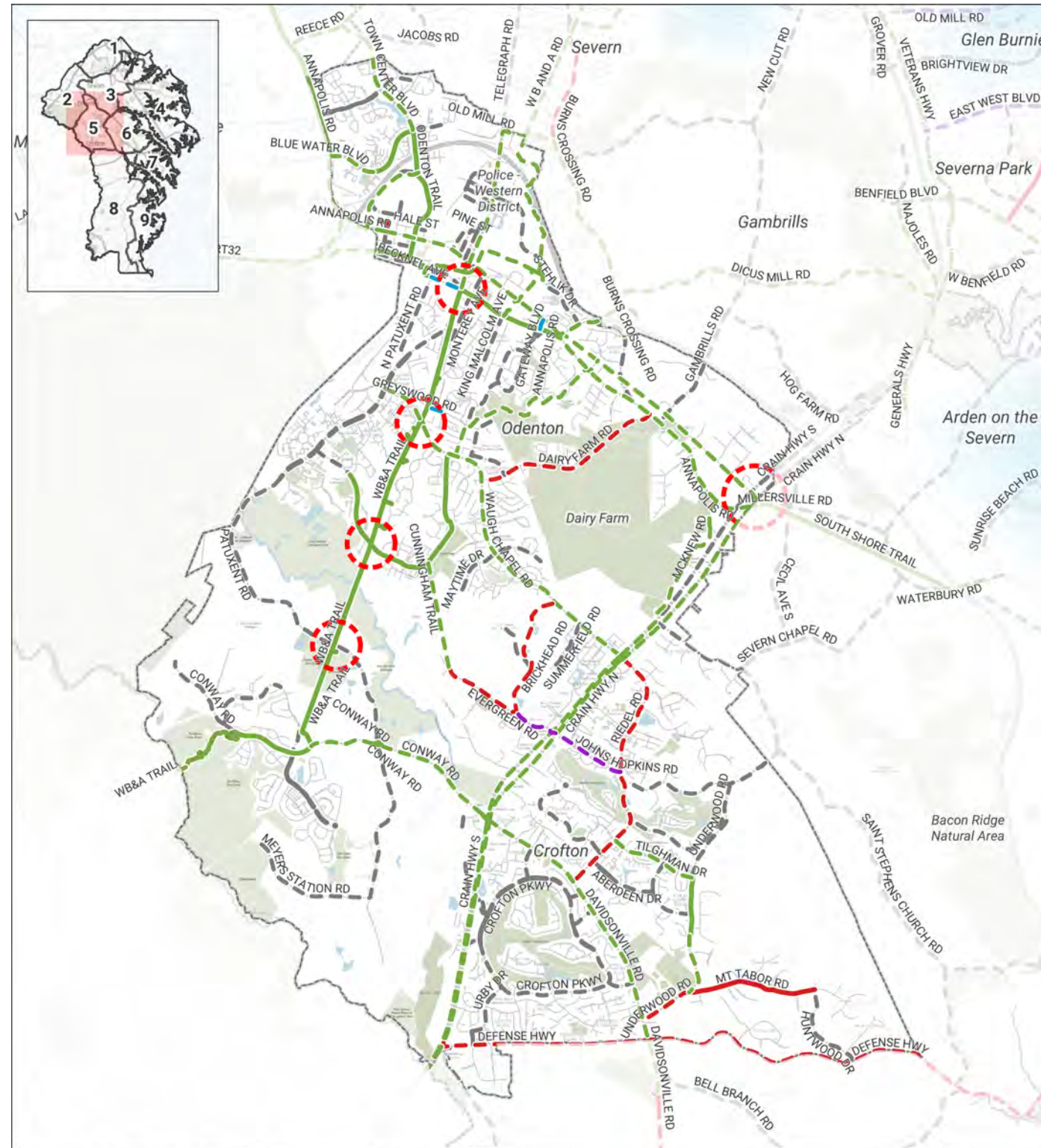
A SUP on Telegraph Road between Old Mill Road and MD 175 (Annapolis Road) in Odenton would be part of a larger connection between existing and proposed SUPs in Odenton Town Center and proposed SUPs extending from the BWI Trail and Severn.

The RPA 5 projects that scored the highest in this Plan’s prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
MD 3 (Crain Hwy S)	Shared-use Path	North of Millersville Rd	Western County Line	\$34,782,505	5.6
MD 175 (Annapolis Rd)	Shared-use Path	Telegraph Rd	MD 3 (Crain Hwy)	\$2,607,248	1.3
Telegraph Rd	Shared-use Path	Old Mill Rd	MD 175 (MD 175 (Annapolis Rd))	\$8,159,044	1.1
Waugh Chapel Rd	Shared-use Path	Crain Highway S	Maytime Dr	\$2,807,805	1.4
Davidsonville Rd	Shared-use Path	Conway Rd, MD 3 (Crain Hwy)	Barbara Swann Way	\$5,734,121	2.8
MD 450 (Defense Hwy)	Bike Lane	MD 3 (Crain Hwy)	N River Rd	\$2,057,473	8.8
Davidsonville Rd/Birdsville Rd	Bike Lane	MD 2 (Solomons Island Rd)	MD 450 (Defense Hwy)	\$3,153,211	12.1
Evergreen Rd/Johns Hopkins Rd	Separated Bicycle Lane	Waugh Chapel Way	Riedel Rd	\$576,148	1.3
Riedel Rd	Bike Lane	MD 3 (Crain Hwy)	Davidsonville Rd	\$1,818,470	2.3

*Projects are not in order of priority and need further study by the County

Regional Planning Area 5



Anne Arundel Region 5 Proposed Bike Network

Facility Types

Proposed

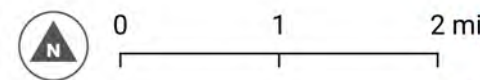
- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Other Symbols:

- Areas for Future Study
- Key Trail Intersections



Map 12: Walk and Roll Anne Arundel! Region 4 proposed bicycle network.

RPA 6 consists of Crownsville and parts of Millersville and is mostly rural.

Priority projects for RPA 6 include:

- MD 450 (Defense Highway)**
 A bicycle lane on MD 450 (Defense Highway) from MD 3 (Crain Highway) to N River Road would connect destinations in Crofton (including jobs, shops, entertainment, restaurants, and more) to the western edge of Parole. It would be relatively close to schools and parks as well.
- South Shore Trail**
 The South Shore Trail is an existing, multi-phased project that is a priority for the County. Once complete, the trail would connect Odenton to Annapolis and the communities in between. The South Shore Trail follows the abandoned roadbed of the WB&A Railroad between Annapolis and Odenton.

Once complete, it will provide about 14 miles of paved pathway for people on foot, bicycle, and wheelchair. It will connect with the WB&A Trail in Odenton on the western end, the Colonial Annapolis Trail in Annapolis on the eastern end, and the B&A Trail.

One priority connection along the South Shore Trail includes a shared-use path along Housley Road and Honeysuckle Lane to connect to Waterworks Park.

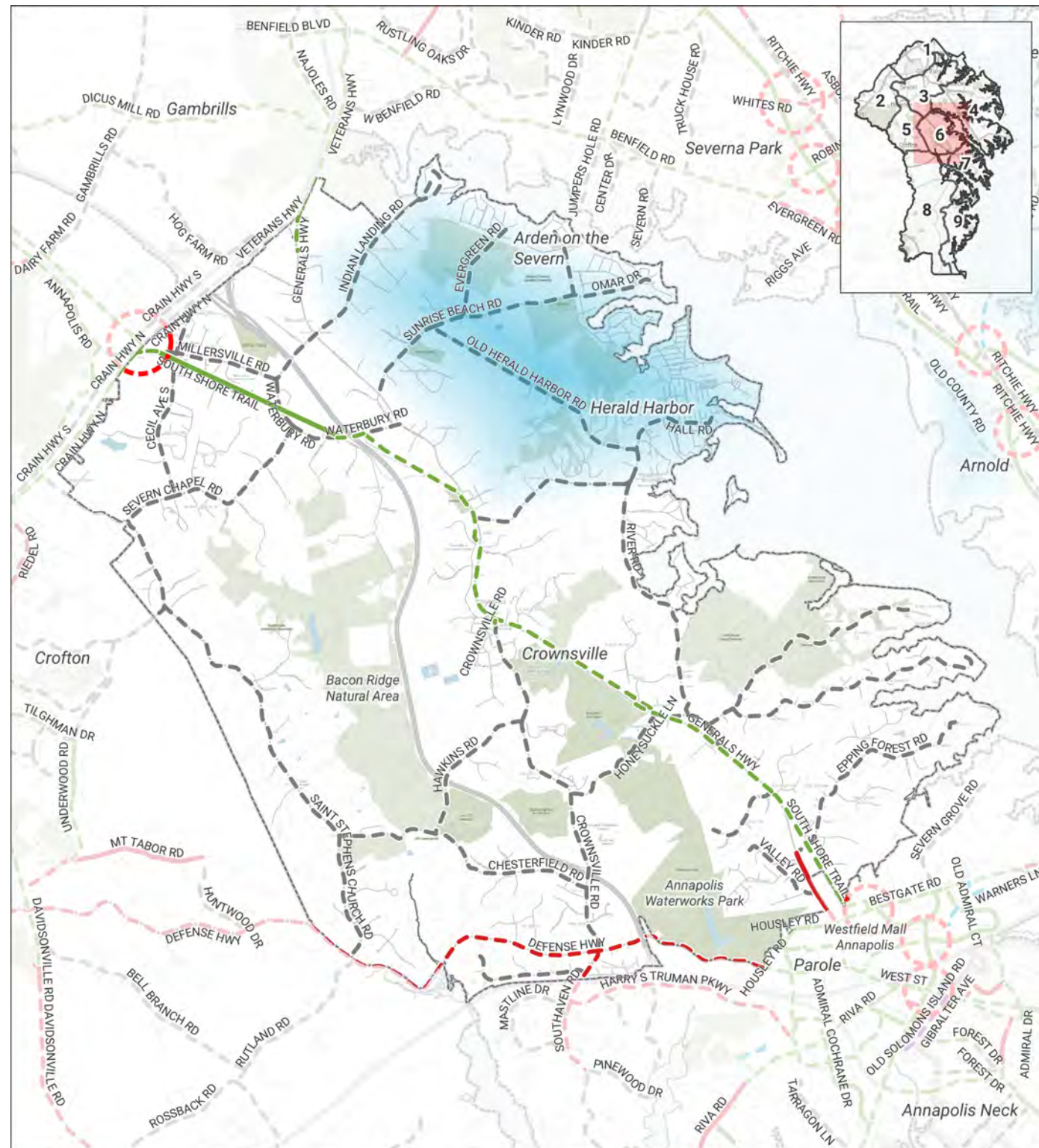
The constructed portion of the South Shore Trail begins along Route 175/Millersville Road in Millersville, close to MD 3 (Crain Highway), and ends at Waterbury Road. Another completed portion runs between Anne Arundel Medical Center and Annapolis Plaza in Annapolis. The trail passes through mainly rural/estate areas, but the connections to Odenton and Parole Town Centers create potential for a future key commuter route. In addition to the Town Centers, Millersville Elementary School and Rolling Knolls Elementary School are located within RPA 6, providing the opportunity to create safe connections to the schools. The main concern is confirming the route of the trail and identifying opportunities to improve wayfinding and crossings.

The RPA 6 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
Generals Hwy	Shared-use Path	Belvoir Farms Rd	Carriage Hill Pkwy	\$2,607,248	1.3
MD 450 (Defense Hwy)	Bike Lane	MD 3 (Crain Hwy)	N River Rd	\$2,057,473	8.8
South Shore Trail	Shared-use Path	Waterbury Rd	Annapolis	\$31,465,000	11.6
S Haven Rd	Bike Lane	MD 450 (Defense Hwy)	Pinewood Rd	\$941,509	1.1
Millersville Rd	Shared-use Path	MD 3 (Crain Hwy)	South Shore Trail	\$401,115	0.2
Crownsville Rd	Shared-use Path	MD 178 (Generals Hwy)	Marbury Dr	\$200,558	0.1
MD 178 (Generals Hwy)	Shared-use Path	Carriage Hill Pkwy	Bestgate Rd	\$2,734,875	1.2

*Projects are not in order of priority and need further study by the County

Regional Planning Area 6



Anne Arundel Region 6 Proposed Bike Network

Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Other Symbols

- Areas for Future Study
- Key Trail Intersections

0 0.9 1.8 mi

WALK & ROLL ANNE ARUNDEL!

Map 13: Walk and Roll Anne Arundel! Region 5 proposed bicycle network.

Parole and the Annapolis Neck reside in RPA 7, as does the City of Annapolis, which, while recognized as containing connections to the County as is shown on the map, was not studied in this plan. The City has its own Bicycle Master Plan.”

Projects that would provide significant improvements for people traveling on foot, bicycle, and wheelchair include:

- West Street SUP**

A SUP on West Street between Generals Highway and Holly Avenue would provide a critical connection in Parole between several shopping, jobs, health care, residential areas, and more.
- Bestgate Road SUP**

A SUP on Bestgate Road between Generals Highway and Bestgate Road would serve people needing to get to jobs, transit, health care centers, and more. Of particular importance are the Westfield Annapolis Mall and a future transit center. The northern side of the road has minimal ROW constraints. This Plan recommends a 12’ SUP with a 2’ buffer on the northern side of Bestgate Road and an upgraded 6’ sidewalk on the southern side. An existing SUP does exist along a portion of this area.
- Admiral Cochrane Drive**

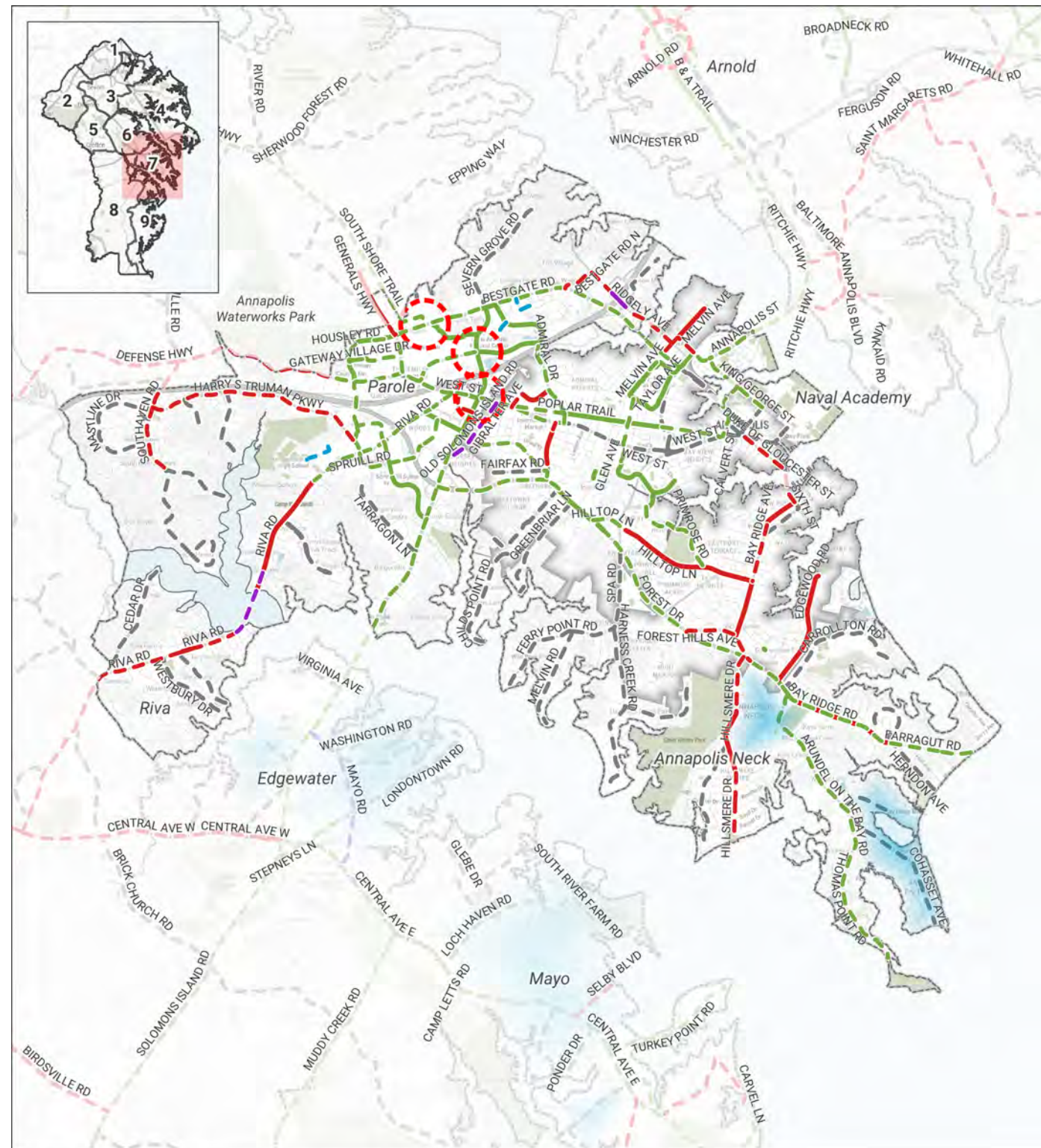
A separated bike lane on Admiral Cochrane Drive between Riva Road and the intersection with Lake Shore Drive and MD 2 (Solomons Island Road) would connect with proposed bicycle and pedestrian facilities, job centers (including several hotels), transit, health care centers, restaurants, services, and more destinations.

The RPA 7 projects that scored the highest in this Plan’s prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
West St	Shared-use Path	Generals Hwy	Holly Ave	\$1,203,345	0.6
Bestgate Rd	Shared-use Path	Generals Hwy	Bestgate Rd	\$1,203,345	0.6
MD 2 (Solomons Island Rd)	Shared-use Path	MD 214 (Central Ave)	MD 260	\$37,148,719	16.3
Admiral Cochrane Dr	Separated Bike Lane	Riva Rd	Lake Shore Dr, MD 2 (Solomons Island Rd)	\$797,478	0.9
Housley Rd	Shared-use Path	Annapolis Ridge Rd including the traffic circle	Generals Hwy with gaps in between	\$1,002,788	0.5
Bay Ridge Rd	Shared-use Path	Bay Ridge Ave, Forest Dr, Hillsmere Dr	Catrina Ln, Farragut Rd	\$3,418,594	1.5
Rowe Blvd	Shared-use Path	N Bestgate Rd	Kirkley Rd	\$1,823,250	0.7
Spruill Rd & Shopping Center Drive	Shared-use Path	Riva Rd	Forest Dr	\$3,500,000	0.6

*Projects are not in order of priority and need further study by the County

Regional Planning Area 7



Anne Arundel Region 7
Proposed Bike Network

Facility Types

Proposed

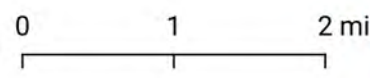
- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Areas for Future Study

- Key Trail Intersections

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane



Map 14: Walk and Roll Anne Arundel! Region 6 proposed bicycle network.

RPA 8 consists of South County, with communities of Davidsonville, Edgewater, Harwood, Lothian, Owensville, Waysons Corner, Friendship, and parts of Owings.

Projects that would provide significant improvements for people traveling on foot, bicycle, and wheelchair include:

• **MD 2 (Solomons Island Road)**

A SUP on MD 2 (Solomons Island Road) between MD 214 (Central Ave) and the Southern County Line would provide extensive access for bicyclists and pedestrians from Edgewater to South County communities of Harwood, Lothian, Tracy's Landing, and Friendship. Several schools and a few jobs, shopping, and recreation centers would be accessible on this route.

• **Davidsonville Road**

A SUP on Davidsonville Road between MD 3 (Crain Highway) and Barbara Swann Way would serve residential areas, connecting them with schools, shopping centers, restaurants, attractions, jobs, and other destinations along this corridor.

• **MD 450 (Defense Highway) Bike Lane between MD 3 (Crain Highway) and N River Road**

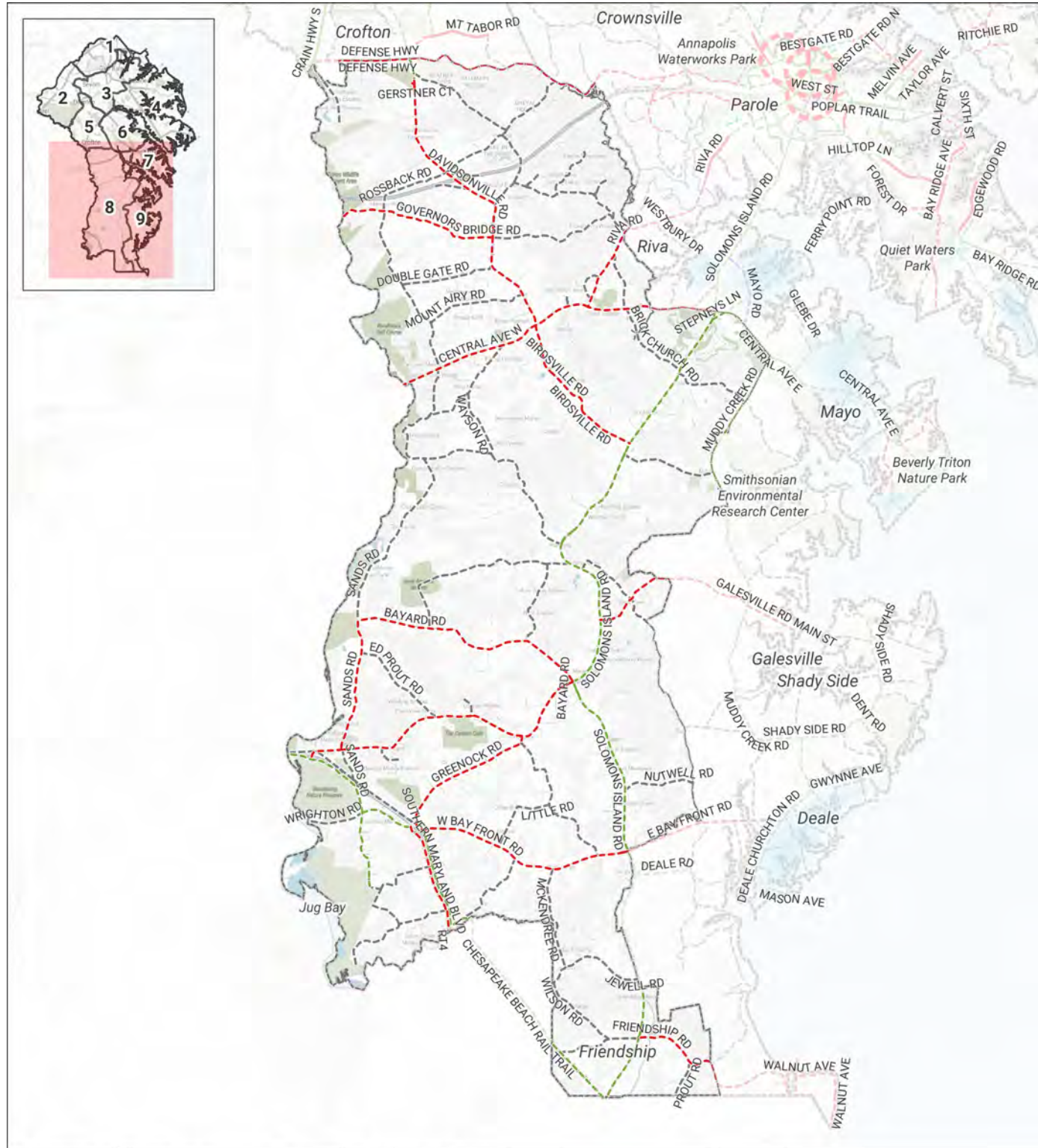
A bicycle lane on MD 450 (Defense Highway) between MD 3 (Crain Highway) and N River Road would provide access between mostly rural communities in Crofton, Gambrills, and parts of Anne Arundel County west of Parole.

The RPA 8 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
MD 2 (Solomons Island Rd)	Shared-use Path	MD 214 (Central Ave)	County Line	\$34,247,928	3.2
Davidsonville Rd	Shared-use Path	MD 3 (Crain Hwy)	Barbara Swann Way	\$5,615,610	4
MD 450 (Defense Hwy)	Bike Lane	MD 3 (Crain Hwy)	Houseley Rd	\$2,057,473	8.8
Stepneys Ln	Shared-use Path	MD 2 (Solomons Island Rd)	Mayo Rd	\$1,604,461	0.8
MD 214 (Central Ave E)	Shared-use Path	Mayo Rd	Rolling Rd	\$6,153,469	2.7
Mt Zion Marlboro Rd	Shared-use Path	Main St	MD 2 (Solomons Island Rd)	\$12,626,006	5.5
Birdsville Rd	Bike Lane	MD 214 (Central Ave)	MD 2 (Solomons Island Rd)	\$2,185,028	2.9
W Bay Front Rd	Bike Lane	MD 2 (Solomons Island Rd)	Southern Maryland Blvd/ Greenock Blvd	\$1,320,687	3.9

*Projects are not in order of priority and need further study by the County

Regional Planning Area 8



Anne Arundel Region 8
Proposed Bike Network

Facility Types

Proposed

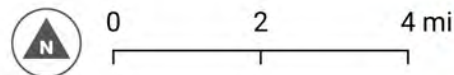
- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Other Symbols:

- Areas for Future Study
- ⊙ Key Trail Intersections



Map 15: Walk and Roll Anne Arundel! Region 7 proposed bicycle network.

RPA 9, on the southeastern shore of the County, is home to parts of Edgewater, Mayo, Galesville, West River, Shady Side, Churchton, Deale, Tracys Landing, and North Beach.

Priority walking and rolling projects in this RPA include:

- **MD 2 (Solomons Island Road)**

A SUP on MD 2 (Solomons Island Road) between MD 214 (Central Avenue) and the intersection with Admiral Cochrane Drive and Lake Shore Road would provide a walking and bicycling connection between Parole, Edgewater, and Harwood.
- **Mayo Road**

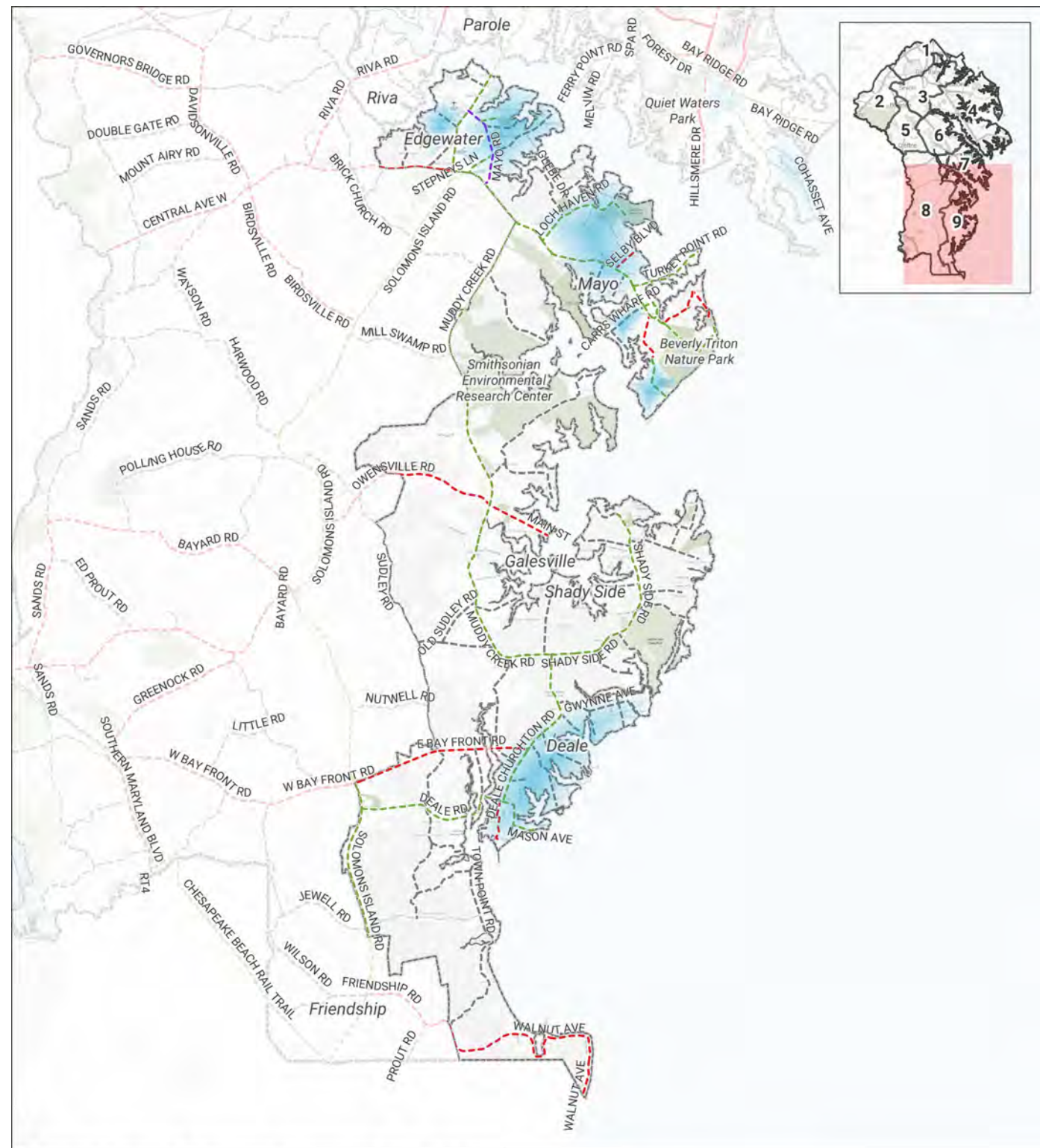
A separated bicycle lane on Mayo Road between MD 2 (Solomons Island Road) and MD 214 (Central Avenue East) would serve the center of Edgewater and Londontowne and its many shops, restaurants, residences, and more.
- **Stepneys Lane SUP between MD 214 (Central Avenue) and Mayo Road**

A SUP on Stepneys Lane between MD 214 (Central Avenue) and Mayo Road would serve numerous job centers, restaurants, a school, a library, a popular recreational walkway, and more. It would also directly connect to the Mayo Road proposed separated bicycle lane and be in close proximity to the proposed MD 2 (Solomons Island Road) SUP.

The RPA 9 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost	Length (mi)
MD 2 (Solomons Island Rd)	Shared-use Path	Admiral Cochrane Dr/Lake Shore Rd	MD 214 (Central Ave)	\$34,247,928	3.2
Mayo Rd	Separated Bicycle Lane	MD 2 (Solomons Island Rd)	Central Ave	\$534,856	1.3
Stepneys Ln	Shared-use Path	Central Ave	Mayo Rd	\$1,002,788	0.6
Central Ave E	Shared-use Path	Mayo Rd	Rolling Rd	\$6,153,469	2.7
Shady Side Rd	Shared-use Path	Deale Churchton Rd	Hayes Rd	\$8,204,625	3.6
W Bay Front Rd	Bike Lane	MD 2 (Solomons Island Rd)	Southern Maryland Blvd/ Greenock Blvd	\$1,320,687	4.0
Deale Rd	Shared-use Path	Deale Churchton Rd	MD 2 (Solomons Island Rd)	\$16,251,721	2.6

*Projects are not in order of priority and need further study by the County



Map 16: Walk and Roll Anne Arundel! Region 8 proposed bicycle network.

Odenton Town Center

As mentioned earlier, this update did not do a countywide sidewalk network map or gap analysis, except within the Town Centers. The following maps represent proposed pedestrian network improvements (Map 17) and proposed bicycle network improvements (Map 18) separately and are derived from either a combination of the previous master plan, new recommendations from the project team, or the current Odenton Town Center Master Plan, or the Parole Mobility Study.

Description and Destinations:

Odenton Town Center is a designated zoning district and is bounded by Patuxent Parkway (Rt 32) and generally Odenton Road to the south. The area is rapidly redeveloping with plans for major and minor subdivisions on much of the land north of MD 175 (Annapolis Rd) (Rt 175), which has been predominantly a mix of large-lot commercial and industrial uses. South of Annapolis Road, the land use is mostly medium-low density with single family detached homes. Fort Meade is located to the northwest of the Town Center across Patuxent Way (Rt 32). Town Center across Patuxent Way (Rt 32). Odenton Elementary School is within Odenton Town Center.

The Penn Line Odenton MARC Station is located on Odenton Road. Amtrak service also uses the rail corridor but does not stop at this station. The rail corridor creates a barrier for the continuation of Odenton Road, which is split into eastern and western segments. There is limited sidewalk infrastructure when accessing the station from either direction. There is a mezzanine to connect passengers from the inbound and outbound platforms.

The Washington, Baltimore and Annapolis Trail (WB&A Trail) begins at the intersection of Odenton Road and Piney Orchard Parkway and runs south to the Patuxent River and the Prince Georges County border. A shared-use path runs along Odenton Road from the WB&A Trail/Piney

Orchard Parkway east to the roundabout intersection of Odenton Road, Annapolis Road, Sappington Station Road, and Higgins Drive. Another shared-use path has been constructed as part of a new residential development on Town Center Boulevard from Odenton Road to Charter Oaks Boulevard, providing access across Patuxent Parkway; it then continues part of the way to Annapolis Road on Charter Oaks Boulevard. The Odenton pedestrian network has significant gaps, especially connecting individual residential subdivisions, along MD 175 (Annapolis Road) and Telegraph Road, and within the northern portions of the Town Center.

Proposed Improvements:

Needed walking and rolling improvements include:

- Continuous connections:
 - » On Route 175 (Annapolis Road) from the Town Center zoning district and the WB&A Trail to the Odenton MARC Station, and as part of the Jessup-Gambrills-Odenton PIA
 - » On Route 170 (Telegraph Road/Piney Orchard Pkwy) to connect to the WB&A Trail and as part of the Northwest County PIA
- Sidewalk gaps filled in neighborhoods and connecting to schools, especially on Route 175, N Patuxent Rd, Waugh Chapel Rd, Monterey Avenue, Rita Drive, King Malcolm Avenue, Bruce Avenue, and Hammond Lane
- Community connections across major barriers such as Patuxent Pkwy (MD 32) to connect to Fort Meade, the National Security Agency, and residential developments
- Shared-use path connections to Fort Meade at designated gates
- Upgraded and more frequent pedestrian crossings, especially across Route 175, Route 170, and Odenton Road

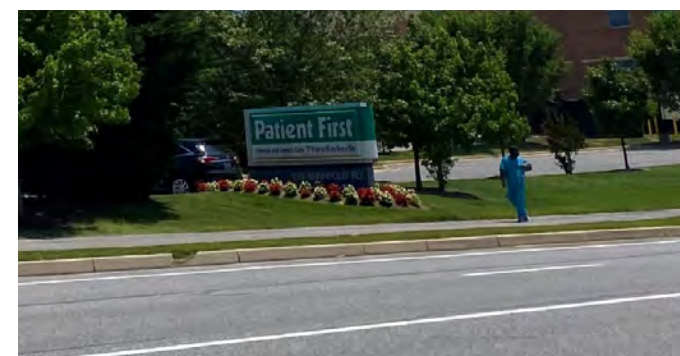
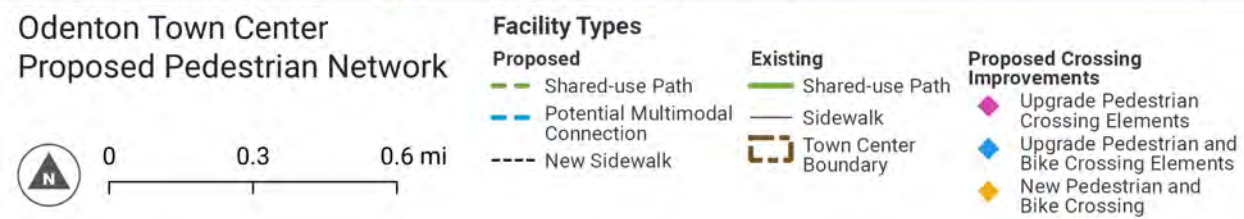


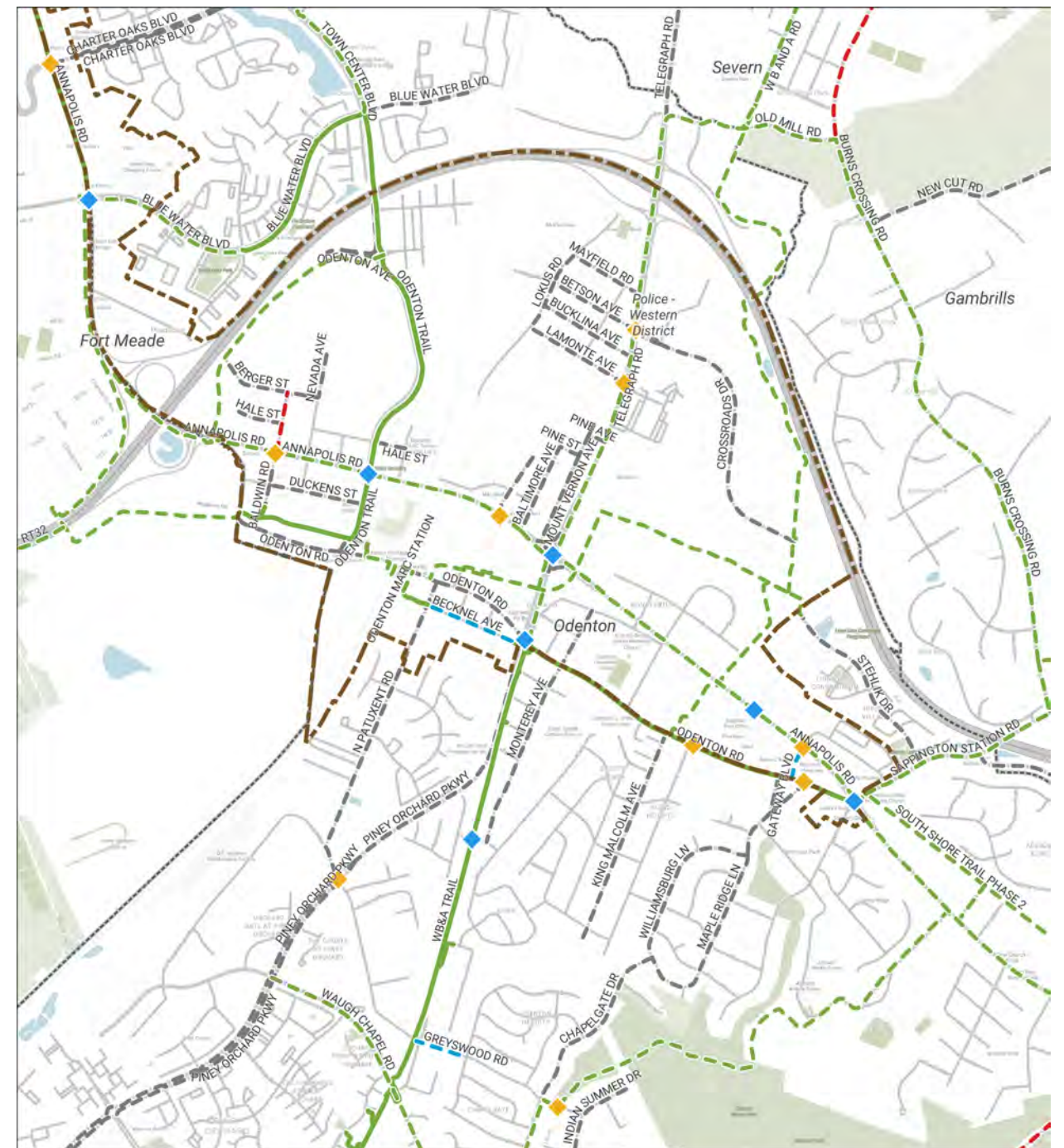
Figure 37: Bus stops and common destinations should be easy to access by people walking or rolling.



Figure 38: Walk & Roll Anne Arundel! implementing more protected bicycle facilities, like the one pictured.



Map 17: Odenton Town Center Proposed Pedestrian Network.



Map 18: Odenton Town Center Proposed Bicycle Network.

Glen Burnie Town Center

The following maps represent proposed pedestrian network improvements (Map 19) and proposed bicycle network improvements (Map 20). Recommendations are derived from the previous master plan and new recommendations from the project team.

Description and Destinations:

The Glen Burnie Town Center is organized around the Glen Burnie Town Center zoning district at the intersections of MD 648 (Baltimore Annapolis Boulevard), MD 3 (Crain Highway), and MD 2 (Ritchie Highway). The Town Center zoning district has a mix of commercial, institutional, and government land uses, including the District Courthouse and the Anne Arundel Community College. Commercial activity extends out from the Town Center zoning district to the north to the Baltimore City line, transitioning to industrial uses west and north of Glen Burnie Bypass, and south on MD 3 (Crain Highway) past Route 100. Outside of the commercial areas, the land use is characterized by low-medium density, single-family detached residential subdivisions (R5). The BWI Thurgood Marshall Airport is located less than a mile to the northwest of the Glen Burnie Town Center.

Within the Glen Burnie Town Center, Corkran Middle School, Glen Burnie High School, Anne Arundel Community College, and Monsignor Slade Catholic School are key destinations for students in the community. In addition, two private schools and two elementary schools are located south of the Town Center but north of Route 100, accessible by Oak Manor Drive and Oakwood Road. Other important multimodal connections include the commercial and medical facilities located south of Route 100 off Quarterfield Road.

The Cromwell Light Rail station is located just north of the Town Center and is not well connected for people walking or bicycling. The Baltimore and Annapolis Trail (B&A Trail)



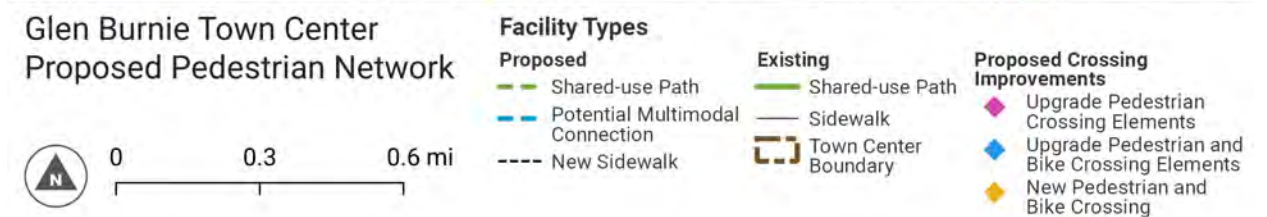
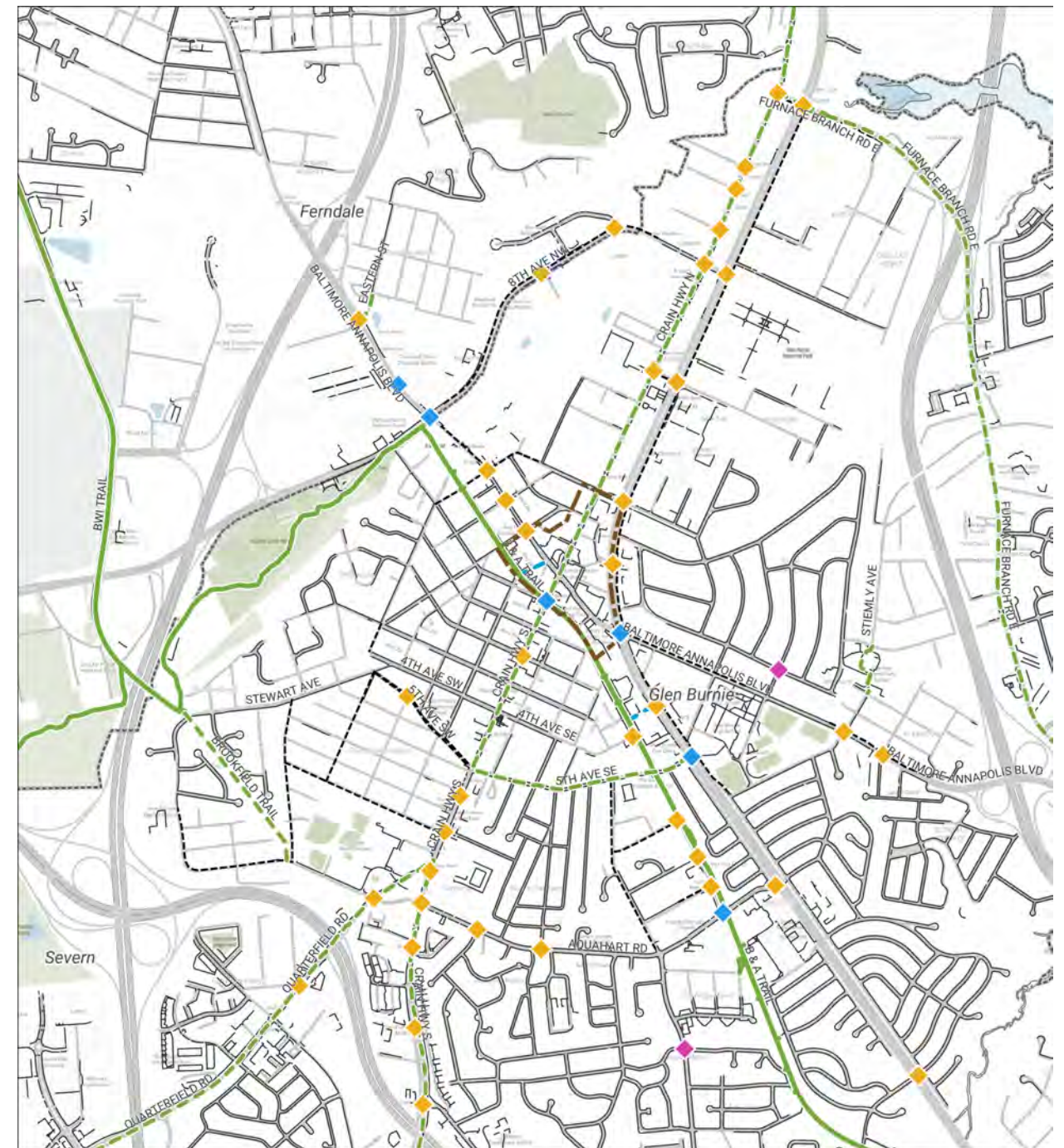
Figure 39: Students bicycling on a Dorsey Road shared-use path in a school zone in Glen Burnie.

runs south from Dorsey Road (north of the Town Center zoning district) to Annapolis, serving as a major, regional multimodal route. At its northern terminus, the B&A Trail connects west to the BWI Trail that loops around the airport. Sidewalk coverage throughout Glen Burnie is generally comprehensive with some exceptions, including MD 2 (Ritchie Highway) north of the Town Center, MD 3 (Crain Highway) north of the Town Center, Quarterfield Road, and the Glen Burnie Heights neighborhood south of the Town Center.

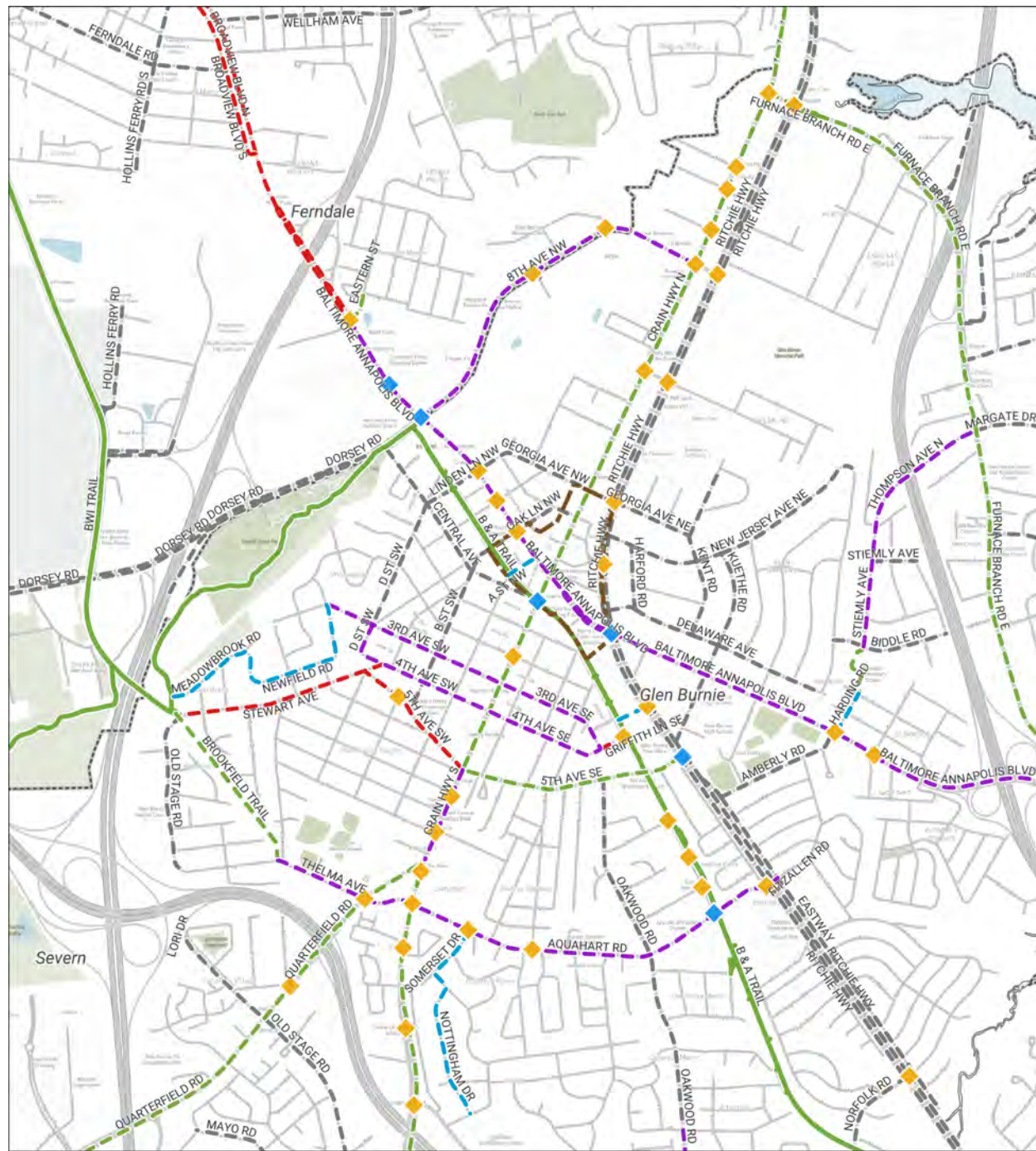
Proposed Improvements:

Needed walking and rolling improvements include:

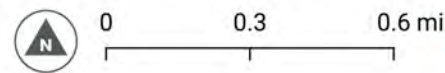
- Continuous connections:
 - » From the Town Center zoning district and the B&A Trail to Cromwell Station
 - » On Route 2 (Crain Highway) as part of North County Corridor.
- Sidewalk gaps filled, prioritizing connections to economic opportunities and services and prioritizing major roads like MD 2 (Ritchie Highway), MD 3 (Crain Highway), MD 648 (Baltimore Annapolis Boulevard), and 8th Avenue.
- Community connections across major barriers such as Route 100 and MD 10 (Arundel Expressway).
- Shared streets, calm enough for bicyclists to share the road with drivers, on Somerset Drive/Nottingham Drive and Meadowbrook Road/Newfield Road as shorter-term alternatives to traveling on MD 3 (Crain Highway) south of Aquahart Road and Stewart Avenue respectively.
- More frequent pedestrian crossings, especially across Route 2, Route 3, Route 648, and on Greenway Road to access the B&A Trail.



Map 19: Glen Burnie Town Center Proposed Pedestrian Network.



Glen Burnie Town Center
Proposed Bike Network



Facility Types		Proposed Crossing Improvements	
—	Proposed Shared-use Path	—	Existing Shared-use Path
—	Proposed Separated Bike Lane	—	Existing Bike Lane
—	Proposed Bike Lane	 	Existing Town Center Boundary
—	Proposed Low Stress Connection	◆	Proposed Upgrade Pedestrian and Bike Crossing Elements
—	Proposed Secondary Network	◆	Proposed New Pedestrian and Bike Crossing

Map 20: Glen Burnie Town Center Proposed Bicycle Network.

Parole Town Center

The following maps represent proposed pedestrian network improvements (Map 21) and proposed bicycle network improvements (Map 22) separately and are derived from either a combination of the previous master plan, new recommendations from the project team, or the Parole Mobility Study.

Description and Destinations:

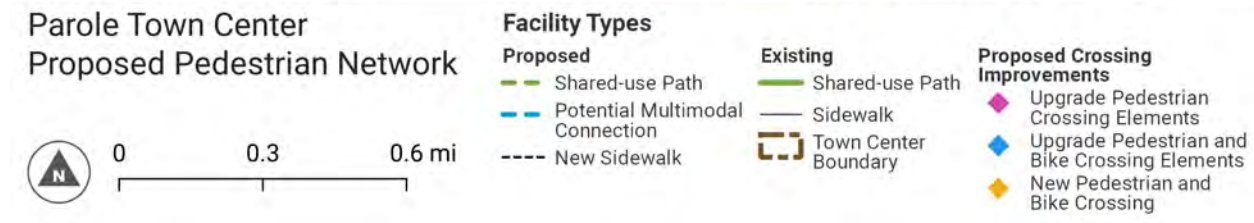
The Parole Town Center has a number of designated subareas, including Westfield Annapolis, Annapolis Town Centre at Parole, Government Park, and residential subdivisions. Schools in the area include Annapolis High School on Riva Road west of the Town Center (the site of which is also home to a swim center attracting many youth), Walter S. Mills-Parole Elementary School, Annapolis Area Christian Middle School, and Monarch Academy Annapolis. Transit is provided by Annapolis Transit, Anne Arundel County, MD Travel Link, County Ride, and MTA, resulting in fairly comprehensive coverage between subareas and multiple overlapping routes on many of the arterials in the Town Center. Connecting people to transit is especially important in Parole because the roads connecting subareas are challenging for people to cross or traverse on foot or bicycle. Connections to proposed transit hubs require special attention, especially connections to the Harry S. Truman Park and Ride on Riva Road and to Annapolis Mall.

Crossings of major and secondary barriers are infrequent and are not safe for walking or bicycling. Some segments of the shared-use path or trail network have been constructed; these need to be connected to each other and the regional system. The Parole Mobility Study (2021) notes that some of the existing shared-use paths, such as along Admiral Drive, MD 2, and MD 450, are low comfort due to insufficient separation between the sidepath and high-speed roadways. Comprehensive sidewalk connections are missing within commercial superblocks with large parking lots and in residential subdivisions.

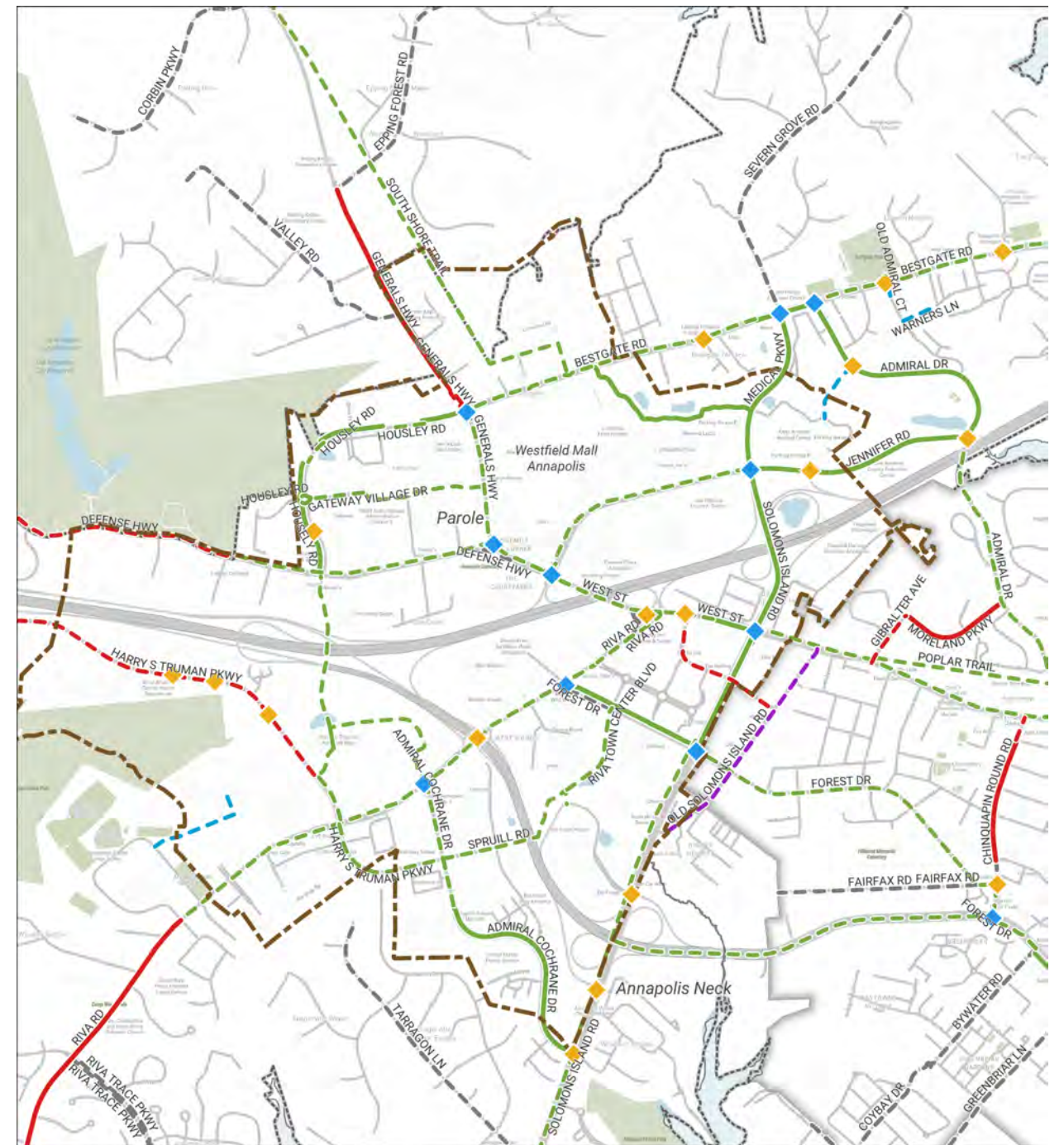
Proposed Improvements:

Needed walking and rolling improvements include:

- Continuous connections:
 - » On Route 178 (Generals Highway) as part of the South Shore Trail PIA
 - » On Forest Drive as part of Annapolis Neck PIA
 - » On Bestgate Road and Route 2 as part of the Parole-Annapolis PIA
- Upgraded and more frequent pedestrian crossings throughout Parole
- A shared-use path connection to Annapolis High School, a swim center, and the Harry S. Truman Park and Ride on Riva Road
- Potential new multimodal connections that could increase connections through large parcels and across major barriers, such as US 301/US 50 and MD 665
- Completion of gaps in the shared-use path network, such as on Housley Road, Jennifer Road, and Harry S Truman Parkway.



Map 21: Parole Town Center Proposed Pedestrian Network.



Map 22: Parole Town Center Proposed Bicycle Network.



Chapter 4: Implementation

Figure 40: Recreational bicyclists in rural Anne Arundel.

It will take significant effort and funding to plan, design, and construct the proposed *Walk & Roll Anne Arundel!* network. Strategic planning to carefully fund and phase projects is crucial to successful implementation of the County’s walking and rolling infrastructure. Coordination is needed between various City, County and State agencies, especially the Departments of Public Works (DPW), Recreation and Parks (AACRP), the Office of Planning and Zoning, MDOT SHA, City of Annapolis, transit agencies, as well as private developers, property owners, and other stakeholders.

This chapter outlines how to implement the County’s active transportation vision, how to select pedestrian and bicycle facilities, what funding sources to use, recommended phasing, priority projects, and performance measures to ensure the network’s success.

Prioritization of Pedestrian and Bicycling Infrastructure

The benefits of walking and rolling infrastructure to Anne Arundel County abound. With a completed multimodal network, a greater number of residents will be able to

get where they need to go to meet daily needs, including those who cannot drive due to age, disability, or cost. These projects are crucial to the economic growth of the County and individual residents’ ability to thrive.

With limited available funding, it is paramount to evaluate and choose transportation projects that will result in the most benefit to the County. To this end, the project team scored each proposed walking and bicycling project according to its ability to support *Walk & Roll Anne Arundel!*’s established goals.

The criteria used to analyze each project relate to a project’s anticipated impacts on crash reduction, connectivity to essential destinations, connectivity to adjacent walking and bicycling facilities, and characteristics of traditionally underserved segments of the population. The prioritization criteria are summarized in Table 12. The prioritization methodology is flexible and may be changed for future evaluation. Refer to Appendix E.2 for the top 50 projects identified by this methodology.

Table 12: Factors and weights used to prioritize bicycle infrastructure and shared-use paths in Anne Arundel County.

Factor	Variable	How Variable Will Be Measured	Reasoning	Factor Weight
Potential Demand	Employment Density	Employment density within 1 mile of proposed bikeway/SUP Additional point for density within ¼ mile	Promotes bikeways/SUP within areas of greater employment density	25%
	Population Density	Population density within 1 mile of proposed bikeway/SUP Additional point for density within ¼ mile	Promotes bikeways/SUP within areas of greater population density	
	Destination Density	Number of schools/colleges, hospitals/clinics, libraries, parks, and shopping centers within 1 mile of proposed bikeway/SUP Additional point for density within ¼ mile	Promotes proposed bikeways/SUP near essential destinations	
Connectivity	Nearby Existing Bicycle/SUP Facilities & Those In Progress	Presence of existing and under construction/design bicycle/SUP facilities directly connecting to proposed facilities	Prioritizes proposed bicycle/SUP facilities in places where a higher density of bicycle facilities already exists	25%
	Nearby Proposed Bikeways/SUPs Nearby	Presence of proposed bicycle/SUP facilities directly connecting to a <i>Walk & Roll Anne Arundel!</i> proposed facility	Prioritizes proposed bicycle/SUP facilities in places where a higher density of bicycle facilities will exist in the future	
	Connectivity to Transit Stops	Number of transit stops connecting to a proposed bikeway/SUP	Promotes bikeways/SUP near transit stations & stops (local bus, regional bus, light rail, commuter rail)	

Factor	Variable	How Variable Will Be Measured	Reasoning	Factor Weight
Safety	Bicyclist Crash Density	Number of crashes involving bicyclists per mile within 150 feet of a proposed bikeway/SUP Crashes will be weighted by severity (fatal, serious injury, non-serious injury)	Promotes safety by elevating bikeways/SUP on streets that have a greater density of bicycle crashes within 150 feet of a segment	25%
	Bicyclist Crash Risk Rate	Risk rate computed from the Safer Streets Priority Finder based on a sliding windows analysis of crash data, functional classification of the road, and census tract data	Promotes safety by accounting for streets with few or no recent bicycle crashes, but that have similar characteristics to streets where crashes have occurred	
	Pedestrian Crash Density	Number of crashes involving pedestrians per mile within 150 feet of a proposed facility Crashes will be weighted by severity	Promotes safety by elevating proposed facilities on streets that have a greater density of pedestrian crashes within 150 feet of a segment	
	Pedestrian Crash Risk Rate	Risk rate computed from the Safer Streets Priority Finder based on a sliding windows analysis of crash data, functional classification of the road, and census tract data	Promotes safety by accounting for streets with few or no recent pedestrian crashes, but that have similar characteristics to streets where crashes have occurred	
Equity	Vulnerable Populations Index (VPI)	Score generated by weighing 7 determinants of transportation vulnerability equally: 1. Poverty 2. People of Color (i.e., non-white, non-Hispanic) 3. Hispanic 4. Limited English Proficiency 5. Disabled 6. Elderly 7. Carless	Promotes bikeways/SUP within areas where people are more likely to rely on them to meet transportation needs	25%

Safety and Crash Reduction

Given the County’s commitment to Vision Zero, a proactive, systemic approach to transportation planning is needed. A systemic approach uses historical crash and roadway data to identify roadway features that are likely to result in crashes. Using this approach, the County can identify roadways that have a high risk for severe crashes, even if they have not yet occurred. With this information, the County can focus resources on the highest-risk roadways.

To this end, the project team evaluated the magnitude of need for safe walking and bicycling infrastructure based on:

1. Data on the location and quantity of pedestrian and bicyclist crashes that occurred within a five-year period (2016 – 2020); and
2. Characteristics of roadways throughout the County that have a greater likelihood of crashes involving people walking or bicycling.

The project team used GIS to determine the location and quantity of bicycle and pedestrian crashes.

The project team followed the methodology of the Safer Streets Priority Finder, a new online tool that allows people to ascertain where the risk of being hit by a vehicle while walking or bicycling is high or low, based on characteristics of the roadway and surrounding community.²⁹

This crash risk analysis is important because locating recent pedestrian and bicyclist crashes does not accurately predict where crashes are most likely to occur in the future throughout the County. Solely utilizing past crash data makes it possible to conclude that these locations are the only ones with the most dangerous travel conditions, which may or may not be the case. Severe crashes resulting in the death or serious injury of a person may occur sporadically throughout a large area. Where and how crashes continue to happen can

29 Safer Streets Priority Finder

also change over time. What is important to understand are the roadway conditions proximate to where crashes commonly occur. Future crashes are more likely to occur on roadways with similar features.

To complete the crash risk analysis, the project team first determined locations in the County with the highest density of crashes, weighted by crash severity, using a “sliding window” approach. As noted on the Safer Streets Priority Finder tool, “Crash density corridors are identified by applying a 1/2-mile moving window aggregation to the street network in the study area. The 1/2-mile sliding windows are created to form corridors using the roadway street name. In this approach, a virtual “window” is moved along each street in 1/10-mile increments, counting the number of crashes by severity and mode that occurred within each successive 1/2-mile segment.”

Crashes resulting in possible injuries or only property damage were not accounted for in the analysis.

The “sliding window” approach allows for the identification of stretches of roadway with the highest number of crashes – and where those stretches overlap. This practice avoids breaking up a high crash corridor area in GIS, which could unintentionally lower the risk rate of a high-risk street.

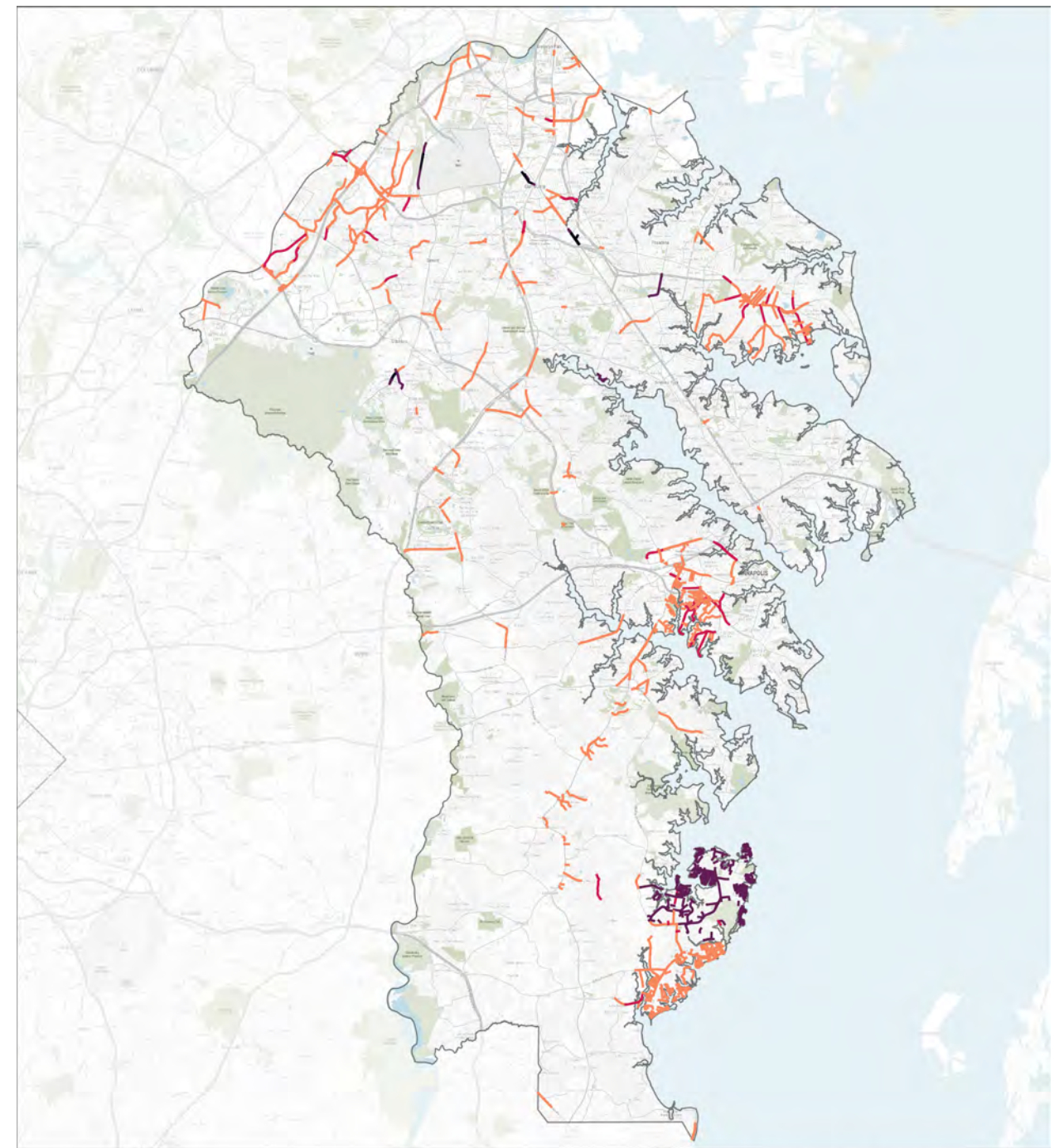
The project team then used the Safer Streets Priority Finder to identify the areas that present the highest risk to people walking and bicycling. The tool processes two data sets:

1. Data from previous crashes, determined in the first step; and
2. Physical characteristics of roadways, using a model developed by the U.S. Department of Transportation. These characteristics include the functional class of the road (or type of road), Vehicle Miles Traveled (VMT) on that section of road, intersection density, employment density, residential population density, activity mix index, and sociodemographic information.

This tool has been used in several cities around the United States; however, this is one of the first times it is being applied in a local planning process. By prioritizing improvements on streets with high crash risk, the tool incorporates the prioritization of facilities on roads with a high level of traffic stress.

The results of the crash risk analysis are shown in Maps 23 and 24. According to this analysis, 32% of all crashes in Anne Arundel County resulted in an injury, possible injury, or death. Between 2016 and 2020, there were nearly 1,600 recorded crashes involving a pedestrian or bicyclist. Notably, over half of all crashes that resulted in the death or injury of a person who was walking or bicycling occurred on arterial roads, even though arterials comprise just 15% of the County’s roadway network. Arterial roads like MD 2 (Ritchie Highway), MD 177 (Mountain Road), or MD 648 (Baltimore-Annapolis Boulevard) are typically large, high-speed and high-volume roads, which create more exposure to risk. Thus, these locations should be prioritized to improve safety for people walking or bicycling. The analysis also identified areas that have a high risk of being crash “hot spots.”

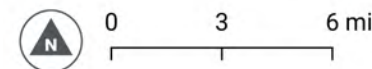
Walk & Roll Anne Arundel! proposed walking and bicycling projects along roadways deemed likely to result in a pedestrian and bicyclist crash received more points in the prioritization process. These roadways are also those where pedestrians and bicyclists experience high levels of traffic stress.



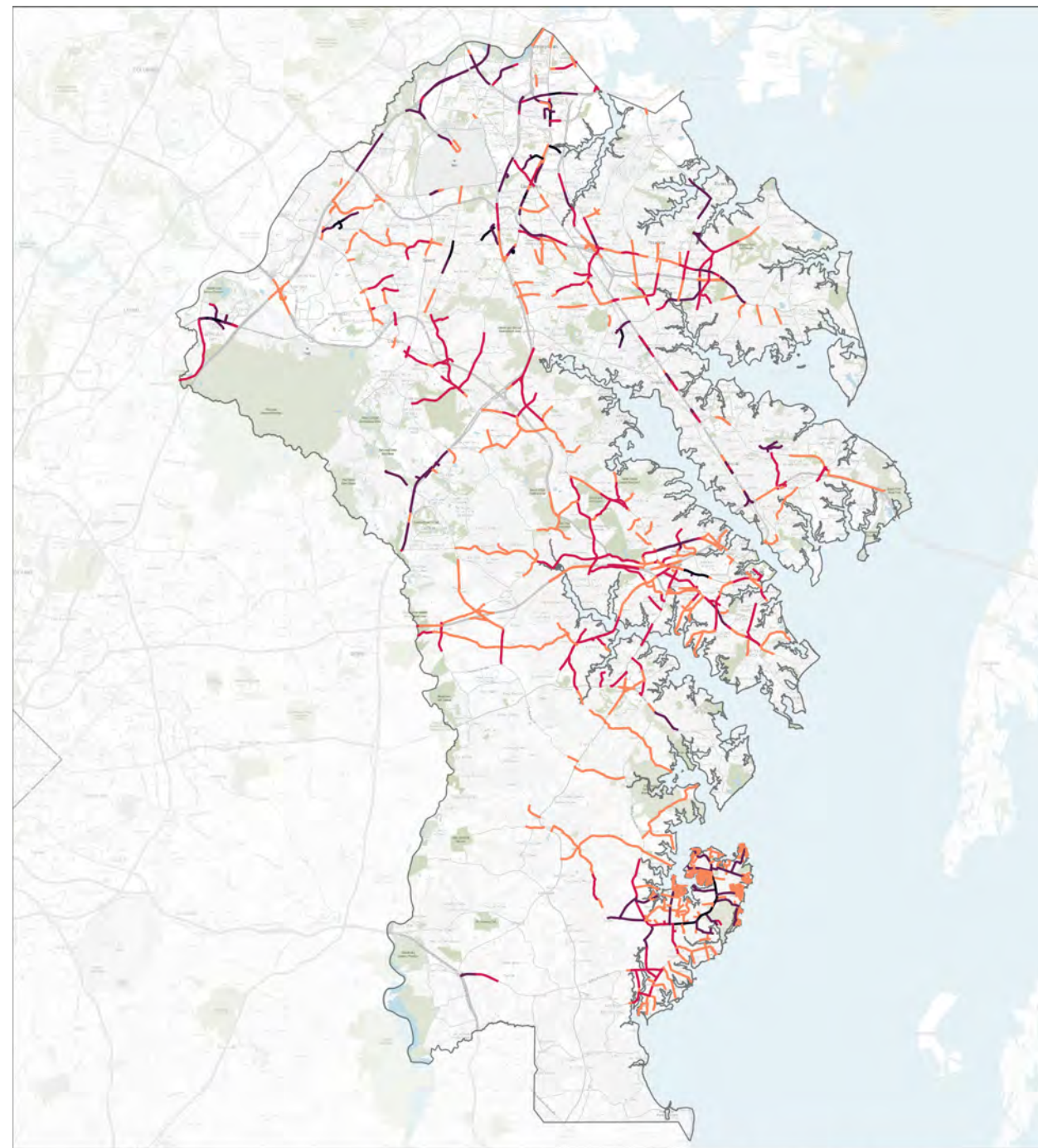
Anne Arundel County
SSPF Bicycle Crash Risk

Bike Crash Risk
 Lower
 Higher

Map generated using the Safer Streets Priority Finder and Anne Arundel County 2016-2020 crash data.



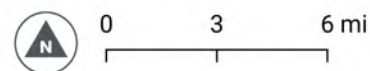
Map 23: Bicycle Crash Risk, 2016-2020.



Anne Arundel County
SSPF Pedestrian Crash Risk

Pedestrian Crash Risk
 — Lower
 — Higher

Map generated using the Safer Streets Priority Finder and Anne Arundel County 2016-2020 crash data.



Map 24: Pedestrian Crash Risk, 2016-2020.

Connectivity to Essential Destinations

This process prioritizes bicycling or shared-use path infrastructure projects within walking or bicycling distance to the following essential destinations:

1. Job centers
2. Schools (elementary, middle and high schools, community colleges, colleges, and universities)
3. Shopping centers (including grocery stores)
4. Hospitals and health care clinics
5. Parks
6. Libraries
7. Residential areas
8. Public transportation stations and stops (regional bus stops, local bus stops, light rail stations, commuter rail stations). The project team assessed the relative density of essential destinations, with areas with higher densities receiving higher priority for multimodal projects than areas with lower densities of key destinations.

The County prioritized multimodal projects within 1 mile of an essential destination. Additional weight was also given to multimodal projects within ¼-mile of an essential destination. A 1-mile and ¼-mile radius were therefore used in the prioritization process.

Equity

To calculate areas of the County where greater numbers of vulnerable populations reside, the project team used the Baltimore Metropolitan Council’s (BMC) Vulnerable Populations Index (VPI). BMC describes the VPI prioritization methodology as such:

“For each of these [vulnerable] populations, Census tracts with concentrations above the regional mean concentration are divided into two categories above the regional mean. These categories are calculated by dividing the range of values between the regional mean and the regional maximum into two equal-sized intervals. Tracts in the lower interval are given a score of 1 and tracts in the upper interval are given a score of 2 for that demographic variable. The scores are totaled from the seven individual demographic variables to yield the Vulnerable Population

Index (VPI). The VPI can range from zero to fourteen (0 to 14). A lower VPI indicates a less vulnerable area, while a higher VPI indicates a more vulnerable area.”³⁰

As a result, multimodal projects in an area with a higher than average rate (based on the regional mean) of one of the 7 demographic characteristics would receive between 1 and 2 points; projects in an area with a higher than average rate of 2 of the 7 demographic characteristics would receive between 2 and 4 points; projects in an area with a higher than average rate of 3 of the 7 demographic characteristics would receive between 3 and 6 points; projects in an area with a higher than average rate of a 6 of the 7 characteristics would receive 14 points, since the most disadvantaged census tracts in Anne Arundel County have higher than average rates of 6 of the 7. Note that no census tracts in Anne Arundel County have higher than average rates of all 7 characteristics of vulnerable populations.

Connectivity

Bicycling infrastructure and multi use paths proposed in *Walk & Roll Anne Arundel!* received points in the prioritization process if they directly connect to existing sidewalks, shared-use paths or bicycling infrastructure, or those currently under construction or being designed.

Proposed walking and bicycling projects also received points if they directly connect to other sidewalks, shared-use paths or bicycling infrastructure proposed in *Walk & Roll Anne Arundel!*.

No points were given to proposed projects that would not directly connect with existing, in progress, or proposed walking or bicycling infrastructure.

Cost and Feasibility

After prioritizing projects using this criteria-based approach, the project team determined estimated costs and feasibility. As discussed in Chapter 5, costs were determined using a cost calculator provided by the Maryland Department of Transportation. Feasibility was suggested based on presence of sufficient space, utility relocation needs, and a rudimentary ROW needs evaluation. Estimated costs and a brief analysis of feasibility for each recommended project is provided in Appendix F.

³⁰ <https://www.baltometro.org/transportation/data-maps/vulnerable-populations-index>.

Strategies for Implementation

Facility Selection

Walk & Roll Anne Arundel! suggests walking and rolling facility types across the County’s roadway network, while still recognizing that the project design process should dictate the final design of facilities.

The County should continue to use leading facility selection resources, especially:

1. FHWA’s Bikeway Selection Guide³¹
2. NACTO’s Urban Street Design Guide³²
3. NACTO’s Urban Bikeway Design Guide³³
4. FHWA’s Safe Transportation for Every Pedestrian Program³⁴
5. FHWA’s Designing Sidewalks and Trails for Access, Best Practices Design Guide: Part 1 of 2³⁵
6. FHWA’s Designing Sidewalks and Trails for Access, Best Practices Design Guide: Part 2 of 2
7. FHWA’s Achieving Multimodal Networks³⁶
8. FHWA’s Separated Bike Lane Planning and Design Guide³⁷
9. AASHTO’s Guide for the Development of Bicycle Facilities³⁸
10. AASHTO’s Guide for the Planning, Design, and Operation of Pedestrian Facilities: 2nd Edition.³⁹

When designing each project, the County should provide the highest feasible standard of walking and bicycling facility; this will ensure the safety, comfort, and willingness to travel for walkers, bicyclists, and others – of all ages and abilities – using the active transportation network.

The Active Transportation Guidelines in Appendix A provide design guidance to support safe, convenient, and accessible travel for people who walk, people with disabilities who use assistive devices, and those who ride bicycles. It covers topics ranging from providing

enough space for people to comfortably move, ensuring protection from moving vehicular traffic, and creating safe, convenient street crossings.

Three important considerations that affect the safety and comfort of people walking and rolling on the County’s active transportation system are:

1. The volume of traffic on a given road;
2. The number of travel lanes; and
3. The speed of vehicular traffic.

Using the guideline documents listed above, the County should provide the safest, most comfortable pedestrian and bicycle facilities along and across roadways, specialized for the characterization and context of each roadway.

Funding

The County should develop a funding strategy in order to pay for the active transportation network. Funding may come from a variety of different sources, including the County, the State of Maryland, the federal government, local jurisdictions, and private developers. Funding programs each have their own stipulations and goals, related to safety, safe routes to schools, recreation, Complete Streets, or other areas of focus.

The County should examine each available source of funding and develop strategies to effectively compete for those dollars, including budgeting for matching funds and assuring projects are ready for grant cycles. Each source of funding dictates certain requirements and goals. Most funding programs require a certain level of planning or design to be accomplished before an award is approved.

The County should continue, and expand upon, its strategy to integrate active transportation projects into roadway modification or resurfacing projects. The County should also consider greenway development in stormwater and flooding response projects, since greenways are often eligible stormwater management infrastructure. The County could also create funding partnerships between different agencies and

organizations, such as public schools, health groups, universities/colleges, or large employers to fund active transportation. Furthermore, future development projects should fund and build walking and bicycling projects that meet the goals and standards established in *Walk & Roll Anne Arundel!*.

The State of Maryland’s available funding programs for walking and bicycling projects are provided in “Appendix B: Bicycle and Pedestrian Funding Programs” of the 2019 Maryland Bicycle & Pedestrian Master Plan Update.⁴⁰

The Federal transportation program funds walking and bicycling projects through a variety of sources. Eligibility of specific facilities and activities is summarized by FHWA in “Pedestrian and Bicycle Funding Opportunities: U.S. Department of Transportation Transit, Highway, and Safety Funds.”⁴¹

In addition, the 2022 Federal Bipartisan Infrastructure Law established four new sources of funding for active transportation projects. Table 13 describes these opportunities.

Table 13: Federal funding sources established by the 2022 Federal Bipartisan Infrastructure Law

New Federal Funding Sources

Rebuilding American Infrastructure with Sustainability and Equity, or RAISE Discretionary Grant program: Provides funding to invest in on-street bikeways, sidewalks, trails, Complete Streets, roadway, rail, transit and port projects.⁴²

The Safe Streets and Roads for All Program: This \$5 billion grant program provides funding to eliminate roadway fatalities and serious injuries, or “vision zero” transportation projects. Eligible projects include bicycle lanes, separated bikeways, signal improvements, traffic calming measures, intersection safety projects, and other safety projects for vulnerable travelers.⁴³

Reconnecting Communities Pilot: This \$1 billion discretionary grant program provides funding for communities to equitably and safely restore community connectivity by removing, retrofitting, mitigating, or replacing transportation facilities that bar mobility, access, or economic development.⁴⁴

The Defense Community Infrastructure Pilot, which funds transportation projects that support a military installation, may be used to fund bicycle and pedestrian facilities.⁴⁵

31 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf

32 [Urban Street Design Guide | National Association of City Transportation Officials \(nacto.org\)](https://www.nacto.org/resources/urban-street-design-guide/)

33 [Urban Bikeway Design Guide | National Association of City Transportation Officials \(nacto.org\)](https://www.nacto.org/resources/urban-bikeway-design-guide/)

34 [Safe Transportation for Every Pedestrian \(STEP - Safety | Federal Highway Administration \(dot.gov\)](https://www.fhwa.dot.gov/safety/steep/safety/steep-safety-federal-highway-administration/)

35 [https://www.nacto.org/resources/designing-sidewalks-trails-access_alexson.pdf](https://www.nacto.org/resources/designing-sidewalks-trails-access/)

36 [Multimodal Networks - Publications - Bicycle and Pedestrian Program - Environment - FHWA \(dot.gov\)](https://www.fhwa.dot.gov/environment/bicycle_pedestrian/publications/separated_bikelane_pdg/page00.cfm)

37 https://www.fhwa.dot.gov/environment/bicycle_pedestrian/publications/separated_bikelane_pdg/page00.cfm

38 <https://www.nacto.org/resources/aashto-guide-for-the-development-of-bicycle-facilities-2012/>

39 [https://aashtojournal.org/2021/12/17/aashto-issues-revised-pedestrian-facilities-guide/](https://www.aashtojournal.org/2021/12/17/aashto-issues-revised-pedestrian-facilities-guide/)

40 https://mdot.maryland.gov/OPCP/Appendix_B_Funding.pdf

41 https://www.fhwa.dot.gov/environment/bicycle_pedestrian/funding/funding_opportunities.pdf

42 <https://www.transportation.gov/RAISEgrants>

43 <https://www.transportation.gov/grants/SS4A#:~:text=The%20SS4A%20program%20funds%20regional,injuries%20on%20our%20nation's%20roadways>

44 <https://www.transportation.gov/grants/reconnecting-communities>

45 <https://oldcc.gov/defense-community-infrastructure-program-dcip>

Network Phasing

The County should phase active transportation project development into three phases, based on funding availability and feasibility.

Phase 1 (1-5 years)

During this phase, the County should pursue projects that complete gaps between existing walking and bicycling facilities or improve existing facilities to the standards established in this Plan.

Phase 1 projects should also be those identified in previous plans, such as:

1. Safe Routes to Schools projects identified in *Move Anne Arundel!*
2. Trail connections
3. The County's Capital Improvement Projects, which are currently funded.

To save time and money, projects in this phase should be constructable within existing rights-of-way, or at least without the need to move existing structures or acquire more than 1/4 of a property owner's property in the frontage zone. Near-term projects should also be identified in already-planned roadway projects that are in preliminary design or close to final design.

The County should continue adding on-street bicycle facilities to resurfacing projects.

Properties undergoing private development should also be prioritized for near-term walking and bicycling facilities that incorporate the standards established in *Walk & Roll Anne Arundel!*.

Beginning in this phase, and continuing through all phases, increase public awareness of available walking and bicycling facilities. Expand programs such as Safe Routes to School. Refine the County's experience with new micromobility devices, such as rental scooters.

Phase 2 (5-15 years)

Active transportation projects accomplished during this phase should meaningfully expand the system constructed during the first phase. Projects should include those that are more time- and capital-intensive to implement, which may require a phasing approach. As the active transportation network is built out, there may be opportunities to group individual projects together. The County should remain flexible and consider all projects during each phase in order to take advantage of opportunities to implement other projects when they arise.

Long-Term (15+ years)

By this period, more developments will have been built throughout the County, constraining available right-of-way along existing roads. The County may need to make difficult decisions about the use of roadway space and examine trade-offs in walking, bicycling, transit, and motor vehicle pathways.

By this time, new technologies will also have emerged or been implemented, making travel patterns difficult to predict.

The County should focus on building out its active transportation network in the Phase 1 and 2 timelines, providing a wider foundation to expand walking and bicycling facilities in the long-term. It is possible the County will need to revisit long-term strategies and projects in 15 years. The County should be diligent to keep track of its progress according to the established performance measures of this Plan.

Phase 1 and Phase 2 projects are identified here. Phase 1 projects – recommended to be constructed in the short-term – consist of the 50 projects that scored the highest, using the Plan's prioritization process, throughout the entire County. Projects recommended for mid-term construction include the top 10 prioritized projects for each of the County's nine Regional Planning Areas. The maps below show Phase 1 projects in red and Phase 2 projects in green.

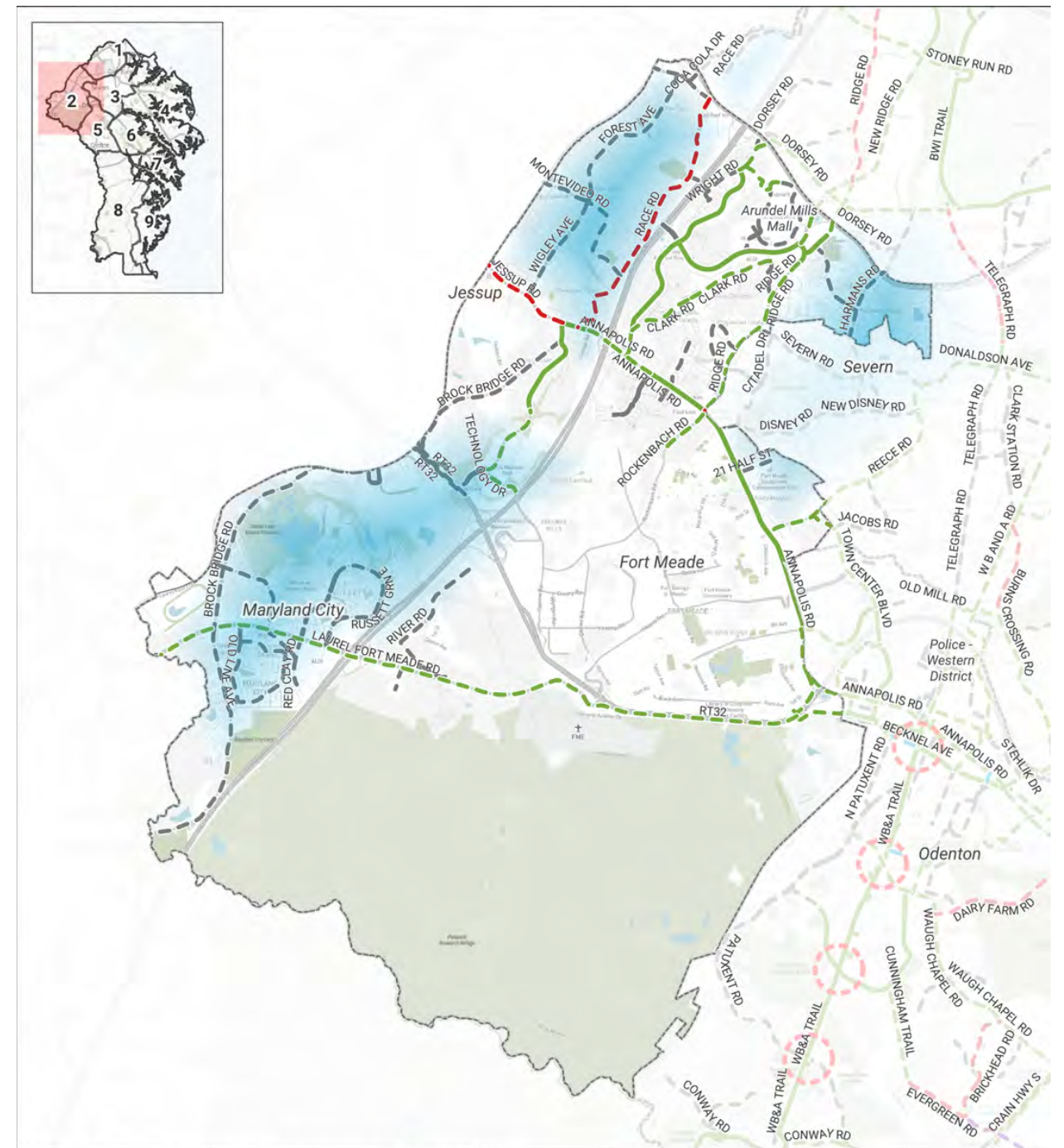


Regional Planning Area 1

The RPA 1 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost
Ritchie Hwy	Shared-use Path	Northern County Line	MD 3 (Crain Hwy)	\$18,980,762
8th Ave NW	Separated Bike Lane	8th Ave NE, MD 3 (Crain Hwy N)	MD 648 (Baltimore Annapolis Blvd)	\$557,025
MD 648 (Baltimore Annapolis Blvd)	Separated Bike Lane	Nursery Rd	MD 177 (Mountain Rd)	\$3,619,476
Belle Grove Rd (Baybrook Connector)	Shared-use Path	Gibbons Ave	Potee St, S Hanover St, Jack St Intersection	\$963,226
MD 713 (Ridge Rd) and New Ridge Rd	Shared-use Path	MD 175 (Annapolis Rd) & Ridge Rd	Stoney Run Rd & Ridge Rd	\$12,400,000
WB & A Rd	Shared-use Path	BWI Trail	Old Mill Rd	\$8,423,415
Ridge Rd	Shared-use Path	New Ridge Rd	Teague Rd	\$3,400,000
Church St	Bike Lane	Ritchie Hwy	County Line	\$439,499

*Projects are not in order of priority and need further study by the County



Anne Arundel Region 2 Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

Key Trail Intersections



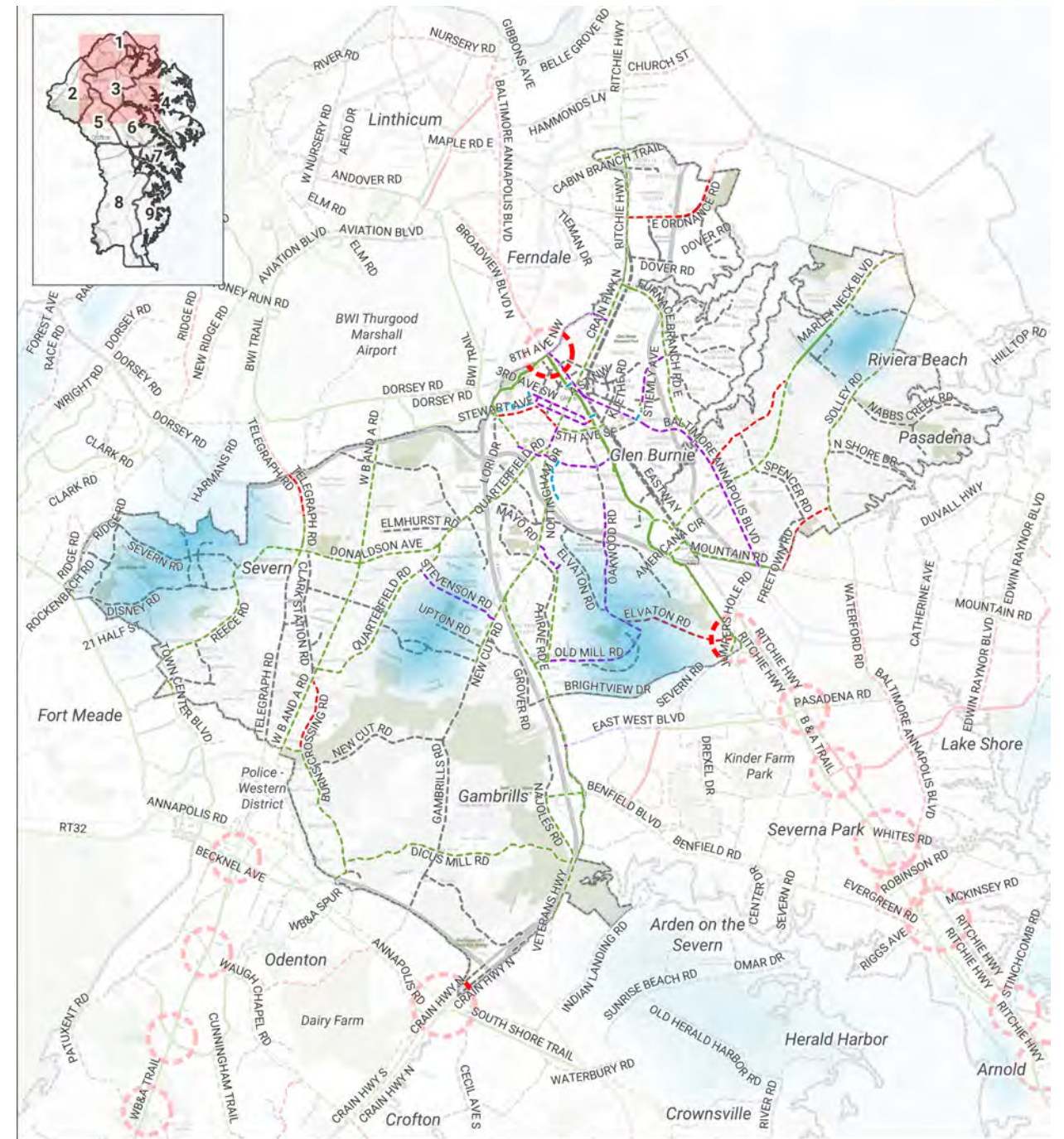
Map 26: Region 2 Proposed Bicycle Network.

Regional Planning Area 2

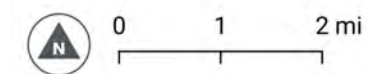
The top prioritized projects for RPA 2 include:

Street Name	Facility Type	Start	End	Estimated Cost
Reece Rd	Shared-use Path	Donaldson Ave, Telegraph Rd	MD 175 (Annapolis Rd)	\$6,454,305
MD 175 (Annapolis Rd)	Shared-use Path	Baltimore Washington Pkwy	South of Rae Rd	\$911,625
Clark Rd	Shared-use Path	Milestone Pkwy	Watts Ave	\$3,008,363
National Business Pkwy	Shared-use Path	Technology Dr	North of Sentinel Way, at the existing SUP on National Business Pkwy	\$1,604,460
Ridge Rd	Shared-use Path	New Ridge Rd	Teague Rd	\$3,400,000
Jessup Rd	Bike Lane	Western County Line	Milestone Pkwy	\$1,256,878
MD 198 (Laurel Fort Meade Rd)	Shared-use Path	Western County Line	Mapes Road	\$11,167,406
Route 32 (Patuxent Fwy)	Shared-use Path	East of Mapes Road	Annapolis Road	\$6,381,375

*Projects are not in order of priority and need further study by the County



Anne Arundel Region 3 Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

- Key Trail Intersections



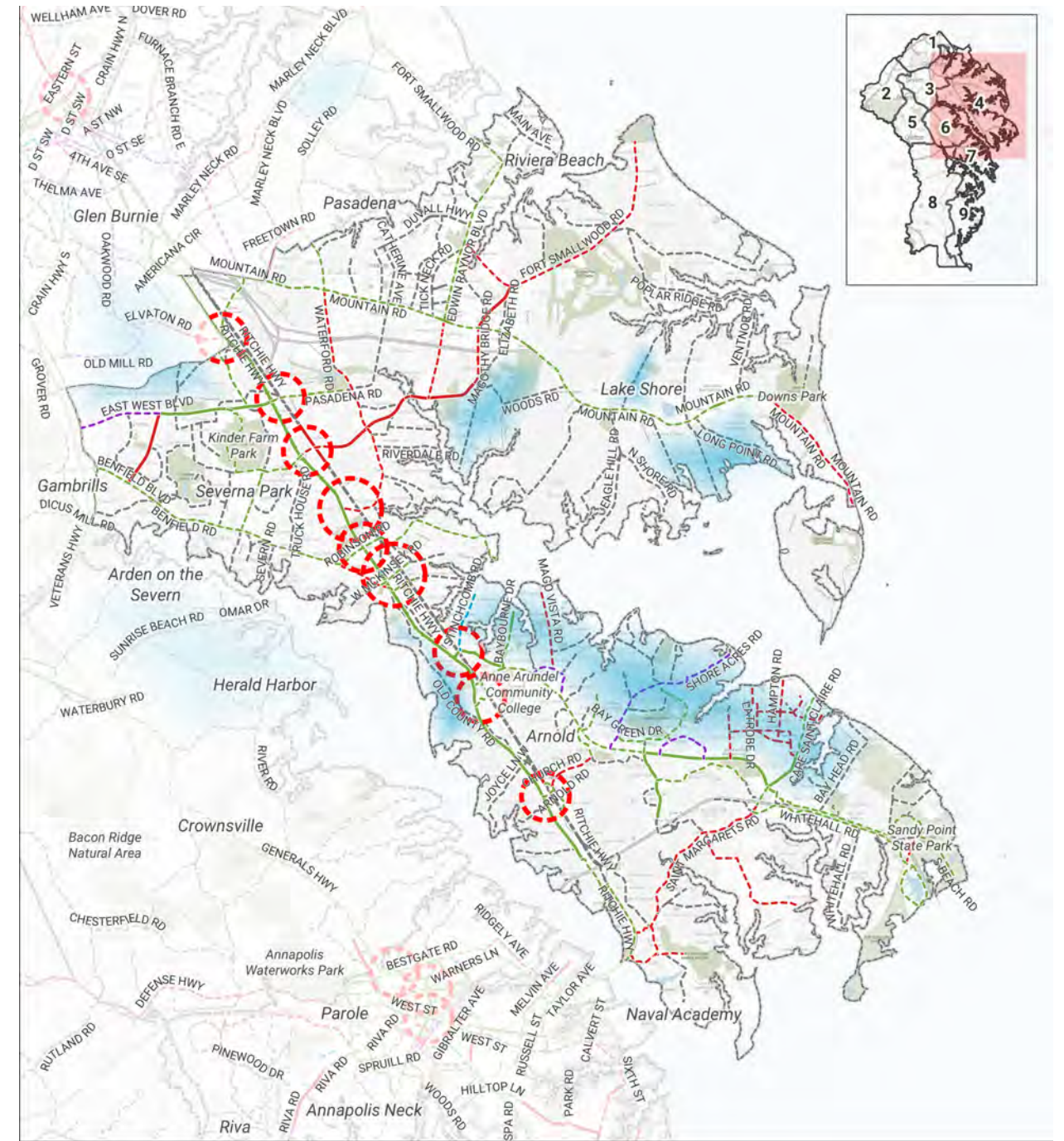
Map 27: Region 3 Proposed Bicycle Network.

Regional Planning Area 3

The RPA 3 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost
Ritchie Hwy	Shared-use Path	Northern County Line	MD 3 (Crain Hwy)	\$18,980,762
MD 648 (Baltimore Annapolis Blvd)	Separated Bike Lane	Nursery Rd	MD 177 (Mountain Rd)	\$3,619,476
Aquahart Rd and Fitzallen Rd	Separated Bike Lane	Aquahart Rd, MD 3 (Crain Hwy S), Thelma Ave,	Fitzallen Rd, Upton Rd	\$1,084,245
MD 3 (Crain Hwy)	Shared-use Path	MD 3 (Crain Hwy N) & Ritchie Hwy	MD 3 (Crain Hwy S) & Veterans Hwy	\$14,808,072
5th Ave SE	Shared-use Path	5th Ave SW, MD 3 (Crain Hwy S)	Ritchie Hwy	\$898,066
Quarterfield Rd	Shared-use Path	MD 3 (Crain Hwy S)	Old Stage Rd	\$1,203,345
8th Ave NW	Separated Bike Lane	8th Ave NE, MD 3 (Crain Hwy N)	MD 648 (Baltimore Annapolis Blvd)	\$557,025
Reece Rd	Shared-use Path	Donaldson Ave, Telegraph Rd	MD 175 (Annapolis Rd)	\$6,454,305
Griffith Ln	Separated Bike Lane and Bike Lane	Crane Highway	Greenway SE	\$495,994
Marley Neck Blvd	Shared-use path	MD 648 (Baltimore Annapolis Blvd), Marley Station Rd	Fort Smallwood Rd	\$9,025,088

*Projects are not in order of priority and need further study by the County



Anne Arundel Region 4 Proposed Bike Network

Facility Types

Proposed

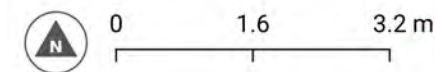
- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

Key Trail Intersections



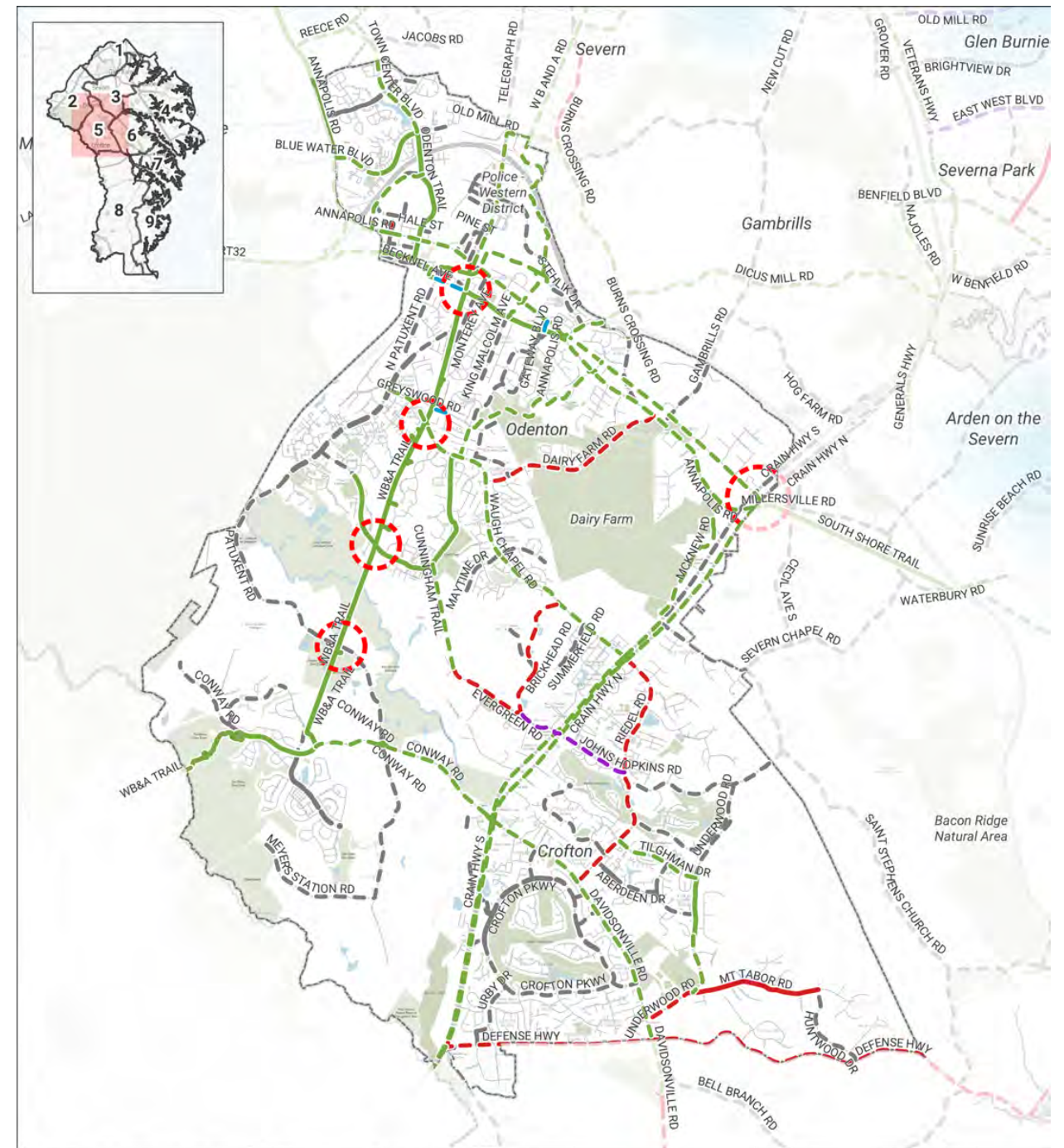
Map 28: Region 4 Proposed Bicycle Network.



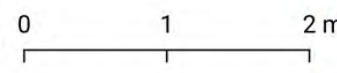
Regional Planning Area 4

The RPA 4 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost
Route 2 (Ritchie Hwy)	Shared-use Path	Near Boulters Way	Naval Academy Bridge	\$5,469,750
Jumpers Hole Rd	Shared-use Path	MD 648 (Baltimore Annapolis Blvd), MD 177 (Mountain Rd)	Benfield Rd	\$4,612,823
MD 177 (Mountain Rd)	Shared-use Path	Ritchie Hwy	Pinehurst Rd	\$7,612,069
Riggs Ave/ McKinsey Rd	Shared-use Path	Riggs Ave & Lennox Ave	McKinsey Rd & Leelyn Dr	\$1,203,345
Robinson Rd	Shared-use Path	Benfield Rd	Asbury Dr & Leelyn Dr	\$1,403,903
MD 648 (Evergreen Rd)	shared-use path	Evergreen Rd & Benfield Rd	Rt 2 Cyprus Creek	\$1,604,460
Bay Green Dr	Shared-use Path	Shore Acres Rd	College Pkwy	\$3,208,920
Benfield Rd	Shared-use Path	Veterans Hwy	Robinson Rd	\$4,612,823
College Pkwy	Shared-use Path	B&A Trail	Peninsula Farm Rd	\$2,279,063
Earleigh Heights Rd	Shared-use Path	Kinder Rd & Wembly Way	W Earleigh Heights Rd & Route 2	\$2,206,133
Fort Smallwood Rd	Shared-use Path	Edwin Raynor Blvd	Fort Smallwood Park	\$1,417,263



Anne Arundel Region 5 Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

Key Trail Intersections



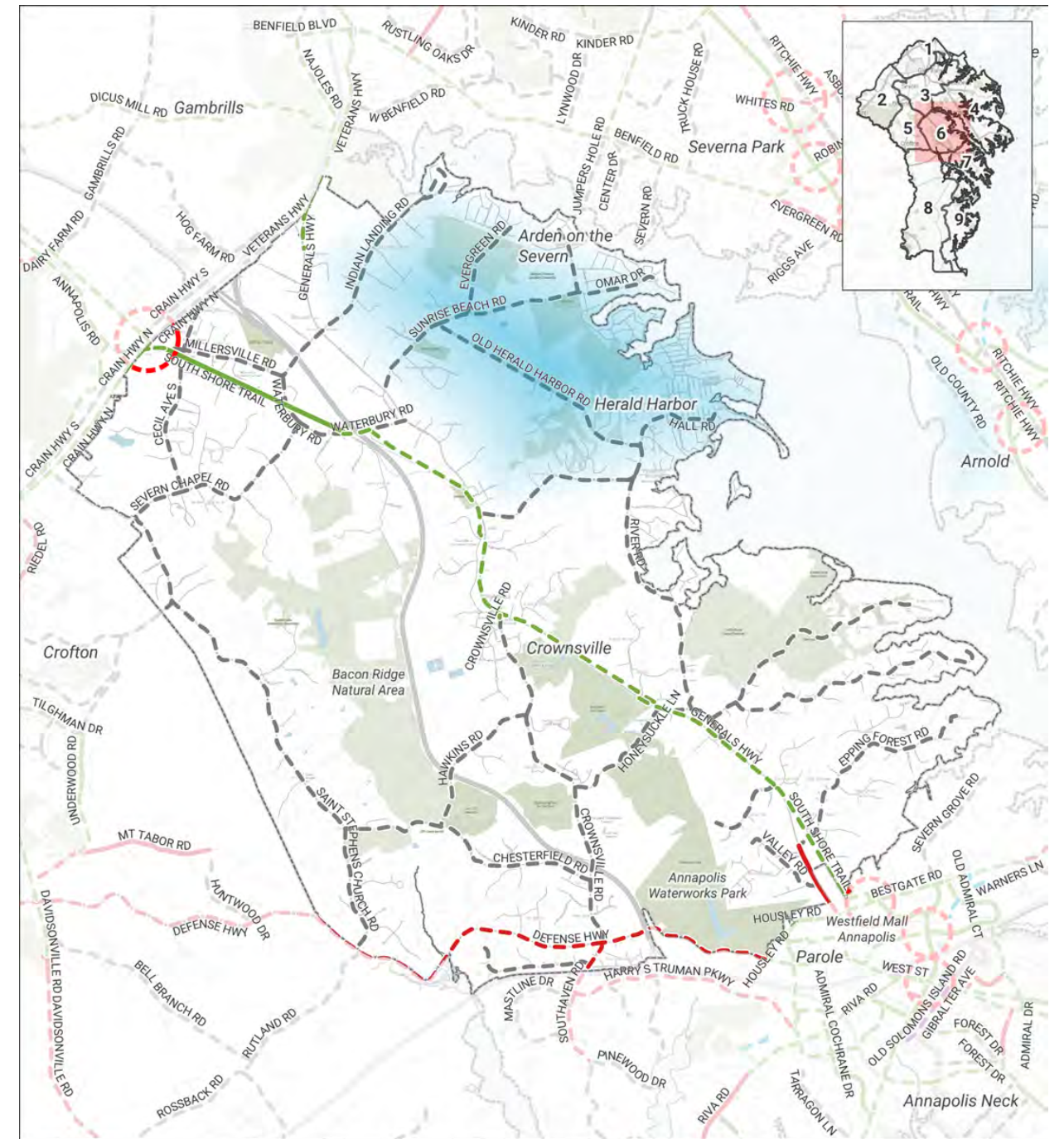
Map 29: Region 5 Proposed Bicycle Network.

Regional Planning Area 5

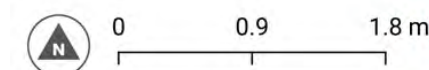
The RPA 5 projects that scored the highest in this Plan's prioritization process are as follows:

Street Name	Facility Type	Start	End	Estimated Cost
MD 3 (Crain Hwy)	Shared-use Path	North of Millersville Rd	Western County Line	\$34,782,505
MD 175 (Annapolis Rd)	Shared-use Path	Telegraph Rd	MD 3 (Crain Hwy)	\$2,607,248
Telegraph Rd	Shared-use Path	Old Mill Rd	MD 175 (MD 175 (Annapolis Rd))	\$8,159,044
Waugh Chapel Rd	Shared-use Path	MD 3 (Crain Highway S)	Maytime Dr	\$2,807,805
Davidsonville Rd	Shared-use Path	Conway Rd, MD 3 (Crain Hwy)	Barbara Swann Way	\$5,734,121
MD 450 (Defense Hwy)	Bike Lane	MD 3 (Crain Hwy)	N River Rd	\$2,057,473
Davidsonville Rd/Birdsville Rd	Bike Lane	MD 2 (Solomons Island Rd)	MD 450 (Defense Hwy)	\$3,153,211
Evergreen Rd/Johns Hopkins Rd	Protected Bicycle Lane	Waugh Chapel Way	Riedel Rd	\$576,148
Riedel Rd	Bike Lane	MD 3 (Crain Hwy)	Davidsonville Rd	\$1,818,470

*Projects are not in order of priority and need further study by the County



Anne Arundel Region 6
Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Other

- Areas for Future Study
- Key Trail Intersections



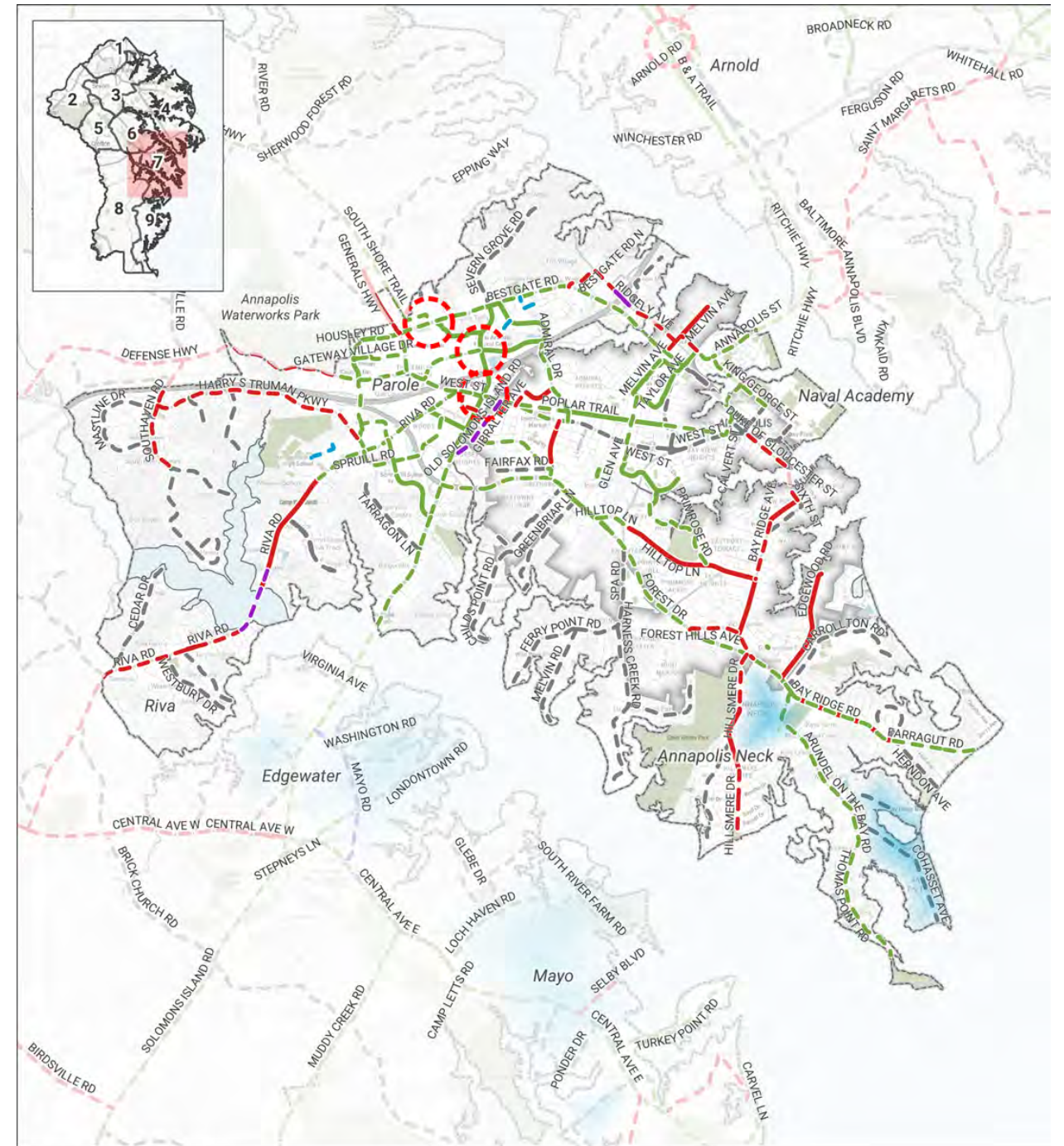
Map 30: Region 6 Proposed Bicycle Network.

Regional Planning Area 6

The top Region 6 projects determined by the *Walk & Roll Anne Arundel!* prioritization process include:

Street Name	Facility Type	Start	End	Estimated Cost
Generals Hwy	Shared-use Path	Belvoir Farms Rd	Carriage Hill Pkwy	\$2,607,248
MD 450 (Defense Hwy)	Bike Lane	MD 3 (Crain Hwy)	N River Rd	\$2,057,473
South Shore Trail	Shared-use Path			\$31,465,000
S Haven Rd	Bike Lane	MD 450 (Defense Hwy)	Pinewood Rd	\$941,509
Millersville Rd	Shared-use Path	MD 3 (Crain Hwy)	South Shore Trail	\$401,115
Crownsville Rd	Shared-use Path	Generals Hwy	Marbury Dr	\$200,558
Generals Hwy	Shared-use Path	Carriage Hill Pkwy	Bestgate Rd	\$2,734,875

*Projects are not in order of priority and need further study by the County



Anne Arundel Region 7
Proposed Bike Network

Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

Key Trail Intersections

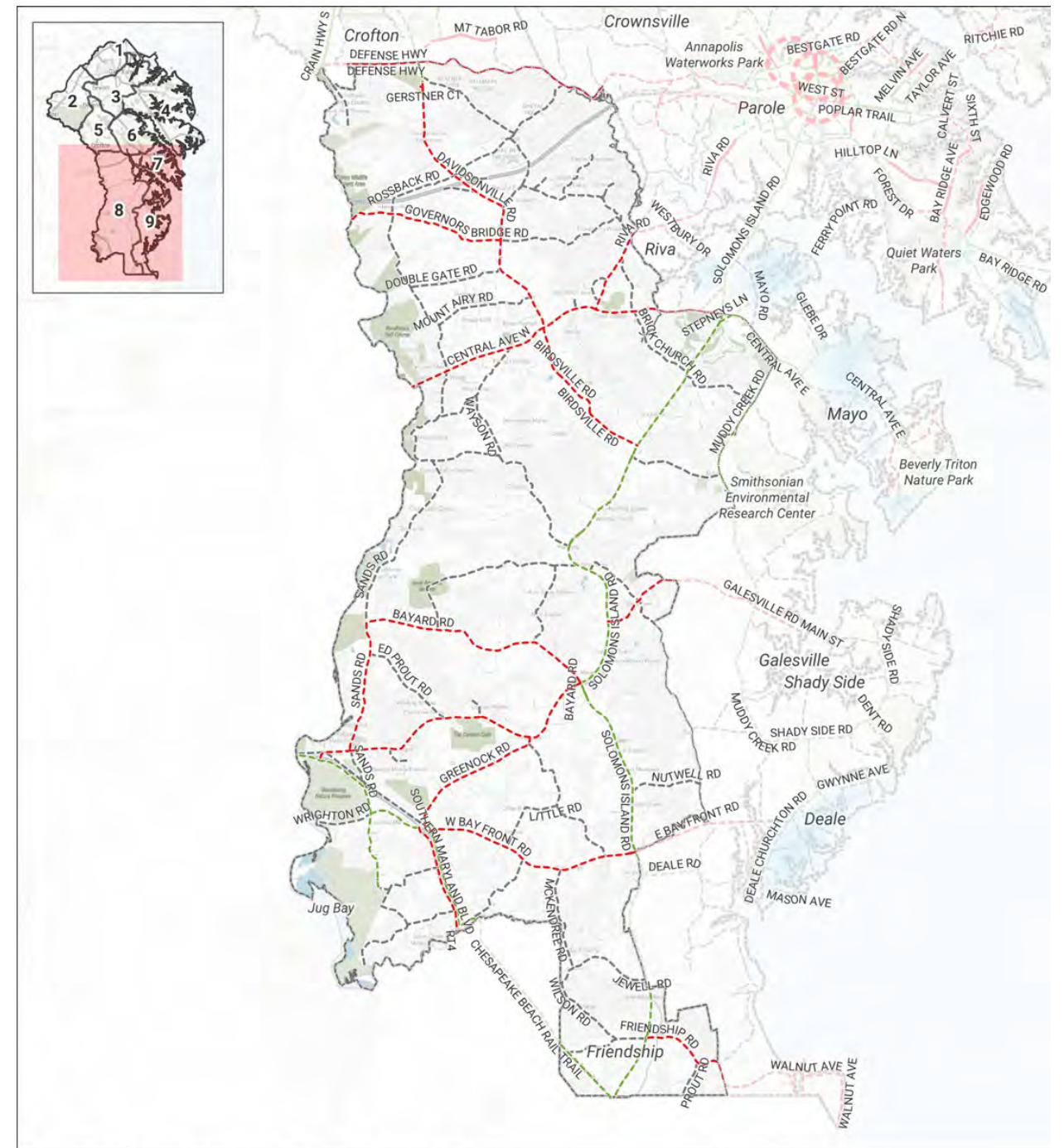


Map 31: Region 7 Proposed Bicycle Network.

Regional Planning Area 7

The top projects determined by the *Walk & Roll Anne Arundel!* prioritization process include:

Street Name	Facility Type	Start	End	Estimated Cost
West St	Shared-use Path	Generals Hwy	Holly Ave	\$1,203,345
Bestgate Rd	Shared-use Path	Generals Hwy	N Bestgate Rd	\$1,203,345
MD 2 (Solomons Island Rd)	Shared-use Path	MD 214 (Central Ave)	MD 260	\$37,148,719
Admiral Cochrane Dr	Separated Bike Lane	Riva Rd	Lake Shore Dr, MD 2 (Solomons Island Rd)	\$797,478
Housley Rd	Shared-use Path	Annapolis Ridge Rd including the traffic circle	Generals Hwy with gaps in between	\$1,002,788
Bay Ridge Rd	Shared-use Path	Bay Ridge Ave, Forest Dr, Hillsmere Dr	Catrina Ln, Farragut Rd	\$3,418,594
Rowe Blvd	Shared-use Path	N Bestgate Rd	Kirkley Rd	\$1,823,250
Spruill Rd & Shopping Center Drive	Shared-use Path	Riva Rd	Forest Dr	\$3,500,000



Anne Arundel Region 8
Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

Key Trail Intersections



Map 32: Region 8 Proposed Bicycle Network.

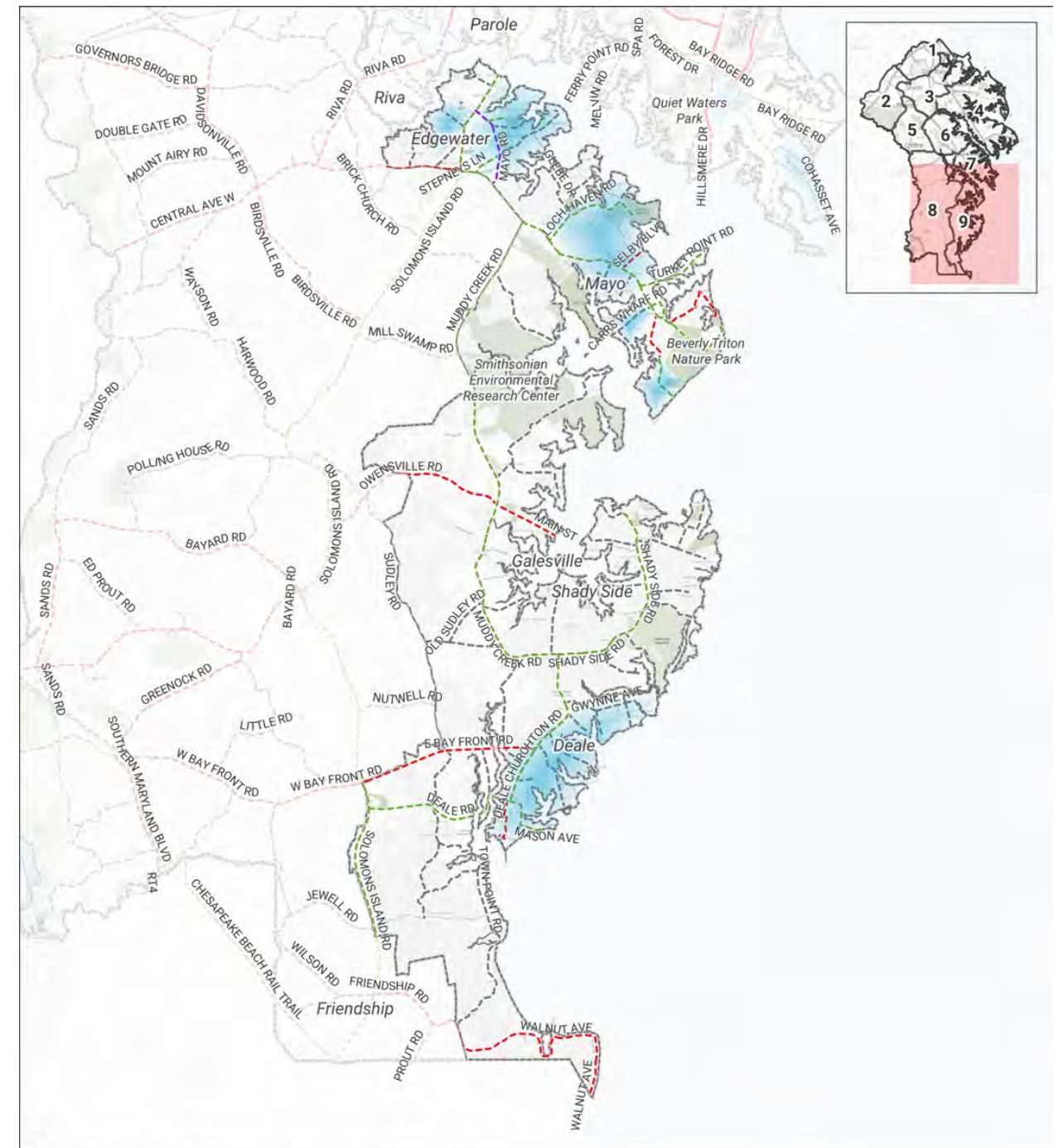
Regional Planning Area 8

The top projects determined by the *Walk & Roll Anne Arundel!* prioritization process include:

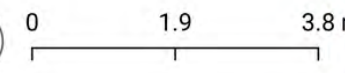
Street Name	Facility Type	Start	End	Estimated Cost
MD 2 (Solomons Island Rd)	Shared-use Path	MD 214 (Central Ave)	County Line	\$34,247,928
Davidsonville Rd	Shared-use Path	MD 3 (Crain Hwy)	Barbara Swann Way	\$5,615,610
MD 450 (Defense Hwy)	Bike Lane	MD 3 (Crain Hwy)	N River Rd	\$2,057,473
Stepneys Ln	Shared-use Path	MD 214 (Central Ave)	Mayo Rd	\$1,002,788
MD 214 (Central Ave E)	Shared-use Path	Mayo Rd	Rolling Rd	\$6,153,469
Mount Zion Marlboro Rd	Shared-use Path	Main St	Route 2 (MD 2 (Solomons Island Rd))	\$12,626,006
Birdsville Rd	Bike Lane	MD 214 (Central Ave)	Route 2 (MD 2 (Solomons Island Rd))	\$2,185,028
W Bay Front Rd	Bike Lane	Route 2 (MD 2 (Solomons Island Rd))	Southern Maryland Blvd/ Greenock Blvd	\$1,320,687

*Projects are not in order of priority and need further study by the County

The top projects determined by the *Walk & Roll Anne Arundel!* prioritization process include:



Anne Arundel Region 9
Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Separated Bike Lane
- Bike Lane

Areas for Future Study

Key Trail Intersections



Map 33: Region 9 Proposed Bicycle Network.

Regional Planning Area 9

Street Name	Facility Type	Start	End	Estimated Cost
MD 2 (Solomons Island Rd)	Shared-use Path	Admiral Cochrane Dr/ Lake Shore Rd	MD 214 (Central Ave)	\$34,247,928
Mayo Rd	Separated Bicycle Lane	MD 2 (Solomons Island Rd)	MD 214 (Central Ave E)	\$534,856
Stepneys Ln	Shared-use Path	MD 214 (Central Ave)	Mayo Rd	\$1,002,788
MD 214 (Central Ave E)	Shared-use Path	Mayo Rd	Rolling Rd	\$6,153,469
Shady Side Rd	Shared-use Path	Deale Churchton Rd	Hayes Rd	\$8,204,625
W Bay Front Rd	Bike Lane	Route 2 (MD 2 (Solomons Island Rd))	Southern Maryland Blvd/ Greenock Blvd	\$1,320,687
Deale Rd	Shared-use Path	Deale Churchton Rd	MD 2 (Solomons Island Rd)	\$16,251,721

*Projects are not in order of priority and need further study by the County

PERFORMANCE MEASURES

Tracking progress on the development of the County's active transportation network is important to ensuring the Plan's success. Knowing whether or not the County is on track to achieve its goals will be important; if it is not, modifications to funding and implementation strategies may be necessary. Achieving goals will both help the County maintain its progress and communicate success to the public.

Community members voiced preferences for goals through the public engagement process described in Chapter 2. To ensure that *Walk & Roll Anne Arundel!* meets these six goals, the Plan contains measurable performance measures, shown in Table 14. These will help the County track the Plan's development to ensure that the community will benefit from them. Some of these indicators are measured year by year, while others are projected over the next 25 years as long-range projects. While it may take the full 25 years to fully accomplish many measures, it is recommended that the County establish milestone targets on the way to the 25-year measures. Setting 10- to 15-year targets are necessary to keep pace with the recommended project development timelines. Targets should ensure that the County is meeting federal, state, and regional funding requirements.

The recommended performance measures are based on best practices, including the Institute of Transportation Engineer's Putting Active Transportation Performance Measures into Practice and FHWA's Guidebook for Developing Pedestrian and Bicycle Performance Measures.⁴⁶

Table 14: Plan Performance Measures.

Category	Performance Measure
Connectivity and Access	One bicycle and one pedestrian capital project per year is constructed for smaller jurisdictions (population 30,000 or less) and two of each project type is constructed for larger jurisdictions (population 30,000 or more).
	The mileage of new shared-use paths, separated bicycle facilities, standard bicycle lanes, sidewalks, trails, and sidewalks increases 5% each year. By 2040, 75% of residents live within ¼ mile of protected walking and bicycling facilities.
Equity	One bicycle capital project and one pedestrian capital project per year is constructed in Vulnerable Population Areas. By 2040, 75% of residents in Vulnerable Population Areas live within ¼ mile of protected walking and bicycling facilities.
	By 2030, decrease fatal and serious injury crashes for all road users by 50%*. Increase the percentage of existing walking and bicycling facilities in good condition by 2.5% per year.
Safety	By 2040 provide sidewalks to 85% of County schools and provide separated bicycle infrastructure to 65% of County schools. Safe walking and bicycling routes should be constructed within a 1-mile radius of elementary schools and a 1.5-mile radius of middle and high schools.
	By 2050, 50% of the County's population will be able to reach the County's Targeted Development, Redevelopment and Revitalization Areas within 30 minutes by foot, bicycle, or transit using protected pedestrian or bicycle facilities.
Strategic Growth and Mode Shift	By 2050, 75% of County residents will be able to access at least three destinations that serve daily needs (such as schools, grocery stores, health care facilities, or transit stops) within a two mile bicycle ride or 1 mile walk. By 2050, the number of trips made by walking, bicycling, and transit will increase five-fold.
	Vehicle miles traveled per capita will decrease by 10% every ten years.
Investment	Funding for active transportation projects will increase by 25% every decade until 2040; on average, funding will increase by 2.5% every year.

*Anne Arundel County's Vision Zero Plan established updated baseline numbers and interim metrics to achieve the *Move Anne Arundel!* goal of reaching zero serious injuries and fatalities on the County's roadways by 2040. This performance measure aligns with the Vision Zero Plan.

⁴⁶ https://www.fhwa.dot.gov/environment/bicycle_pedestrian/publications/performance_measures_guidebook/



Chapter 5: Designing Streets for Walking and Rolling

Figure 41: Annapolitan Fourth of July. Photo credit: Michael mangold. Photo acquired on Flickr.com

The details of street design are paramount to ensuring that people traveling on foot, bicycle, wheelchair, or other non-motorized method are safe, comfortable, and well connected to the places they need to go.

This chapter includes recommendations for the design of Anne Arundel County’s streets using land use as the context as opposed to solely relying on functional classification. Solely using functional classification does not take land use and other factors that impact the desire to ride or walk to destinations into account. *Walk & Roll Anne Arundel!* establishes recommendations for various typologies, from rural highways to suburban arterials to commercial corridors.

While recommended street features and their widths are depicted in the images below, the County should remain flexible in street design features to either provide enhanced conditions for bicycling and walking (e.g., maximum widths for bicycle infrastructure, sidewalks, or landscaped buffers) or provide feasible infrastructure design in constrained areas. To illustrate street design that may be considered in the latter scenario, constrained concepts are provided for select street typologies. These concepts may be considered on streets with a lack of ROW or other significant constraints.

It should also be noted that the State’s SUP width requirement is 10 feet on State right-of-way or constructed using State funding. If this width cannot be met, a design waiver is required.

These preliminary concepts are for planning purposes only. Field verification, site condition assessments, engineering analysis, and design are necessary prior to implementing recommendations contained herein.

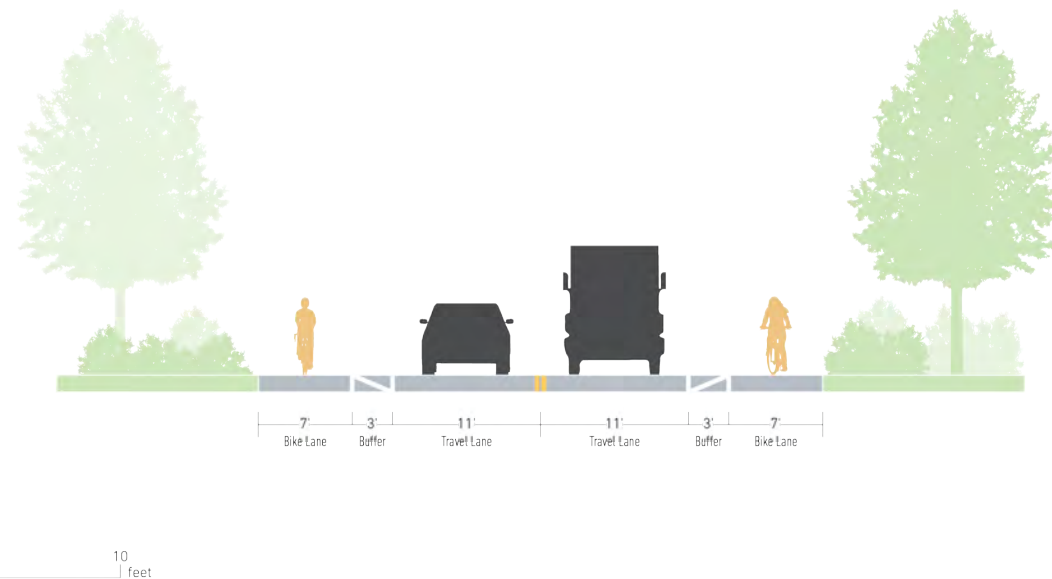


Figure 42: Recommended street design for Rural Collectors.

47 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwas18077.pdf

48 <https://www.fhwa.dot.gov/publications/research/safety/pedbike/05085/chapt14.cfm>

49 <https://roads.maryland.gov/mdotsha/pages/index.aspx?PagelId=409>

Street Design Concepts Per Street Typology

Rural Collector

Design Considerations

- On roadways with bicycle lanes, shoulder width should be 10'. The buffer within this 10' should be no less than 3'.
 - » Sources: FHWA Bikeway Selection Guide⁴⁷ and upcoming AASHTO Bicycle Design Guidelines
- Add rumble strips between the bicycle lane and travel lane to alert drivers and bicyclists when veering out of their path. Where bicyclists cross out of the bicycle lane, discontinue rumble strips and add vertical posts.⁴⁸
- On state-owned arterials, lane width may be reduced to 11' per the *MDOT SHA Access Manual* when speed limit is less than 40 mph; on collectors, lane width may be 11'.⁴⁹ The County may ask for exceptions to this standard in order to seek 11' lanes on roadways with design speeds of more than 40 mph.
- In addition to a bicycle lane on the shoulder, a shared-use path parallel to the roadway may be constructed to provide a separated pathway for pedestrians and bicyclists. The recommended SUP width is 11', plus 2' graded shoulders.

Suburban Collector

Design Considerations

- Per the “Anne Arundel County Design Manual”⁵⁰, sidewalks should be provided unless the following requirements are met:
 - » The projected Average Daily Traffic (ADT) on the roadway for a 25-year time frame is less than 400, and
 - » The average lot size is greater than 30,000 SF, and
 - » There are no pedestrian generators (schools, commercial areas, parks, transit, etc.) within 1.5 miles of any entrance to the development.
- On roadways with design speeds of 30 mph – 40 mph, separated bicycle lane, buffered bicycle lanes, or shared-use paths, should be provided.⁵¹ Buffers should be 3' wide. Bicycle lanes should be 6' wide.⁵² SUPs should be 11' wide.
- Standard bicycle lanes, of at least 5' (excluding curb and gutter pan) and no buffer, may be provided on roadways with a design speed of less than 30 mph.⁵³
- Per the County Design Manual, street tree planting strips should be a minimum of 5'.
- Recommended land use categories or policy areas include: mixed-use, TOD policy area, Village Center, Town Center, Small Business District, Critical Economic.⁵⁴



Figure 43: Recommended street design, and alternative street design in constrained areas, on Suburban Collectors.

50 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

51 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwas18077.pdf

52 <https://nacto.org/publication/urban-bikeway-design-guide/bike-lanes/conventional-bike-lanes/>

53 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwas18077.pdf

54 <https://www.aacounty.org/departments/planning-and-zoning/long-range-planning/general-development-plan/>

2-Lane Suburban Collector

Design Considerations

- Per the “Anne Arundel County Design Manual”⁵⁵, sidewalks should be provided unless the following requirements are met:
 - » The projected Average Daily Traffic (ADT) on the roadway for a 25-year time frame is less than 400, and
 - » The average lot size is greater than 30,000 SF, and
 - » There are no pedestrian generators (schools, commercial areas, parks, transit, etc.) within 1.5 miles of any entrance to the development.
- On roadways with design speeds of 30 mph – 40 mph, separated bicycle lane, buffered bicycle lanes, or shared-use paths, should be provided.⁵⁶ Buffers should be 3’ wide. Bicycle lanes should be 6’ wide.⁵⁷ SUPs should be 11’ wide.
- Standard bicycle lanes, of at least 5’ (excluding curb and gutter pan) and no buffer, may be provided on roadways with a design speed of less than 30 mph.⁵⁸



Figure 44: Recommended street design on 2-lane Collectors in suburban areas.

Commercial Corridor Arterial

Design Considerations

- Shared-use paths should be 11’.⁵⁹
- Per the “Anne Arundel County Design Manual”⁶⁰, sidewalks should be provided unless the following requirements are met:
 - » The projected Average Daily Traffic (ADT) on the roadway for a 25-year time frame is less than 400, and
 - » The average lot size is greater than 30,000 SF, and
 - » There are no pedestrian generators (schools, commercial areas, parks, transit, etc.) within 1.5 miles of any entrance to the development.
- Sidewalks should be 8’-10’ wide in areas with designated land use categories of mixed-use, TOD, Town Centers, Village Centers, Small Business Districts, Critical Economic. Sidewalks should be 6’ wide in other zoning districts or *Plan2040* policy areas.⁶¹
- Recommended land use or policy areas per *Plan2040* include: mixed-use; TOD policy area; Village Center; Town Center; Small Business District.⁶²
- Per County Design Manual, street tree planting strips should be a minimum of 5’.
- Features such as bicycle racks, scooter corrals, benches, pedestrian-scale lighting, and bike share stations should be provided.
- Exceptions may be requested to MDOT SHA’s Access Manual, which directs 12’ travel lanes on arterials (and collectors with speeds greater than 40 mph).⁶³
- On county-owned roads, refer to the County’s Typical Section requirements.⁶⁴



Figure 45: Recommended street design on Commercial Corridor Arterials with three travel lanes in each direction.

55 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

56 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwas18077.pdf

57 <https://nacto.org/publication/urban-bikeway-design-guide/bike-lanes/conventional-bike-lanes/>

58 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwas18077.pdf

59 <https://nacto.org/publication/urban-bikeway-design-guide/>

60 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

61 <https://nacto.org/publication/urban-street-design-guide/street-design-elements/sidewalks/>

62 <https://www.aacounty.org/departments/planning-and-zoning/long-range-planning/general-development-plan/>

63 <https://roads.maryland.gov/mdotsha/pages/index.aspx?PageId=409>

64 <https://www.aacounty.org/departments/publicworks/orange-notices/DPW-2o-03.pdf>

Boulevard

Design Considerations

- Boulevards provide a high degree of separation between high speed roadways and non-vehicular activity to include walking and bicycling facilities and commercial and residential areas.
- Per the “Anne Arundel County Design Manual”⁶⁵, sidewalks should be provided unless the following requirements are met:
 - » The projected Average Daily Traffic (ADT) on the roadway for a 25-year time frame is less than 400, and
 - » The average lot size is greater than 30,000 SF, and
 - » There are no pedestrian generators (schools, commercial areas, parks, transit, etc.) within 1.5 miles of any entrance to the development.
- On roadways with design speeds of 30 mph – 40 mph, separated bicycle lane, buffered bicycle lanes, or shared-use paths, should be provided.⁶⁶ Buffers should be 3’ wide. Bicycle lanes should be 6’ wide.⁶⁷ SUPs should be 11’ wide. A boulevard median with SUP should be designed to avoid crashes at intersections.⁶⁸
- At intersections, implement traffic calming measures such as smaller turning radii, and increase visibility of people walking and bicycling. Frontage roads should be stop-controlled unless cross traffic does not have enough room to pass.⁶⁹
- Implement access management strategies.⁷⁰
- Per the County Design Manual, street tree planting strips should be a minimum of 5’.



Figure 46: Recommended street design, and alternative street design in constrained areas, on Boulevards.

Neighborhood Arterial

Design Considerations

- Per the “Anne Arundel County Design Manual”⁷¹, sidewalks should be provided unless the following requirements are met:
 - » The projected Average Daily Traffic (ADT) on the roadway for a 25-year time frame is less than 400, and
 - » The average lot size is greater than 30,000 SF, and
 - » There are no pedestrian generators (schools, commercial areas, parks, transit, etc.) within 1.5 miles of any entrance to the development.
- On roadways with design speeds of 30 mph – 40 mph, separated bicycle lane, buffered bicycle lanes, or shared-use paths, should be provided.⁷² Buffers should be 3’ wide. Bicycle lanes should be 6’ wide.⁷³ SUPs should be 11’ wide.
- Per the County Design Manual, street tree planting strips should be a minimum of 5’.
- Exceptions may be requested to MDOT SHA’s *Access Manual*, which directs 12’ travel lanes on arterials (and collectors with speeds greater than 40 mph).⁷⁴
- On County-owned roads, refers to the County’s Typical Section requirements.⁷⁵



Figure 47: Recommended street design, and alternative street design in constrained areas, on Neighborhood Arterials.

65 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

66 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf

67 <https://nacto.org/publication/urban-bikeway-design-guide/bike-lanes/conventional-bike-lanes/>

68 nacto.org/publication/urban-street-design-guide/streets/boulevard/

69 nacto.org/publication/urban-street-design-guide/streets/boulevard/

70 nacto.org/publication/urban-street-design-guide/streets/boulevard/

71 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

72 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf

73 <https://nacto.org/publication/urban-bikeway-design-guide/bike-lanes/conventional-bike-lanes/>

74 <https://roads.maryland.gov/mdotsha/pages/index.aspx?PageId=409>

75 <https://www.aacounty.org/departments/public-works/orange-notice/DPW-20-03.pdf>

Main Street

Design Considerations

- Sidewalks should be 8’.
- Per the “Anne Arundel County Design Manual”⁷⁶, sidewalks should be provided unless the following requirements are met:
 - » The projected Average Daily Traffic (ADT) on the roadway for a 25-year time frame is less than 400, and
 - » The average lot size is greater than 30,000 SF, and
 - » There are no pedestrian generators (schools, commercial areas, parks, transit, etc.) within 1.5 miles of any entrance to the development.
- Bicycle lane design:
 - » On roadways with design speeds of 30 mph – 40 mph, buffered bicycle lanes should be provided, buffer width should be 3’.⁷⁷ Bicycle lanes should be 6’ wide.⁷⁸
 - » Standard bicycle lanes, of at least 5’ (excluding curb and gutter pan) and no buffer, may be provided on roadways with a design speed of less than 30 mph.⁷⁹
- Per the County Design Manual, street tree planting strips should be a minimum of 5’.
- Parallel or angled parking may be provided adjacent to travel lanes. Bicycle lanes and sidewalks should be located on the other side of parking lanes, separated from travel lanes. Parking spaces should be separated sufficiently at intersections and driveways to increase driver and bicyclist site distance and prevent collisions with bicyclists traveling in the bicycle lane.
- Recommended land use or policy areas per *Plan2040* includes: mixed-use, TOD policy area, Village Center, Town Center, Small Business District, Critical Economic.⁸⁰
- Street features should be added, including bicycle parking, benches, and street trees/landscaping.

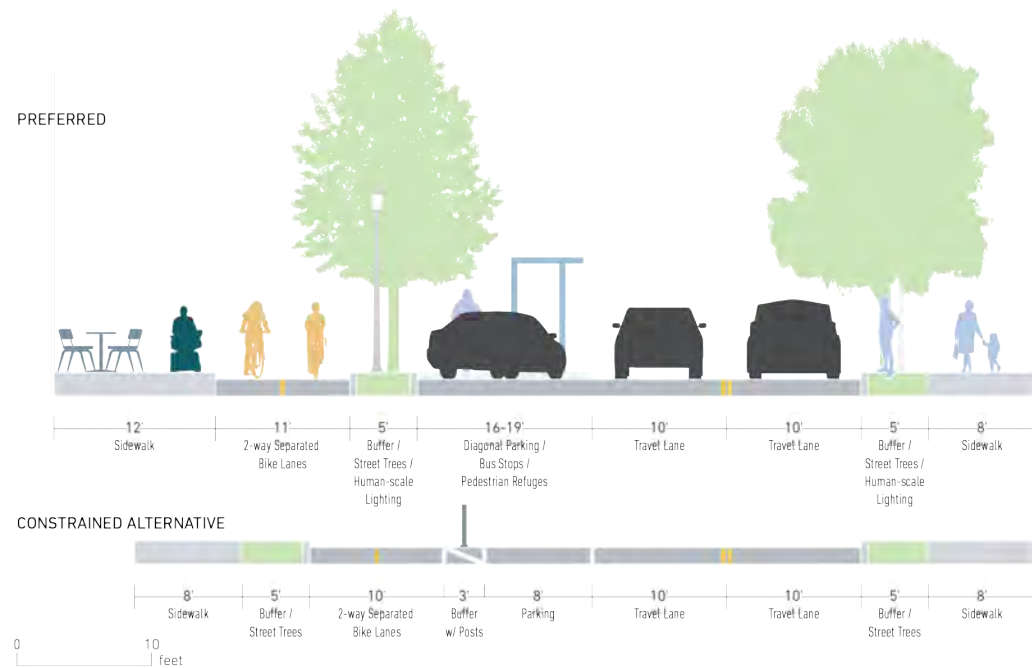


Figure 48: Recommended street design, and alternative street design in constrained areas, on Main Streets.

76 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

77 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf

78 <https://nacto.org/publication/urban-bikeway-design-guide/bike-lanes/conventional-bike-lanes/>

79 https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf

80 <https://www.aacounty.org/departments/planning-and-zoning/long-range-planning/general-development-plan/>

Bi-Directional Cycle Track Option

Design Considerations

- A bi-directional cycle track is best:
 - » On streets with few conflicts (driveways or cross-streets) on one side of the street. It is also a good option for one-way streets with bus stops, in order to reduce space required to reduce conflicts between transit riders and bicyclists;
 - » When space is unavailable for a one-way cycle track on both sides of the street.
 - » On streets with more destinations on one side of the street, reducing the need for bicyclists to cross the street.
 - » On Streets with extra right-of-way on one side;
 - » On high-stress streets with multiple travel lanes, higher traffic volumes, and higher travel speeds where physical separation between motorists and bicyclists is required for a low-stress route. Separated bicycle lanes and cycle tracks on both sides of the street should also be considered.⁸¹
- Cycle track width should be 12’. In constrained areas, where ROW or terrain pose feasibility constraints, minimum width is 8’.⁸²
- The buffer between a cycle track and travel lanes should be 3’ and may include bollards, concrete barriers, planters, or other crash-resistant protective infrastructure.⁸³



Figure 49: Recommended cross section for a street with a bi-directional separated cycle track.

81 nacto.org/publication/urban-bikeway-design-guide/cycle-tracks/two-way-cycle-tracks/

82 nacto.org/publication/urban-bikeway-design-guide/cycle-tracks/two-way-cycle-tracks/

83 nacto.org/publication/urban-bikeway-design-guide/cycle-tracks/two-way-cycle-tracks/

Street Design Concepts in Town Centers

The project team recommended street design concepts in the three Town Centers to identify roads that may be candidates for “road diets”. A road diet, or lane reconfiguration, involves reducing the number of travel lanes on a street. The benefits of a road diet include reduced crashes for a low cost. Road diets are described in more detail in chapter 3.

Glen Burnie

COMMERCIAL SUBURBAN ARTERIAL: MD 648 (West of 8th Avenue)

This street design could be applied to MD 648 (West of 8th Avenue), a State-owned minor arterial with an AADT of 16,401 and a speed limit of 40 mph. MD 648 at this location is in one of the County’s Transit-Oriented Development Policy Areas.

The existing street has four travel lanes of 12’, a center lane, and a right turn lane. One side of the street has a 5’ sidewalk.

A reconfigured street could have no right turn only lanes and 11’ travel lanes; the remaining space would leave room for separated bicycle lanes on both sides of the street. The AADT of 16,401 supports a road diet recommendation.

In this Transit-Oriented Development Policy Area, sidewalks should also be upgraded to 8-10’ widths and be provided continuously throughout the corridor. Pedestrian and bicyclist crossings should also be improved in this area.

Future design could also provide a safe and seamless transition to the B&A Trail. ROW and above ground utilities constrain the potential for a sidepath option.



Figure 50: A Google street-view of the existing conditions of MD 648, West of 8th Avenue. Photo from Google Maps.



Figure 51: Recommended cross section for commercial suburban arterials.

MAIN STREET: MD 648 (Baltimore Annapolis Boulevard) (Between 8th Avenue and MD 2 (Crain Highway))

This street design could be applied to MD 648 from 8th Avenue to MD 2, a State-owned minor arterial with an AADT of 16,401 and a speed limit of 30 mph. MD 648 at this location is in one of the County’s Transit-Oriented Development Policy Areas. AADT is 16,401 with peak hour traffic volumes of 636 northbound and 612 southbound in the morning and 569 northbound and 702 southbound in the afternoon and evening.

The current cross section for this segment of MD 648 has two 17’ travel lanes and 5’ sidewalks on both sides of the street.



Figure 52: A Google street-view of the existing conditions of MD 648, between 8th Avenue and MD 2. Photo from Google Maps.

The recommended cross section follows a Main Street typology with two 11’ travel lanes, 8’ sidewalks, 5’ bicycle lanes, and 2’ buffers between the bicycle lanes and the travel lanes.

There are multiple driveway and side street crossings in this segment, warranting the need for high-visibility crosswalks such as rectangular rapid flashing beacons. Access management could be employed to reduce driveways and conflict areas.

Bus service and stops along MD 648 is also a consideration. The County, SHA, and transit agency will need to design safe and accessible bus stops. The renderings below show options for grade-separated bicycle lanes and sidewalks at bus stops, or bicycle/pedestrian mixing zones at stops.

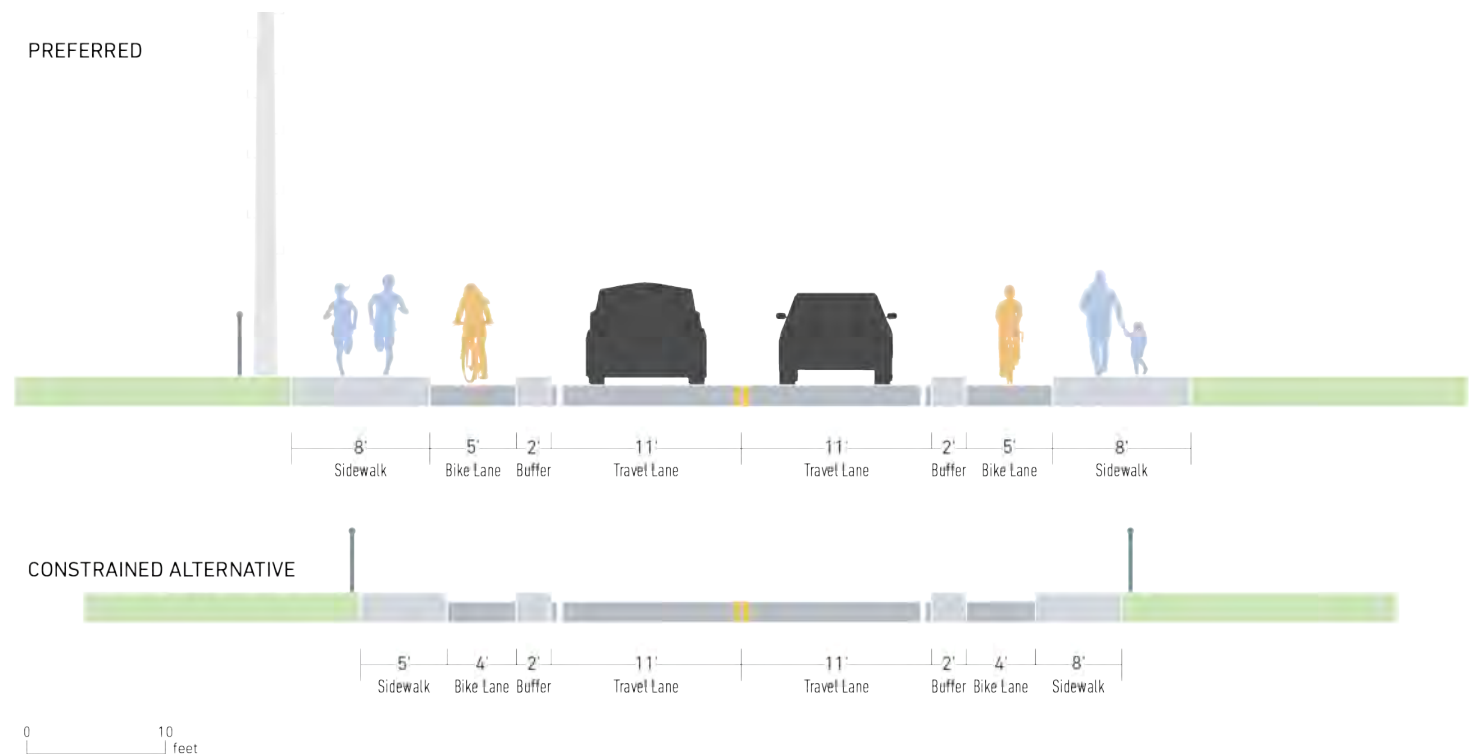
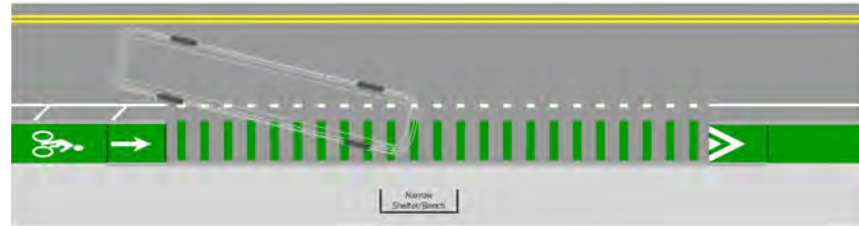


Figure 53: Recommended street design, and alternative for street design in constrained areas, on Main Streets.

At-Grade Separated Bike Lane and Sidewalk

BUS/BIKE MIXING ZONE BUS STOP CONFIGURATION



CONSTRAINED BUS STOP CONFIGURATION



BIKE/PED MIXING ZONE BUS STOP CONFIGURATION

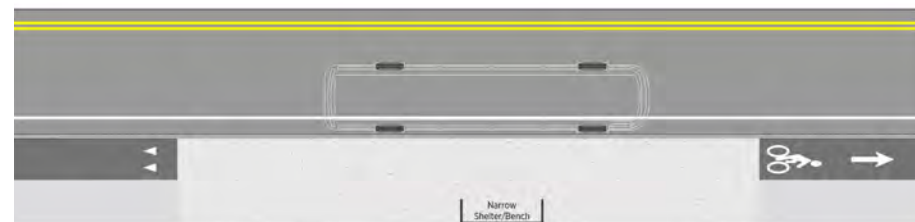


Figure 54: Multiple preferred bus stop configurations for an at-grade separated bike lane and sidewalk.

Table 15: Considerations for Bus Stop Configurations.

Constrained Bus Stop Configuration Examples	Considerations
<p>Option 1 : At-Grade Separated Bike Lane and Sidewalk. If grade-separated bike lane, transition to street level at bus stop. Transition from vertical separation (curb or flex post) to painted buffer.</p>	<ul style="list-style-type: none"> Maintains existing sidewalk boarding Does not accommodate an 8' ADA accessible boarding/alighting zone
<p>Option 2: Grade-Separated Bike Lane and Sidewalk. Bike lane elevated to sidewalk level at bus stop. Bikes yield to pedestrians at crosswalks.</p>	<ul style="list-style-type: none"> Elevates bike lane to sidewalk level to create 8' boarding/alighting zone Pedestrians must cross bike lane
<p>Option 3: Bike/Pedestrian Mixing Zone. Bike lane elevated to sidewalk level at bus stop. Bikes yield to pedestrians in mixing zone.</p>	<ul style="list-style-type: none"> Elevates bike lane to sidewalk level mixing zone Continuous pedestrian path; bikes and peds share space

MAIN STREET: MD 648 (Baltimore Annapolis Boulevard) MD 2 (Ritchie Highway)

The third recommended configuration for MD 648 in Glen Burnie is between Crain Highway and MD 2 (Ritchie Highway), also a State-owned minor arterial in a Transit-Oriented Development Policy Area. The AADT remains at 16,401 with a speed limit of 30 mph.

The street contains an 11' parking lane and 11' travel lane on one side, an 11' turn lane, and two 11' travel lanes in the opposite direction. On average, 9' sidewalks (with variability in the width) exist on both sides of the road.

The recommendation is to design a road diet by removing outer lanes, which currently function as parking lanes during off-peak hours, and consolidating parking (with 8' lanes) to one side of the street. A separated bicycle lane should be constructed.

Design should consider the pinch point at the median. In addition, the SHA Access Manual call for 12' center turn lanes; however, 10' center turn lanes are appropriate in urban environments such as this.



Figure 55: The existing cross section for MD 648, between MD 2 and MD 3. Photo from Google Maps.

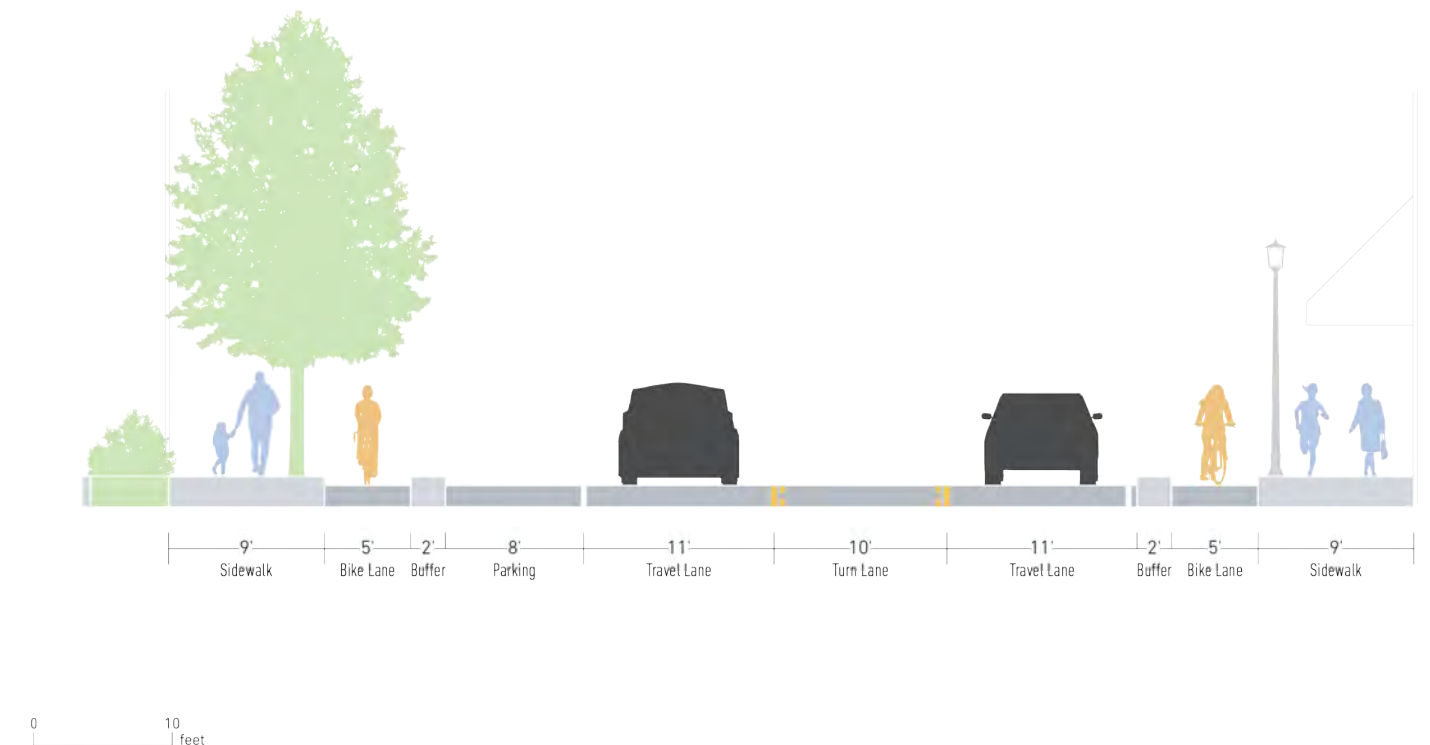


Figure 56: Recommended street design for the main street corridor of MD 648 that runs between MD 2 and MD 3.

Odenton

COMMERCIAL CORRIDOR ARTERIAL: MD 175

MD 175 in the Odenton area is a state-owned principal arterial in the County's Town Center Development Policy Area. Much of this portion of the corridor is also within the Transit-Oriented Development Policy Area Overlay. The AADT is 28,181 and the speed limit is 40 mph. The current cross section has two 11' travel lanes in each direction, a 12' center turn lane and a 3' sidewalk on one side.

The recommended cross section removes the center turn lane to reallocate space for a shared-use path with a buffer to replace the current sidewalk.

Challenges with this design include above ground utilities, and steep grades on either side of MD 175, a bridge over the rail corridor. Design should also investigate providing an 11' shared-use path on the south side of MD 175, and keeping a sidewalk on the north side.



Figure 57: Recommended street design for MD 175, designated as a commercial corridor arterial.

Parole

BOULEVARD: BESTGATE RD

Bestgate Road in Parole is a County-owned minor arterial. Much of the eastern portion of this corridor is within the *Plan2040* Town Center Development Policy Area consistent with the Parole Town Center. In addition, much of that portion of the corridor is within the Transit-Oriented Development Policy Area overlay. The western portion of the corridor is within the Neighborhood Preservation Development Policy Area. The 2019 AADT was 25,811 and the speed limit is 35. Currently, there are two 11' travel lanes in each direction, a 14' center planting strip, and 4' sidewalks with a 2' buffer on both sides.

The recommended cross section removes the center turn lane to reallocate space for a shared-use path with a buffer to replace the current sidewalk.

With minimal ROW constraints on one side, the recommendation is to construct a 12' shared-use path with a 2' buffer on the northern side of Bestgate Road and upgrade the sidewalk on the southern side to 6'.



Figure 58: The existing cross section of Bestgate Road. Photo from Google Maps.

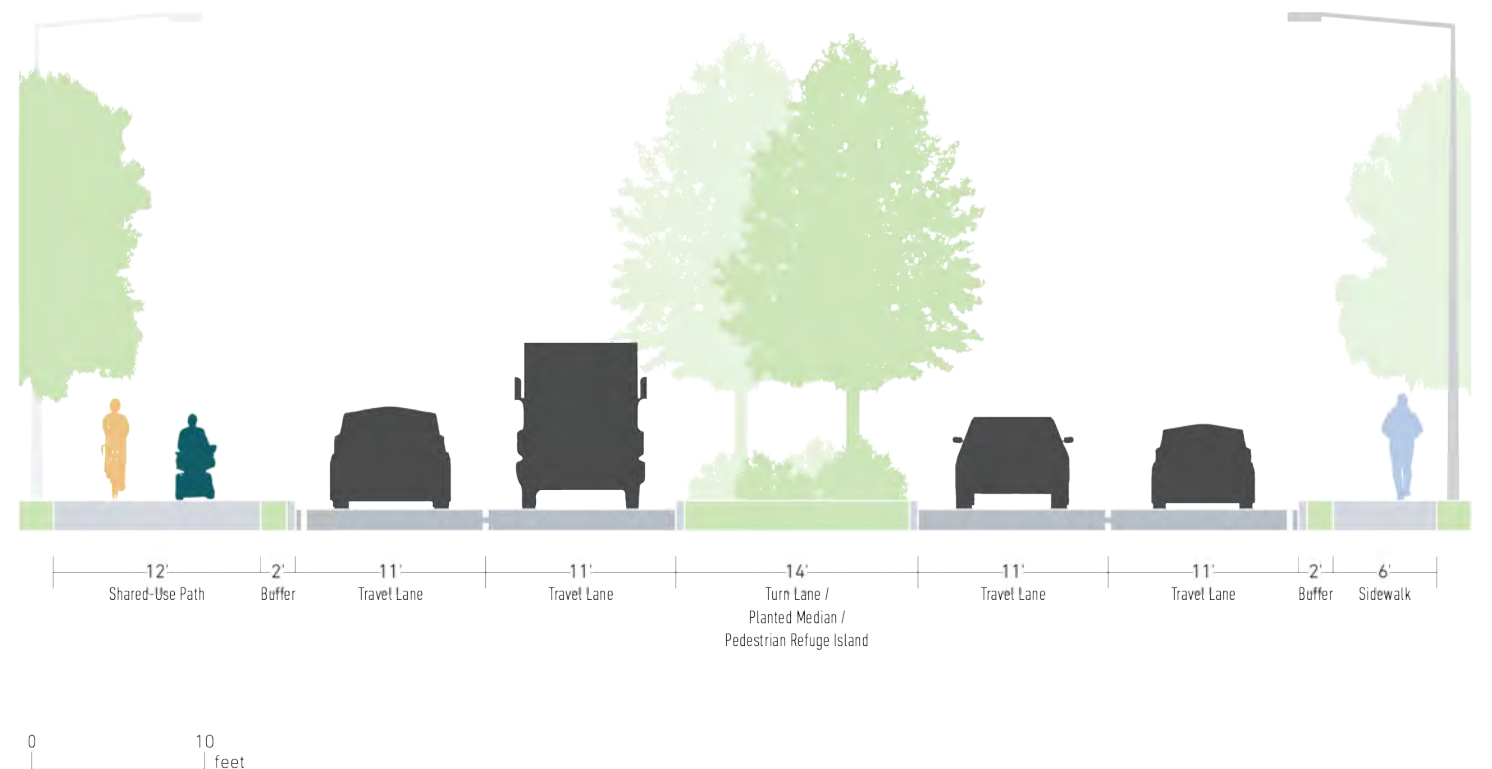


Figure 59: Recommended boulevard street design for Bestgate Road.

NEIGHBORHOOD ARTERIAL: HOUSLEY ROAD

Housley Road is also a County-owned minor arterial and is within the *Plan2040* Town Center Development Policy Area. Much of the corridor is also within the Transit-Oriented/ Development Policy Area. Policy Area defined as Transit-Oriented/Town Center to Neighborhood Preservation. The speed limit is 30 mph and AADT is unknown.

The existing street has two 12' travel lanes in each direction and a 4' sidewalk with a 4' buffer on one side.

The preferred cross section proposes completing gaps in the shared-use path on the western side and upgrading SUP width from 8' to 11'. The sidewalks on the eastern side of Housley Street should also be continuous, and existing sidewalks should be widened to 6'. The four travel lanes should be narrowed to 11'. To manipulate during the design phase, variable ROW and sidewalk setbacks exist throughout the corridor.



Figure 60: Existing cross section of Housley Road. Photo from Google Maps.

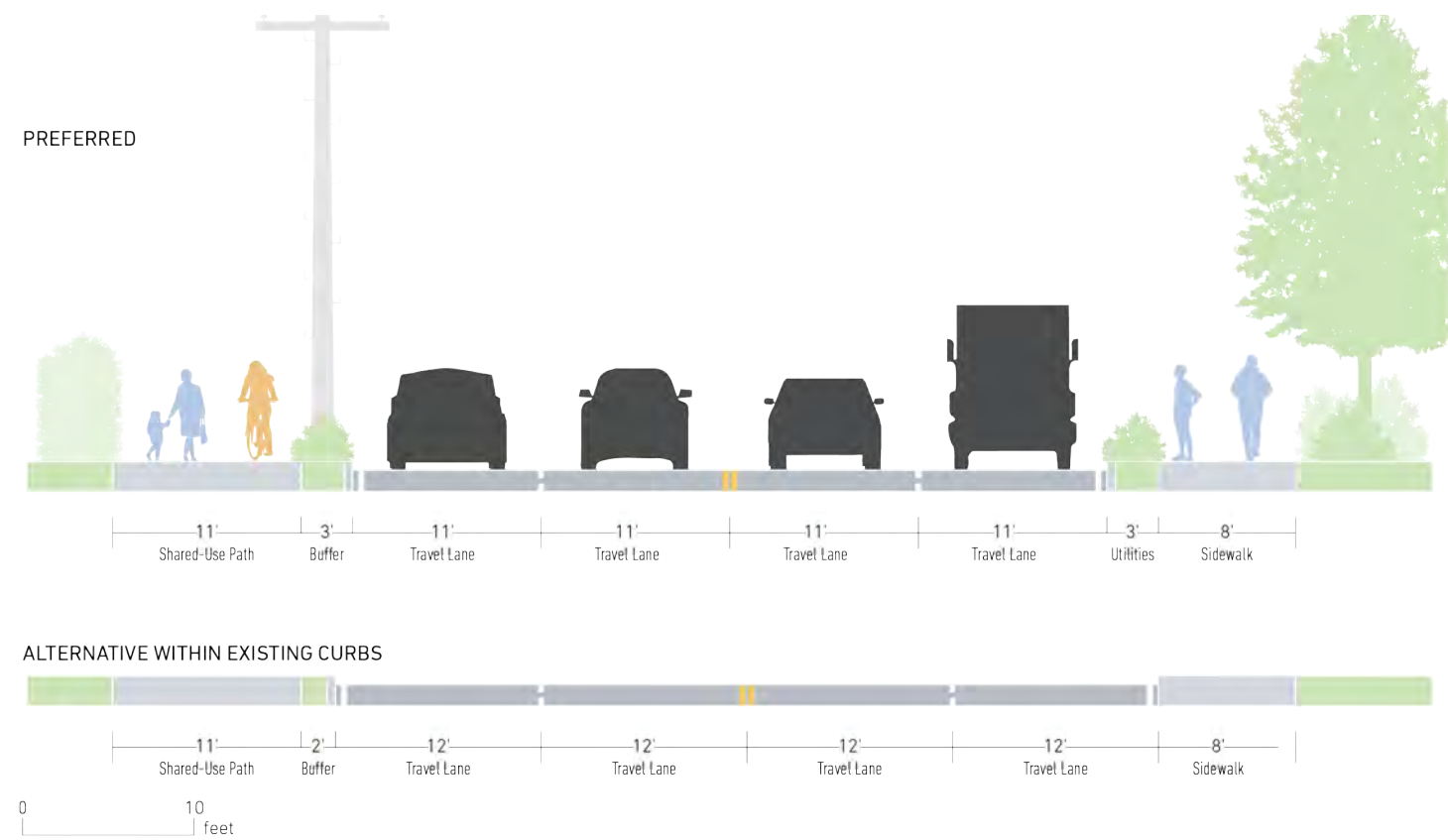


Figure 61: Preferred street design, with alternative option for the existing curbs, for Housley Road.

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Chapter 6: Policy and Design Challenges



Figure 62: A group of cyclists enjoying a group ride together.

Decades of auto-oriented transportation and land use planning and design throughout the United States have left fewer choices and more dangerous conditions for people who travel on foot, bicycle, wheelchair, and other nonmotorized modes. Over the past twenty years, higher standards in bicycle and pedestrian planning have evolved. While Anne Arundel County and the State of Maryland have made progress updating policies to standardize the design and construction of multimodal transportation facilities, there remain deficiencies in transportation planning, policies, programs, and procedures.

Furthermore, the County’s land use policies and regulations still emphasize vehicular travel. Improvements to County and State processes can ensure that high-quality walking and rolling infrastructure is seamlessly built throughout the County. To support a more balanced approach to transportation and land use planning the team conducted a high-level policy review.

The project team reviewed current County planning and regulatory documents, including:

- *Move Anne Arundel!* the County’s first Transportation Functional Master Plan⁸⁴
- Anne Arundel County’s “Complete Streets Policy”⁸⁵
- Maryland State Highway Administration’s “Complete Streets Policy”⁸⁶
- Anne Arundel County Code: “Article 13 – Public Works”⁸⁷
- Anne Arundel County Code: “Article 17 – Subdivision and Development”⁸⁸
- Anne Arundel County Code: “Article 18 – Zoning”⁸⁹
- “Anne Arundel County Engineering Design Manual”⁹⁰
- Maryland SHA “Bicycle Policy and Design Guidelines”⁹¹
- Maryland SHA “Context Driven Guide”⁹²

84 <https://www.aacounty.org/departments/transportation/move-anne-arundel/#:~:text=The%20Transportation%20Functional%20Master%20Plan,local%20and%20state%20fiscal%20constraints.>

85 <https://www.aacounty.org/departments/transportation/bike-pedestrian/complete-streets/>

86 https://roads.maryland.gov/OPPEN/SHA_Complete_Street_Policy.pdf

87 https://www.aacounty.org/departments/transportation/move-anne-arundel/#:~:text=The%20Transportation%20Functional%20https://codelibrary.amlegal.com/codes/annearundel/latest/annearundelco_md/0-0-0-111531#JD_Article13Title2Master%20Plan,local%20and%20state%20fiscal%20constraints.

88 https://www.aacounty.org/departments/planning-and-zoning/forms-and-publications/Article_17.pdf

89 [§ 18-17-201. Zoning violations; enforcement. \(https://codelibrary.amlegal.com/codes/annearundel/latest/overview\)](https://codelibrary.amlegal.com/codes/annearundel/latest/overview)

90 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

91 https://roads.maryland.gov/ohd2/bike_policy_and_design_guide.pdf

92 <https://experience.arcgis.com/experience/3476e680584c49e48303fe6d52ceeda9>

The project team identified a variety of policy and design barriers that hinder multimodal transportation construction, do not prioritize high-quality walking and rolling infrastructure, or present barriers to people with disabilities.

Current Regulatory or Policy Inhibiting Factors

Sidewalks and bikeways are vital pieces, of infrastructure for the County’s transportation system, connectivity, and quality of life. However, there are a variety of policy and design barriers in existing planning documents and legislation that prevent the construction of sidewalks, bicycling infrastructure, and safe crossings, including active transportation infrastructure with adequate design standards. These include:

Sidewalks

Minimum sidewalk width is only 5’, with no incentives for the County or developers to build wider sidewalks. The County passed an ordinance in 2021 making bicycle riding on sidewalks legal, making it even more important for sidewalks to be wider and allow for safe and comfortable access for all travelers.

Private Development

In addition to the need for more incentives, the County’s current policy allows developers to pay a fee-in-lieu amounting to a cost cap that is respective to a development’s quantity of units or square footage. The cost cap’s low fee schedule consistently results in inadequate funding of sidewalks by developers. In addition, the County does not have a requirement that developers install sidewalks or bicycle infrastructure on roadways adjacent to or affected by their property. This results in missing bicycle and pedestrian improvements and connections in private site development.

Traffic Impact/Adequate Public Facilities Studies

Neither pedestrian and bicycle projections, nor infrastructure, are sufficiently studied through the Adequate Public Facilities or Traffic Impact Study review processes. Furthermore, safety impacts are not adequately analyzed in these reviews.

State Coordination

On State Highways, often the County’s standard response is to “defer ROW dedication and frontage improvements to SHA”; however, the State is unable to require improvements unless an access permit is required. The County plans for improvements on State roadways but does not consistently request those improvements be made during the State’s project design process. In addition, there is a lack of clarity on which government agency, SHA or the County’s Public Works Department, is responsible for completing sidewalk construction on State-owned roadways.

Complete Streets Policy

Also relevant to the County’s pedestrian and bicycling network, the “Complete Streets Policy” states that exceptions can be made that would prohibit providing favorable conditions for all modes of transportation. There are no specifications or prioritizations for different modes, making it easier to not construct industry-standard sidewalks, bicycle lanes, and safe crossings. There are also no “shall” statements in the policy. Furthermore, there is a lack of clarity on how to implement the policy during all phases of project development, in requiring Complete Streets in specific jurisdictions, and in using specific design guidance documents.

County Funding

Historically, the County has funded bicycle and pedestrian infrastructure at a low level through the following sources:

- The Sidewalk/Bikeways Fund, which approves funding at about \$500,000 yearly;
- Funding for Anne Arundel County Schools to build school sidewalk projects at \$500,000 per year;
- The Department of Recreation and Parks’s CIP budget, which funds trails.

The County also utilizes the following revolving funds for specific uses:

- Transportation Facilities Planning;
- Severn Harman Ped Net;
- Arundel Mills LDC Roads;
- Ped Improvements - SHA Facilities.

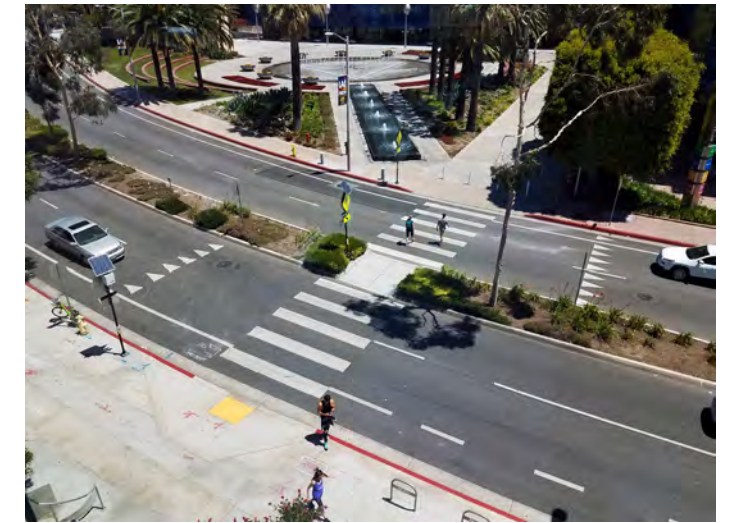


Figure 63: Infrastructure like rectangular rapid flashing beacons support safe crossings.

Bicycle Network

Anne Arundel County has been increasingly building its bicycle network, with the completion of about 30 miles of on-road infrastructure. This includes a combination of trails, sidepaths/shared-use paths, separated bicycle lanes, conventional bicycle lanes, shared lanes, and signed bicycle routes.

Trails and Sidepaths

Maintenance

A large portion of the County’s bicycle network consists of paved trails and sidepaths, comprising over half of all bicycle facilities. Trails are bicycle facilities that are physically separated from motor vehicle traffic by an open space or barrier and have their own right of way. Trails are usually managed by the Anne Arundel County Department of Recreation and Parks and other jurisdictions like the City of Annapolis. County-sanctioned trails, as shown on the Department of Recreation and Park’s trails map, are sometimes in the road right of way. This plan differentiates these as sidepaths. Sidepaths are within the roadway right of way, yet separated from the vehicular travel lanes, but are typically closer in proximity to vehicular traffic. Generally speaking, responsibility for

management and maintenance of all facilities within the road right of way is with the Public Works Department. In addition, some small connectors to trails and sidepaths are built by community associations.

This plan recommends that maintenance responsibilities be clearly defined as more and more shared-use paths, be they trails or sidepaths, are encouraged and built as a result of the recommendations of *Move Anne Arundel!* and *Walk and Roll Anne Arundel!*.

Trails as Transportation

Additionally, most of the County’s regulatory and planning documents surrounding the pedestrian and bicycle network only account for on-road facilities. Trails do not benefit from the same caliber of guidance. However, trails are able to connect people where they need to go when located within bicycling distance between essential destinations. The Capital Crescent Trail in Montgomery County, MD, for example, is an integral part of the community’s transportation system with over 1,000,000 people using the trail every year.⁹³ County Code should recognize trails and sidepaths as not only recreational venues but also as important transportation facilities.

Trails and sidepaths provide the most separation from the dangers of motor vehicle traffic. Additionally, they can be completed separate from roadway projects, which can allow for the gaps in the County’s bicycle network to be filled in more quickly and at the industry’s best standards.

On-Road Bicycle Facilities and Supportive Infrastructure

In addition to the County’s “Complete Streets Policy,” the State of Maryland has a “Complete Streets Policy;” however, neither provide specific requirements that promote high-quality bicycling infrastructure that ensures the safest possible conditions and a high degree of comfort. While the County has a “Complete Streets Policy,” it has not yet implemented Complete Streets in County code and design manuals.

The “Complete Streets Policy” has a minimum travel lane requirement for cars, but it does not have specifications for the width of bicycle lanes and buffers. This further emphasizes the auto-centric approach in the policy, as well as other planning documents for the area.

⁹³ Kraut, Aaron (4 September 2015). “New Numbers Show How Many People Are Using the Capital Crescent Trail”. Retrieved 6 September 2017.

Connections to Transit

An important piece of providing a safe, connected public transportation system in Anne Arundel County is providing the infrastructure that creates safe, first and last mile pedestrian and bicycle connections to transit. These connections are necessary to fill the gaps in areas that may not be served by transit. In reviewing the Anne Arundel County Code and Zoning Ordinances, the county has a designation of “Transit Mixed Use” for areas surrounding transit and rail stations such as the Odenton MARC station. This designation allows for more high-density mixed-use development and additional pedestrian and bicycle paths, but it does not require it, thus missing an opportunity to incentivize designers to develop facilities that would meet industry best practices.

Anne Arundel County should consider the benefits of developing a more in-depth policy for bicycle and pedestrian facilities in Transit Mixed Use areas and Transit-Oriented Developments such as in Odenton, Glen Burnie, Linthicum, Jessup, and Brooklyn Park. Without specific requirements the County risks leaving transit-dependent people with unsafe connections to transit in the County.

The findings from this review provide important groundwork for the policy recommendations that follow. Although there are challenges, the County has significant opportunities to create safe, connected, and accessible communities. In order to do so, the County needs to define definitive standards for Complete Streets and bicycle and pedestrian facilities that are inclusive of all modes and abilities.

Policy Recommendations

As it has been seen in model cities across the world, explicit policy changes that promote and enable better walking and biking infrastructure are pivotal elements of influencing a culture-shift towards walking and biking as a form of active transportation. These changes can come in the form of multiple legislative actions, whether it be the implementation of a new policy or an amendment of an existing policy. An “Interagency Policy Discussion Memo,” which consists of a summary of conversations with County and State staff on policy and related recommendations,” is found in the appendix.

Increase Funding for Pedestrian and Bicycling Infrastructure

The County should consider a significant increase in Capital Improvement Projects (CIP) funding for pedestrian and bicycle infrastructure in order to:

- Provide funding for projects in *Walk & Roll Anne Arundel!*;
- Provide required funding for grant matches from federal, state, and regional sources;
- Implement the County’s recently adopted Vision Zero Anne Arundel County;
- Maintain the County’s expanding shared-use path network.

Align Resources and Processes to Compete for State and Federal Funds

The County seeks funding through a number of long-standing and new State and Federal grant opportunities. Locally, the County has successfully coordinated with the MDOT Secretary’s Office to implement bicycle facilities, such as through the MDOT Kim Lamphier Bikeways Network Program.

To strengthen the County’s coordination with the State, and its competitiveness for State and Federal funding, the County should consider the following strategies.

Project management

Improve proactive management of the pipeline of projects through all phases of project development and funding across various departments and agencies to ensure that the County has a steady flow of projects that are ready to compete for grants. Oversight should also be improved to ensure that projects are ready to advance in a timely manner once funding is received, so that funding awards do not need to be returned to State or Federal agencies.

Improve coordination between the County and the State

The County and the State should identify opportunities to formalize and streamline coordination processes, such as during development review.

Align State and County Planning Documents

Identify opportunities to better align state and county planning documents with the design processes and guidance. In part, this goal can be achieved through

increased sharing of tools and data. One opportunity to coordinate development and use of emerging tools and data sources is during the quarterly coordination meetings held between the County and the State.

Maintenance of Facilities

Routine maintenance of off-road pedestrian and bicycle facilities falls to different entities based on the jurisdiction and type of facility. Along state highway, sidewalks and shared-use paths (including sidepaths) are constructed by MDOT SHA and maintained by the local jurisdiction as set forth in the Code of Maryland Regulations. If a facility requires full reconstruction, this responsibility typically goes back to the state. County-owned off-road facilities may be maintained by Recreation and Parks or Department of Public Works, meaning that shared-use paths in the County are maintained with different funding pools and processes.

Increase funding allocations to match maintenance needs

Proper budgeting over time can provide a year-over-year picture of how financial resources are spent, regardless of whether funds originate from County Council, voter-approved bonds or state and federal sources. This can contribute to financial decisions on how to better allocate resources. Providing budget records can help plan for the growth of the active transportation system and provide an accurate representation of budgeting scenarios when requesting additional resources from the County Council.

County departments already provide budget records online and coordinate with the public through the Transportation Commission, the Bicycle Commission, the Commission on Disabilities, the Greater Baybrook Alliance, and additional organizations. In addition, the County Office of Transportation coordinates quarterly meetings with SHA and the Bicycle Advisory Commission invites County agencies and SHA staff to their bimonthly meetings.

Through stakeholder outreach with County agencies and the public, two key items were identified to ensure the County can improve on its existing communication and coordination with the public to successfully implement the hundreds of miles of newly proposed bicycle and pedestrian facilities:

- The need for better coordination (e.g., with MOUs and regular meetings) across multiple county agencies to maintain and expand operations and maintenance – with an emphasis on shared-use paths and sidepaths;

- The need for increased staffing and budgets for operations and maintenance to help with maintaining the increasing mileage of on-road and off-road facilities, especially given the significant increase in proposed shared-use paths and sidepaths.

While annual operation and maintenance costs may seem intimidating, it is important to note that the return to the community in terms of recreational benefits, health, fitness, and economic development have been shown through numerous studies nationwide to be multi-fold, such as the reports Protected Bike Lanes Mean Business by PeopleforBikes and the Alliance for Biking and Walking, Understanding Economic and Business Impacts of Street Improvements for Bicycle and Pedestrian Mobility - A Multi-City Multi-Approach Exploration by Portland State University, and CDC Recommendations for Improving Health through Transportation Policy by the U.S. Centers for Disease Control and Prevention.

Elevate the profile of active transportation across County agencies

Elevating the profile of active transportation across agencies as essential infrastructure in Anne Arundel’s transportation and recreation system would increase the likelihood of receiving more support for the program. Increased support can unlock more resources, staff capacity, funds, and public support as the active transportation system expands. To match the ambitious network goals of this plan, the agency’s traditional approach to bicycle and pedestrian development and management will need to evolve. One such example is the staffing structure for undertaking trail-related work is unlikely to be adequate or efficient for a future that demands even greater expansion of the network, especially with the increase of proposed shared-use trails, and more sophisticated approaches to design, maintenance, and management. While further study and staff dialogue is needed, it is clear that the demand for more multimodal infrastructure will continue to grow and the public agencies that deliver these services will need to respond with improved systems, programs, and resources. This elevated profile will also help with presenting budget and resource requests to County Council as financial and capacity needs increase over time.

Coordinate with State agencies

Opportunities at the State are also emerging to take a more proactive asset management approach to existing facilities to optimize available maintenance funding. MDOT SHA Highway Office of Highway Development’s

pedestrian, ADA, and bicyclist teams are exploring this approach for their facilities and could afford an opportunity for the County to collaborate, learn from their process, and share relevant data.

Furthermore, the County should increase coordination with MDOT SHA as the Context Driven Guide and System Preservation efforts continue to develop.

In addition, the County, MDOT SHA District, and MDOT SHA Asset Management should increase data sharing and on-going coordination to identify and track needs specifically at roadway crossings.

Pedestrian and bicyclist crossing distances

The County should use context classification to match criteria for site access and intersecting street crossing distances. These criteria could include a recommended maximum distance between crossings, include key destinations, land uses, and transit access priorities such as locations with bus stops.

Priority Letters

The County should formalize the process for County staff to provide project information when preparing Priority Letters to include clear information on needs (e.g., project limits, cost estimates) to better coordinate this time sensitive process.

Continue Vision Zero Implementation

The County should continue to implement the recently adopted Vision Zero Plan, which seeks to reach zero serious injuries and fatalities on the County’s roadways by 2040. Resources should be allocated to support this implementation.

Integrate Walking and Rolling into Complete Streets Policy

The overarching goal of *Walk and Roll Anne Arundel!* is to provide the framework to ensure that Anne Arundel County provides a complete network of transportation options for all travelers, regardless of their mode, age, or ability.

The County should formally adopt legislation to codify Anne Arundel County’s “Complete Streets Policy” to promote better walking, bicycling, and rolling infrastructure in the County. In February 2014, the County Council passed Resolution 45-14, adopting a “Complete Streets Policy” for Anne Arundel County. This resolution

included language promoting the implementation of Complete Streets Guidelines, as well as other guiding principles surrounding design regulations and standards.

While the adoption of the County’s “Complete Streets Policy” provided a foundation to integrate Complete Streets design principles into transportation projects, it stopped short of establishing exact procedures and requirements that would consistently yield projects with walking and bicycling infrastructure. The appendix, “Policy Recommendations: Additional Detail,” contains specific language that should be considered for incorporation into Anne Arundel County’s formalized “Complete Streets Policy” to:

- Add a section to establish the County’s commitment to Complete Streets design and construction in all phases of private and public project development.
- Add a section to clearly indicate jurisdictions in which the County’s Complete Streets policy applies, including municipalities within the County.
- Specify that context-sensitive walking and bicycling facilities shall be designed and constructed on all arterial and collector roadways, as designated in the “Secondary Network” of *Walk & Roll Anne Arundel!*
- List specific design guides that public agencies and private developers shall use to develop walking and bicycling facilities.
- Specify that private developers shall design and construct context-sensitive walking and bicycling facilities in the frontage zone of their property.
- Establish a modal hierarchy when planning, designing, and constructing roadway projects:
 - » Walking
 - » Cycling/public transit/micromobility
 - » Taxi/commercial transit/shared vehicles
 - » Single occupancy vehicles.

Support for Local Complete Streets Policies

The County should work with local jurisdictions within its boundaries to adopt and collaborate on local Complete Streets policies. Regular trainings should be provided involving County staff, SHA staff, BMC staff, and citizen representatives from each of the nine regions identified in *Plan2040* in order to foster collaboration and increase

expertise on the application and design of Complete Streets projects.

Design Standards

Sidewalks

The “Anne Arundel County Design Manual”⁹⁴ should be updated to include *Walk & Roll Anne Arundel!* standard designs and dimensions for walking and bicycling projects. For example, sidewalks should be 6’, with an absolute minimum of 5’. A sidewalk directly adjacent to moving traffic should be 8’ with a 2’ buffer.⁹⁵ Sidewalks in downtown or town center districts should be 8-12’.⁹⁶

Complete Streets Guidelines

In addition to revisions to the “Complete Streets Policy,” a core recommendation of this plan is for the County to develop and adopt a revised set of Complete Streets Guidelines. These guidelines will be used as a reference document for planners, developers, and engineers throughout the development process to ensure that the standards of the newly updated “Complete Streets Policy” are properly upheld.

The update to the Complete Streets Guidelines, and the County’s design standard documents, such as the “Anne Arundel County Design Manual,”⁹⁷ should be updated with specific preferred standards for pedestrian and bicycle projects by *Walk & Roll Anne Arundel!* proposed street typology. For example, the documents should make clear:

- Prioritization of travel lane widths of 10’, or 11’ where there is bus and large truck traffic
- Preferred sidewalk minimum widths and buffer widths, following *Walk & Roll Anne Arundel!* and NACTO Design Guides
- Preferred bicycle facility type and widths, and buffer widths, following *Walk & Roll Anne Arundel!* and NACTO Design Guide
- Constrained minimum design details for vehicular, bicycle, pedestrian and shared-use infrastructure where space does not allow for preferred infrastructure
- Guidelines for curbside management, street furniture, bicycle parking, green street components, utilities, streetlighting, transit areas, bus bays, wayfinding, and more

94 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

95 <https://nacto.org/publication/urban-street-design-guide/street-design-elements/sidewalks/>

96 <https://nacto.org/publication/urban-street-design-guide/>

97 <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

Development Requirements

The County should strengthen partnerships with and requirements of private developers to provide low-stress walking and bicycling facilities as part of their private developments. To accomplish this, the County should implement the following recommendations:

1. Bicycle and pedestrian needs are currently identified through the Bicycle, Pedestrian, Transit Assessment (BPTA) process, Master Plans, and looking at proposed facilities near the development. To strengthen project identification and development through the BPTA process, the County should:
 - Update the tools available for use in the BPTA to better reflect latent and future demand for pedestrian and bicycle facilities, such as local updated mode split and target mode split. Increased data sharing between interagency partners could support this endeavor. For example, MDOT TSO has a Short Trip Opportunity Area analysis that can help identify latent demand and MDOT MTA has an inventory of Bus Stop Access needs.
 - Increase the dollar amount of the cost cap for sidewalks required by the BPTA Bill 78-18.
2. The County should require developers to construct context-sensitive walking and bicycling facilities that connect to their properties. To accomplish this, the County should:
 - Allow developers to contribute towards a Capital Improvement Project (CIP) – for which the County has already committed funding – that would improve conditions related to the development project. This would support the construction of continuous facilities along a developing corridor with multiple parcels through an integrated approach, rather than funding piecemeal corridor development.
 - If the County pursues requiring developers to design and construct context-sensitive walking and bicycling infrastructure along the zones of their private developments, there should be a process by which developers could opt into the CIP match as described above.
 - Relatedly, it is important to note that funding active transportation projects in the CIP, and approving active transportation projects in the County’s plans (such as *Walk & Roll Anne Arundel!* and area plans), bolsters partnerships with developers. This funding

and plan adoption shows a commitment to the pedestrian and bicycling network, providing more opportunities for developers to contribute to a corridor-wide project rather than advancing only a part of a planned project without the certainty of the timely completion of a fully functional network.

3. Scenic and Historic Roads requirements may limit bicycle and pedestrian facilities that may be installed on these designated roadways. The Scenic and Historic Roads section of the County Code should be revised to encourage multimodal transportation options, rather than limit them.
4. The County’s Adequacy of Public Facilities ordinance must be followed when implementing bicycle and pedestrian facilities. The ordinance should be revised to include level of service and safety analyses for bicyclists and pedestrians during the traffic impact study process. Options to mitigate failures should be provided.
5. Bicycle and pedestrian facilities should also be designed to prevent traffic fatalities and serious injuries, following the County’s Vision Zero Executive Order and Action Plan.

Incentives for Active Transportation Commutes

The County should provide incentives to encourage people to commute via walking or bicycling. The County should also partner with businesses and other entities to encourage their workforce to do the same.

Bicycle Friendly Community Recognition

The County should commit to achieving the League of American Bicyclist’s recognition of Silver, then Gold, in the Bicycle Friendly Community program.⁹⁸

Safe Routes to School

Anne Arundel County Public Schools (AACPS) should focus efforts on improving safety for students to walk or bicycle to school and encouraging students to do so. Currently, there are joint efforts between AACPS and OOT to implement a Safe Routes to School program. AACPS should hire a full-time bicycle and pedestrian planner, strengthen programs to promote walking and bicycling to school, and launch Safe Routes to School initiatives where they do not exist. This position could be shared between AACPS and OOT.

Access for People with Disabilities

In talking with County staff and researching existing County policies, the following strategies are offered to improve transportation access for people with disabilities:

1. Anne Arundel County should establish an ADA Self-Evaluation and Transition Plan.
2. Develop guidance for transportation planning and engineering to ensure that the transportation system meets the needs of people with disabilities. ADA should be treated as a minimum and not a standard for the County, given that people with disabilities still have difficulty navigating a transportation system that complies with ADA. Preliminary ideas for this guidance include:
 - a. Designing public transportation stations and stops that make it easy for people with disabilities to travel by following universal design guidelines listed in the appendix;
 - b. Designing floating bus stops to be accessible for pedestrians with vision and other disabilities.
 - c. Designing bicycle facilities that work for bicyclists with disabilities by following the source listed in the appendix;
 - d. Make design decisions with the needs of people with autism and other behavioral conditions.
3. Increase staff capacity at the Department of Public Works and the Disability Commission to evaluate all engineering plans for roadway, pedestrian, and bicycle facilities to ensure comprehensive access for people with disabilities is designed and constructed.
4. Assess project designs for accessibility at multiple stages before construction, especially for nonconventional designs.

5. Assess projects post construction.
6. Build a project’s budget to allow for revisions to improve accessibility.
7. Systematize engagement of people with disabilities in the planning and design processes, such as by following guidance listed in the appendix.
8. Provide regular trainings to County staff on accessible planning and design.
9. Develop a proactive sidewalk repair and maintenance program for sidewalks on County and State-owned roadways.

Additional detail with guidance to implement the above recommendations is found in the Appendix D, “Network Gap Analysis Memorandum.”



Figure 64: Improving accessibility improves the walking experience for all pedestrians.

⁹⁸ <https://www.bikeleague.org/community>

Appendices

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Appendix A: Review of Previous Plans Memorandum

Assessment of Previous Plans County Plan Review

Over the past twenty years, Anne Arundel County has shown an increasing commitment to improving walking and bicycling by investing in new sidewalks, expanded trails and bicycle lanes, and safer intersections. In recent years, the County has emphasized land use that places homes, shops, and jobs closer to one another – and closer to transit – which will make it easier for more people to walk and bike. This Plan will build upon that legacy and many of the County’s previous planning efforts.

Below is a summary of recent plans covering pedestrian and bicycle issues in Anne Arundel County, including plans produced by the State of Maryland, City of Annapolis, and other entities.

County Plans

2003 Pedestrian and Bicycle Master Plan

The *2003 Pedestrian and Bicycle Master Plan*, the last countywide active transportation master plan, notes that the County’s road network leaves people walking and bicycling at a disadvantage, with barriers like major highways and arterial roads, and disconnected neighborhoods and commercial centers, that force people to drive, even very short distances. The plan set a goal of 215 additional miles of bike lanes and shoulders and 160 additional safe intersections by 2018, expanding on the 28 miles of trails that existed at the time.

The Plan’s primary recommendations were to expand off-road trails; integrate walking and bicycling facilities in new development and transportation projects; and establish new sources of funding for active transportation. Its policy recommendations include providing more opportunities for children and adults to learn safe riding and walking behaviors; and creating an organizational structure within the county to implement active transportation, which ultimately led to the creation of the Bicycle Advisory Commission. Included in the plan are high-priority bicycle routes and pedestrian improvement zones, which include the County’s three town centers at Parole, Glen Burnie, and Odenton, as well as commercial areas in Severna Park and Crofton. Finally, there is an action plan for initial projects and programs, as well as sample corridors for pedestrian and bicycle improvements.

2013 Pedestrian and Bicycle Master Plan

Building on the *2003 Pedestrian and Bicycle Master Plan*, the 2013 plan focused on the parts of the county with current or planned water and sewer service. In these areas, it identifies “credible” pedestrian and bicycle projects and ranks them using a three-tiered system. Nineteen pedestrian and bicycle “attractors,” such as job centers, transit stations, and shopping centers, were used to weight the ranking system.

The plan also set important active transportation policies for the future. It established pedestrian and bicycle design guidelines and recommended the County adopt a Complete Streets Policy, which was approved in 2014.

The plan led to the creation of a dedicated pedestrian and bicycle coordinator in county government. Opportunities for additional growth in this plan include coordination with Anne Arundel County Public Schools, including improving walking and bicycling connections to schools and expanding the County’s Safe Routes to School program in new development and transportation projects; and establish new sources of funding for active transportation. Its policy recommendations include providing more opportunities for children and adults to learn safe riding and walking behaviors; and creating an organizational structure within the county to implement active transportation, which ultimately led to the creation of the Bicycle Advisory Commission. Included in the plan are high-priority bicycle routes and pedestrian improvement zones, which include the County’s three town centers at Parole, Glen Burnie, and Odenton, as well as commercial areas in Severna Park and Crofton.

Finally, there is an action plan for initial projects and programs, as well as sample corridors for pedestrian and bicycle improvements



²¹ <https://www.walkscore.com/score>

2020 General Development Plan (Plan 2040)

Adopted in 2020, *Plan 2040* is Anne Arundel County's overarching vision for how the county should grow and evolve over the next 20 years. It was the result of extensive community outreach with dozens of public forums and thousands of individual comments, and has three main themes: Green, Smart, and Equitable. *Plan 2040* reflects ongoing trends that are reshaping the county, such as increased commuting to Washington, DC and Howard County instead of Baltimore City, recurring concerns about infrastructure supporting continued population and job growth, and an effort to address racial and socioeconomic inequity and provide economic and social opportunities to all county residents.

The plan identifies several challenges that the Pedestrian and Bicycle Master Plan can address, including making the county more resilient to climate change, investing in more transportation choices, encouraging transit-oriented development, and providing better access to the waterfront and park system. It identifies areas that will experience limited development, such as agricultural areas and the peninsulas, as well as targeted locations for redevelopment and revitalization, all of which will require further investments in pedestrian and bicycle infrastructure.



Plan 2040 lays out specific goals that support this plan, such as:

- GOAL NE3: Expand, enhance, and continue to protect the County's greenways, open space, rural areas, including the Priority Preservation Area (PPA).
- GOAL BE7: Promote vibrant, high-quality development in Town Centers that provides opportunities to live, work, learn, and play without daily use of a car
- GOAL BE10: Relieve traffic congestion and improve mobility options and safety in the Critical Corridor Policy Areas
- GOAL BE15: Provide a well-maintained multimodal transportation network that is safe, efficient, environmentally sensitive, and provides practical and reliable transportation choices and connections for all users.
- GOAL BE16: Increase the County's resilience to future changes in climate and reduce emissions of greenhouse gases.
- GOAL HC8: Provide a diverse range of accessible public recreational facilities to serve the needs of all County residents.
- GOAL HE1: Promote economic development that supports smart growth and provides opportunities for all County residents.

Green Infrastructure Plan

The County has recently adopted a *Green Infrastructure Plan*, which updates the 2002 *Greenways Plan*. It proposes expanding on the County's existing parks and open space network with a series of "hubs," or areas of natural land at least 250 acres in size, connected by "corridors" and areas of contiguous forest. Many of these hubs and corridors are potential locations for trails, and previous small area plans have recommended them as such. The plan also identifies more local forms of green infrastructure, such as small neighborhood parks, street trees, and stormwater management. Recommended actions in the Green Infrastructure Plan include a comprehensive trails master plan, which will complement *Walk & Roll Anne Arundel!*

Land Preservation, Parks, and Recreation Plan (Draft)

Adopted in July of 2022, the Land Preservation, Parks, and Recreation Plan will serve as the master plan for the County's recreational programming, land preservation, and park acquisition and development activities. The plan emphasizes that the County's trail network is intended for both recreation and transportation, and identifies ten new or expanded trail facilities that the County has previously envisioned:

- Broadneck Trail
- Chesapeake Beach Rail Trail
- Colonial Annapolis Maritime Bike Route
- Light Rail Trail
- MD 175 Trail
- Marley Neck Trail
- Odenton Trail
- South Shore Trail
- WB&A Spur

Vision Zero Anne Arundel County

Between 2015 and 2019, there were 223 deaths and 1,282 serious injuries on roads in Anne Arundel County. In response, the County developed *Vision Zero Anne Arundel County*, which was adopted via Executive Order #58 on July 5, 2022.

It follows in the footsteps of Maryland's Vision Zero plan along with the stated commitment of *Plan 2040* and *Move Anne Arundel!* to create a safe, equitable transportation network that eliminates preventable deaths and injuries. The plan is structured around the following six "Es": Engagement, Enforcement, Engineering, Emergency Medical Services, Equity, and Evaluation. It identifies specific conditions and behaviors that lead to crashes such as poor road design, speeding, improper turning, or distracted driving. Based on each of these conditions, the plan sets benchmarks for reducing deaths and injuries over time using a strategic countermeasure implementation plan.

Anne Arundel County Complete Streets Policy and Guidance

Anne Arundel County adopted its complete streets policy by County resolution on October 6, 2014. The policy directs the County to work with partners at other agencies to ensure complete streets principles are incorporated in a consistent and integrated manner on all public and private roads in the County. The Policy's stated goal is to "approach every transportation improvement and project phase, regardless of source of funding, as an opportunity to create safer, more accommodating, and more accessible streets for all users." Furthermore, the policy states "It shall be the policy of Anne Arundel County that all projects in the public right of way, both new and retrofit, shall be designed and constructed in accordance with the [County's] Complete Streets Guiding Principles." In order to fully implement this policy, the County will need to make related changes to County code, design manuals, policies, and processes.

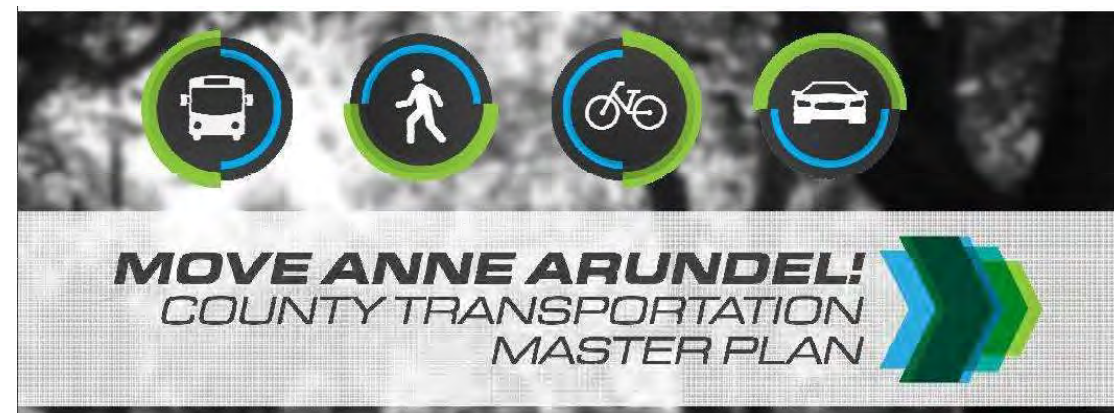


The Office of Planning and Zoning developed a Complete Streets case study that identified MD 648 (Baltimore Annapolis Boulevard) to support Complete Streets Guidance. It explored the feasibility, affordability, and applicability of re-designing a typical arterial road as a complete street. Bicycle and pedestrian enhancements such as connected, ADA accessible sidewalks and separated bike lanes were considered. Instead, 5-foot bicycle lanes were recommended “throughout the segment.” The project team notes that this facility selection falls short of recommended criteria in the *FHWA Bikeway Selection Guide* based on the motor vehicle traffic volume, speed, and number of lanes on a typical suburban and rural arterial of this type.

Move Anne Arundel! Final Transportation Functional Master Plan (2019)

Move Anne Arundel! is the County’s transportation functional master plan, which lays the groundwork for the current PBMP. It builds on previous County efforts including the 2012 Corridor Growth Management Plan and the Major Intersections and Important Facilities Study. The plan identifies the County’s three town centers and its schools as focal points for pedestrian investments and emphasizes a low-stress bicycle network that uses on-street connections to tie the trail network to nearby destinations. Some of the high priority low-stress bike network investments in *Move Anne Arundel!* include:

- WB&A Trail Bridge Crossing over the Patuxent River
- South Shore Trail from Annapolis to Odenton
- Shared use path from Odenton to Baltimore City line
- Shared use paths or separated bikeways on peninsular routes, such as the Annapolis Neck and Mayo Peninsula
- Broadneck Peninsula Trail from the B&A Trail to Sandy Point State Park
- Extending existing shared use paths, including the Marley Neck Boulevard, East West Boulevard, MD 175 sidepaths, and the Odenton Trail.



Move Anne Arundel! (MAA) “Tech Memo 2: Developing A Vision, Goals, And Performance Measures”

This memo documents the Vision, Goals, and MOE development process for MAA. It included a review of best practices, relevant Federal policies, and a review of 9 peer counties’ comparable plans. Recommended vision, goals, and performance measures are not documented here, as they were recommendations, and will be included in the review of the MAA final plan.

MAA “Tech Memo 5: Analysis of Traffic Crashes”

“Between 2015 and 2017 there were 100 documented crashes involving pedestrians and 27 involving bicyclists across the county. 58% of pedestrians were involved in crashes that occurred on State roads, and 41% of pedestrian involved crashes occurred on local roads. Local roads were slightly more dangerous for bicyclists—59% of bicycle involved crashes occurred on local roads, and 37% occurred on state roads. There were two crashes reported on interstate highways; a bicycle involved accident on a ramp, and a pedestrian involved accident on an interstate.”

MAA Tech Memo 8: Review of Key Corridors The plan specifically discussed the following pedestrian and bicycle considerations for five key State corridors: I-97, US 50, MD 32, MD 2, and MD 100.

Table 22: Subjects covered in transportation goals for a subset of jurisdictions statistically similar to Anne Arundel County.

Subject	Montgomery	Bucks	Arapahoe	Frederick	Baltimore	Howard	St. Charles	Wake	Boulder
Safety	X	X		X			X		
Maintenance				X				X	X
Service Quality		X	X	X					
Conservation/Environment					X	X	X	X	X
Non-Automotive Options	X	X	X	X	X	X	X	X	
Goods	X	X							
Air Travel	X	X					X		
Conservation	X	X				X	X	X	
Land Use/Transportation		X	X	X	X			X	
Accessibility						X			X
Economic					X		X		X
Transportation Mindfulness						X			
Community Stewardship								X	X
Agency						X			
Transit Service Expansion						X		X	
Transportation Demand Management				X		X			
Resiliency									
Cost-Effectiveness						X			X
Affordability							X		
Public Involvement									
Equity									X
Tracking									

I-97	<p>Bike/ped traffic is excluded on interstate highways unless it is located on an approved adjacent path or facility. As such, parallel routes should be explored. The Bicycle and Pedestrian Master Plan contains recommendations along MD 178 (Generals Highway) to MD 450 (Defense Highway), and West Street continues the corridor into Annapolis.</p> <p>Pedestrian and bicycle improvements on MD 178 are recommended from Knollwood Drive to the Annapolis city line. Intersection improvements are recommended at the intersection of Housley Road, Bestgate Road, and MD 178.</p> <p>Use existing wide shoulders and right-of-way on MD 178 between West Street and Knollwood Drive for separate bike lanes</p> <p>MD 178 north of Knollwood Drive could support separated bike facilities up to the Paul Birch Drive intersection, where the right-of-way narrows</p> <p>Consider bike lanes on West Street from US 50 to Taylor Avenue</p> <p>Incorporate separated bike lanes with improvements at the Housley Road/Bestgate Road/MD 178 intersection.</p> <p>Advance the bicycle and pedestrian facility recommendations in coordination with land use changes at the former Crownsville state hospital.</p>
US 50	<p>No pedestrian or bicycle improvements are recommended for US 50. This corridor is a limited access highway, and no pedestrians or bicycles are permitted.</p> <p>Continuous, parallel routes should be explored. MD 179 (St Margaret's Road), MD 648 (Baltimore Annapolis Boulevard), and MD 450 (Defense Highway) are parallel routes to US 50 that are included in the BPMP. Bicycle improvements are recommended:</p> <ul style="list-style-type: none"> » On MD 179 from Baltimore Annapolis Boulevard to Whitehall Road. » On MD 450 from MD 3 (Crain Highway) to MD 424 (Davidsonville Road). » Through advancing projects from the BPMP and: <p>By upgrading bicycle lanes to separated facilities on the Naval Academy Bridge over the Severn River.</p> <p>By enhancing and completing the Poplar Trail or consider bicycle lanes on West Street from Taylor Avenue to US 50.</p> <p>Consider advisory bike lanes on Defense Highway west of MD 178</p>
MD Route 2	<p>Jumpers Hole Road operations, safety and bicycle/pedestrian improvements between MD 2 and East-West Boulevard and between MD 2 and MD 177</p> <p>Reconstruction of MD 648 operations, safety and bicycle/pedestrian improvements between MD 2 and MD 10</p> <p>MD 2 is a four-lane divided roadway with recommendations for many opportunities for bicycle and pedestrian improvements, as there are long and frequent gaps in the sidewalk network and few accommodations for bicyclists who would use this corridor. Sidewalks extend along cross streets into residential neighborhoods at most intersections, but they do not parallel Governor Ritchie Highway. MD 2 North is one of the most dangerous sections of roadway for bicyclists and pedestrians in Anne Arundel County with particular hotspots at Ordnance Road, B&A Boulevard, and Benfield Boulevard.</p> <p>The B&A Trail parallels MD 2 from US 50 to Crain Highway. With improved connections from MD 2, shorter bicycle trips may be more likely. Improvements are needed along MD 2 to provide access to area businesses.</p>
MD 100	<p>No pedestrian or bicycle improvements are planned along MD 100 in the 2013 PBMP. Parallel roadways have more convenient access to residences, businesses and other community destinations for people who are walking and bicycling. Plans for additional bicycle and pedestrian improvements on MD 170, MD 177 and MD 176 are possible to create a parallel bicycle and pedestrian route parallel to MD 100.</p>

Area Specific Plans

The following are plans for local areas in the County, including town centers, the City of Annapolis, and previous planning studies for individual roads and trails. Small area plans, which cover land use and transportation recommendations within individual communities within the County, are in the process of being reorganized and updated. The County is in the process of drafting regional plans for the nine regions identified in *Plan 2040*, with Regions 2, 4, and 7 getting kicked off in December 2021. Regions 1, 3, and 9 will begin plan development in 2022, and Regions 5, 6, and 8 in 2024. These plans may result in some new bicycle and pedestrian related recommendations.

Annapolis Ped/Bike Plan (2011)

The City's 2011 plan recommends several bike facilities on streets that connect to the County, including the following:

- Poplar Trail: City Dock to the Annapolis Mall
- Spa Creek Trail: neighborhood and trail route connecting City Dock to Parole Town Center
- West Annapolis/Hilltop/Bay Ridge Loop
- Forest Trail: a separated trail along the entire southern edge of Forest Drive

Odenton Town Center Master Plan (2016)

The Odenton Town Center is an area that has experienced a high volume of residential and business growth throughout recent decades and is expected to continue that trend in the decades to come. The Odenton Town Center Master Plan, with the last update completed in 2016, provides planning and implementation guidance and requirements for development and redevelopment within the Town Center. It also provides a basis for planning public expenditures needed to support future growth in Odenton. The plan provided objectives and goals that are relevant to *Walk and Roll Anne Arundel!* such as:

- Plan Objective 3: "Provide standards and requirements that ensure the development of a coordinated network of protected natural greenways, natural and high-quality urban public spaces, and pedestrian/bike ways. Provide for an overall unified design character for the streetscapes and public spaces of the various areas of the OTC."
- "Pedestrian connections from transit to Core and Historic sub-areas are a primary focus; in the Industrial area, pedestrian improvements are focused on crossing Telegraph Road. MARC station TOD is intended to be a multiuse development that is pedestrian focused. Urban design standards and frontage requirements are meant to enhance pedestrian environment."
- "Two hiker-biker trails will ultimately connect the OTC with other parts of the region. The South Shore Trail will be a paved multi-use trail that runs along the abandoned WB&A rail line between Annapolis and Odenton. The trail is being constructed in phases, and when completed it will be a component of the East Coast Greenway and American Discovery Trail. The WB&A Trail will be a paved multi-use trail extending along the WB&A rail line south of Odenton. When completed, it will link the South Shore Trail in Odenton with the Patuxent River and an existing rail trail in Prince George's County."

An update is currently being produced.¹

Glen Burnie Redevelopment Plan

Parole Mobility Plan (2020)

The recently completed Parole Mobility Plan identifies three main issues in the Parole Town Center: A lack of walking and biking access and connectivity; a lack of safe roadway crossings for walking and biking; and traffic congestion. To address this, it lays out a vision to redevelop Parole as a well-designed urban place including future transit-oriented development.

Acknowledging that there are limited opportunities to widen roads or intersections, the plan recommends increasing connectivity for all travel modes, including:

- New roadway extensions, bridges, and frontage roads
- Build out of a shared-use path network
- Improved pedestrian facilities and reduction in block sizes to increase walkability
- Access to transit

2017 Baltimore Metropolitan Council Patapsco Regional Greenway Concept Plan and Implementation Matrix

This plan proposes two greenway alignments between BWI and points south, north, and west, tying into an eventual trail network between BWI and Baltimore. The first follows the county's Bicycle and Pedestrian Plan recommendation of a proposed trail from the Patapsco River at MD 648 to Maple Road. The second greenway segment proposes a new trail parallel to Stony Run from the Patapsco River at I-195 to the BWI Trail following Ridge Road and Corporate Center Drive.

The plan has been amended to follow a different alignment.

Current Anne Arundel County Capital Improvement Program (CIP) Projects

State and Regional Plans

Maximize 2045: A Performance-Based Transportation Plan

Maximize 2045, Baltimore Metropolitan Council's (BMC) regional transportation plan, establishes regional goals, strategies, and initiatives through the life cycle of the plan (2024-2045). The plan identifies major surface transportation projects that are expected to be implemented in this time period, as well as revenue projections and cost estimates for project funding purposes. The plan's nine, broad regional goals and their respective strategies include:

- **Improve system safety:** In order to improve system safety in the region, BMC is pushing to adopt relevant plans that seek to reduce transportation-related injuries and fatalities, such as a Vision Zero Action Plan. In addition, the strategies to improve safety include preventative actions, education, and additional enforcement. This consists of improving traveler safety for all modes, improving the responses of emergency services, and educating travelers of all modes on safe travel techniques.
- **Improve and maintain the existing infrastructure:** The strategies involved in improving and maintaining existing infrastructure include the continued improvement of the condition of roadway systems, such as pavement, bridges, and tunnels, as well as the replacement of aging infrastructure. This includes aging traffic signals and transit vehicles, on a timely, systematic basis. Another strategy within this goal is the emphasis on improving bicycle and pedestrian conditions, as well as transit infrastructure and stations. Through the completion of this goal, BMC hopes to encourage local jurisdictions to develop management programs that assess the conditions of existing infrastructure to allow for replacement of assets on a regular, timely basis.
- **Improve accessibility:** To improve accessibility, the strategies involve looking at equity and the overall connectivity of the transportation network. This includes increasing options for those who are elderly, disabled, minority, low-income, or transit dependent. BMC will also continue to invest in pedestrian and bicycle facilities, while also ensuring that conditions meet or exceed ADA standards. Not only would the BMC like to make these changes in the public sector, but they hope to influence the private sector to provide the appropriate access on their properties as well.
- **Increase mobility:** Working to increase mobility, BMC put an emphasis on strategic congestion mitigation and management. Accounting for all modes, the plan also includes the strategy of developing and supporting regional, long-distance bikeway network with consistent guide signage.
- **Conserve and enhance the environment:** To accomplish the goal of conservation and enhancement of the environment, the plan has a strategy of funding multimodal transportation networks that will promote active living and minimize transportation-related emissions. This includes ensuring that the region conforms to state air quality standards and invests in the appropriate transportation programs and practices that support and promote the use of fuel-efficient best practices.
- **Improve system security:** While working to improve system security, the plan includes strategies involving the coordination between different municipalities and agencies. More

¹ <https://www.aacounty.org/departments/planning-and-zoning/long-range-planning/odenton-town-center/>

specifically, these strategies consist of improving agencies' response and recovery from emergencies, identifying appropriate policies and procedures for communications and resource sharing, increasing closed-circuit security systems, and appropriate preparation for natural disasters.

- Promote prosperity and economic opportunity: To work towards achieving the goal of promoting prosperity and economic opportunity, BMC details strategies that emphasize the connection and influence that land use decisions have on other aspects of the network and economy. This includes related decisions in transportation planning, housing availability, and the costs of transportation. The plan also lists strategies to promote the development surrounding transit stations, as well as investment in transportation assets that promote tourism and the movement of tourists.
- Foster participation and cooperation among stakeholders: In order to meet the goal of fostering participation and cooperation among stakeholders, *Maximize 2045* endorses strategies that involve the coordination of planning and programs across all modes, agencies, and municipal boundaries to ensure appropriate participation. These strategies could be either traditional, in-person, or use alternative means such as social media, virtual meetings, surveys, and more. Additionally, ensuring that businesses, developers, and communities are engaged to build support for approaches and strengthening the public-private partnership is a key strategy in reaching this goal.
- Promote informed decision making: BMC and its members share the goal of promoting informed decision making. To achieve this, BMC provides the ability for members to analyze, compare, and share data on system conditions and performance, coordinate transportation investments with state planning policies and local long-term plans, and plan for the effects of emerging transportation technologies and other long-term changes.²²

These goals are paired with respective strategies to help achieve their implementation, performance measures to assess progress, and performance targets to guide the creation of a stronger, successful transportation network in the region.

With the creation of a safe, accessible bicycle and pedestrian network, the implementation of *Walk & Roll Anne Arundel!* will help the Baltimore region meet the goals of *Maximize 2045*.

Bus Stop Assessment for the LOTS

The Baltimore Metropolitan Council studied the existing conditions of bus stops maintained and operated by the Locally Operated Transit Systems (LOTS) in the BMC region, determined ADA compliance for each stop, evaluated existing multimodal connections and amenities for passengers at each stop, and recommended bus stop improvements.

State Plans

The following are state-level plans and policies that encompass and impact conditions in Anne Arundel County.

2040 Maryland Bicycle and Pedestrian Master Plan

The *2040 Maryland Bicycle and Pedestrian Master Plan*, approved in 2019, is the statewide framework for making Maryland a great place for safe and connected walking and bicycling for people of all ages and abilities. With extensive input from a wide variety of stakeholders, the plan presents the state with a fresh perspective and strategic focus towards reaching its goal of making Maryland a great place for all walkers and cyclists.

The plan emphasizes multimodal connections such as bus stop and transit station area improvements, improving access on transit vehicles, and secure bike parking. It also identifies "Short Trip Opportunity Areas" with a high potential for walking and bicycling trips, including areas like Annapolis, the Route 2 corridor, and the Route 3 corridor. To connect these location and others, the plan has a "Spine Network" of regional connection like the BWI Trail. Other projects identified in the plan include the WB&A Patuxent River Bridge Crossing near Odenton, under construction as of winter 2022.

The plan identified five goals, with their objectives and strategies, to guide State support for bicycle and pedestrian activity in Maryland. The goals relate to safety, connected networks, analysis and planning, partnerships, and economic development.

- Improve the Safety of Bicycle and Pedestrian Travel through education, enforcement, and infrastructure solutions. The objectives to reach this goal include reducing cyclist and pedestrian fatalities, improving the maintenance and operations of safe access for pedestrians and cyclists, and improving appropriate education and enforcement for safe multimodal conditions.
- Create Connected Networks enhance transportation choice and multimodal connectivity through a linked network by leveraging investment in planned routes to support the creation of safe, low-stress bicycle and walking routes for all active transportation travelers. Achieving this goal would also entail the improvement of bicycling and walking accessibility to transit facilities.
- Provide Analysis and Planning Support for efficient and equitable planning and project development with data driven tools and innovative techniques.
- Build Partnerships to promote active transportation and strengthen the health of our communities. By leveraging the appropriate partnerships, Maryland residents will be encouraged to participate in active transportation to meet their transportation needs while also strengthening community partnerships.
- Advance walking and bicycling as an economic development strategy.²³ This includes the objective of developing bicycle and pedestrian facilities that promote active tourism and expanded access to the economic benefits of walking and bicycling to more Maryland residents and businesses.

With its network-based approach, *Walk and Roll Anne Arundel!* overlaps with the goals of the *2040 Maryland Bicycle and Pedestrian Master Plan* and will be a pivotal piece to ensuring that Anne Arundel County continues to progress towards these statewide goals.

SHA “Complete Streets Policy”

The Maryland State Highway Administration (SHA) passed a policy in 2012 that affirms a “philosophy of developing context sensitive solutions that support pedestrian, bicycle, ADA and transit accessibility.” The policy envisions a multimodal, connected network throughout Maryland and directs MDOT planners and engineers to consider and implement multimodal, complete streets and safety elements for all road users in every project they undertake. The policy also identifies exceptions, performance measures, and the

SHA Context Sensitive Solutions Manual

SHA’s *Context Sensitive Solutions Manual* is a planning, design and management tool intended to support efforts to preserve, maintain and enhance Maryland’s Byways. The guidelines are for reference in project and program development to help staff and other stakeholders “recognize the scenic values, historic significance and overall character of the state’s byways and the need to gain concurrence from the relevant regulatory agencies, while at the same time considering safety, operations, maintenance, constructability, stakeholder input, impact on the route’s users and neighbors, and cost.” The primary focus of the document is on historic and environmental preservation in more rural roadway design.

Additional SHA Project Planning Studies and Design Development

Three recent plans from the State Highway Administration cover pedestrian and bicycle issues:

- MD 198 (MD 295 to MD 32): The purpose of the project is to improve existing capacity, traffic operations, as well as vehicular and pedestrian safety along MD 198, while supporting existing and planned development in the area. Bicycle and pedestrian access will be provided where appropriate. (BRAC Related)
- MD 175 (Mapes Road to MD 32): Convert existing four lane roadway to six lane highway, including sidewalk and shared use path.
- MD 295 (MD 100 to I-195): Widen MD 295 from 4 to 6 lanes from MD 100 to I-195 including an interchange at Hanover Road and improvements to Hanover Road from the CSX tracks in Howard County to MD 170, including a shared use path.

²² https://www.mdot.maryland.gov/OPCP/BW_2019_01_08MDOT_Final_Version_High_Res_with_Page_Borders.pdf

Other Initiatives

Anne Arundel Trail Network

The Anne Arundel Trail Network is a concept that joins existing, planned, and future trails into a network that connects people to destinations like schools, parks, transit, employment, and other destinations. It also provides regional connections to the Capital Trails Coalition Network in the greater DC area and the envisioned Baltimore Greenway. The concept was developed by the Anne Arundel County Bicycle Advisory Commission and Bicycle Advocates for Annapolis & Anne Arundel County.

The findings from this plan review will form a foundation for this plan, informing new network, policy, and implementation recommendations.

Appendix B: Crash Analysis Memorandum

MEMORANDUM

February 22, 2022

To: Tanya Asman, Bicycle and Pedestrian Planner
Organization: Anne Arundel County Office of Transportation
From: Dan Reed, Project Manager
Project: Pedestrian and Bicycle Master Plan

Re: Crash Analysis

Anne Arundel County has made significant progress toward improving the walking and bicycling experience, but challenges remain. The following is a discussion of historical crash trends and a systemic analysis of the factors that lead to crashes involving pedestrians and bicyclists.

Previous Crash Analysis

According to Move Anne Arundel!, the county has the fifth-highest rate of pedestrian and bicyclist-related crashes in the state, and between 2013 and 2017, an average of eight pedestrians, bicyclists, and motorists died on the County's roads. That plan identified the following historical trends in pedestrian and bicyclist crashes:

- 58% of pedestrians were involved in crashes that occurred on State owned roads, and 41% of pedestrian involved crashes occurred on County-owned roads.
- County roads were slightly more dangerous for bicyclists—59% of bicycle involved crashes occurred on County roads, and 37% occurred on state roads.
- There were two crashes reported on interstate highways; a bicycle involved accident on a ramp, and a pedestrian involved accident on an interstate.
- The highest crash hotspot on local roadways is the Maryland State House area in Annapolis; five of the top ten local route crash hot spots occur on Forest Drive in Annapolis.

This plan also identified ten "hot spots" for crashes in the County, notably the Route 2 corridor between Brooklyn Park and Severna Park, and the Route 3 corridor between Glen Burnie and Crofton. Other areas of concern included Pasadena, Odenton, Laurel, and Hanover, as well as Parole Town Center.

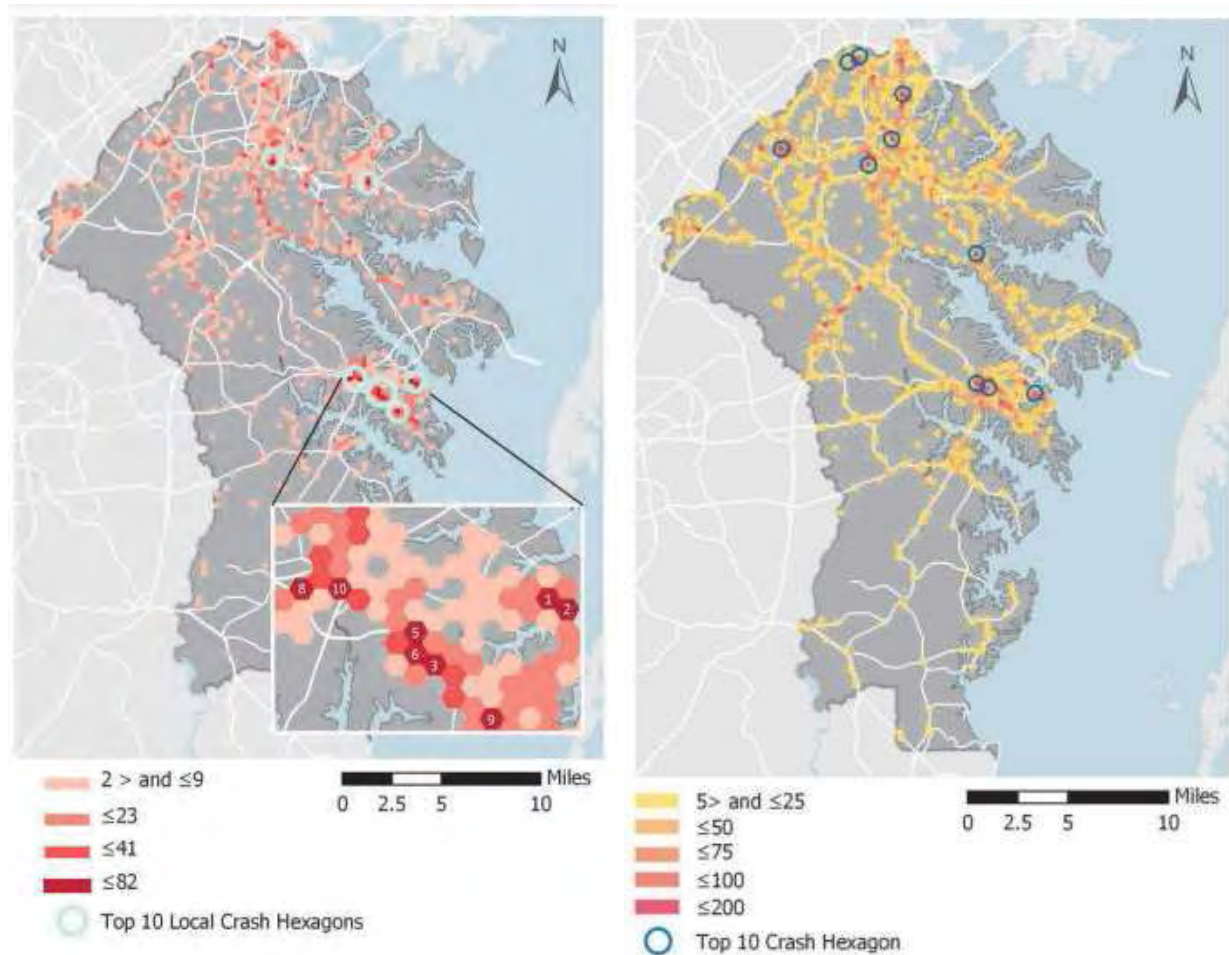


Figure 1 Maps of crash data between 2015 and 2017 on local routes (left) and state routes and Interstates (right) from Move Anne Arundel! The circles identify the top 10 locations for crashes during that period.

Looking at where crashes have occurred in the past, it's possible to conclude that these are the only areas with dangerous travel conditions. Severe crashes may happen infrequently, and where and how they happen can change over time. Conditions along a road near a crash site are typically similar to those at the exact location where the crash occurred. In addition, there may be other roads in the County that have a risk of high crashes in the future. These locations may have similar features to areas where crashes have recently happened.

Systemic Safety Analysis

The County's commitment to Vision Zero requires us to use a proactive, systemic approach to transportation planning. A systemic approach uses historical crash and roadway data to identify roadway features that are linked with crashes, so the County can identify locations that have a high risk for severe crashes, even if none have happened there recently. With this information, we can focus resources on the highest-risk locations.

First, the project team used a "sliding window" approach, which uses crash data from the County and state of Maryland between the years of 2016 and 2020, a set of over 50,000 crashes. The analysis looks at the area within 0.5 mile of each crash, then again in an area offset 0.1 mile from that crash. This process was repeated as many times as needed for each identified road segment.

The project team then used a tool called the Safer Streets Priority Finder to understand what areas present the highest risk to people walking and bicycling. Data from previous crashes is combined with physical characteristics of that area to calculate risk, using a model developed by the US Department of Transportation. Those characteristics include the functional class of the road (or type of road), Vehicle Miles Traveled (VMT) on that section of road, intersection density, employment density, residential population density, activity mix index, and sociodemographic information. This tool has been used in several cities around the United States, but this is one of the first times it's being applied to a community as part of a comprehensive planning process. This is one of the first times it's being applied to a community as part of a comprehensive planning process.

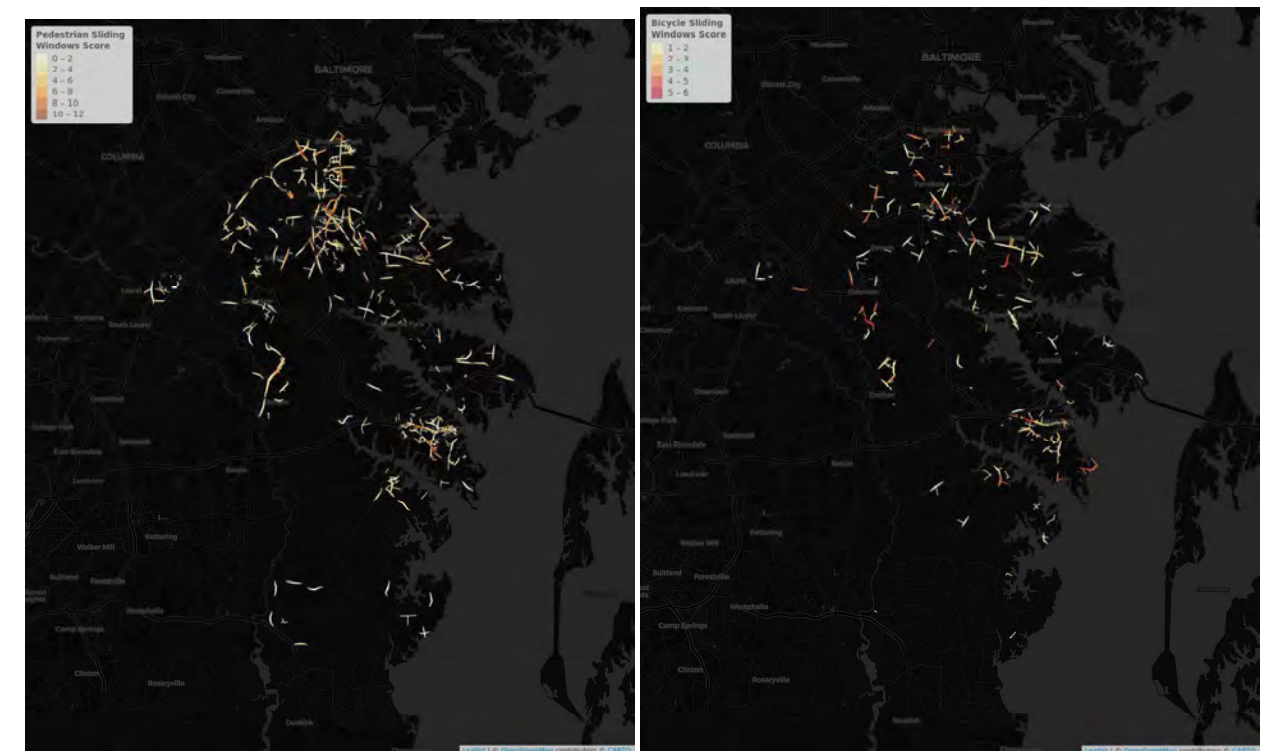


Figure 2 Maps of sliding windows analysis for pedestrian (left) and bicycle (right) crashes, using data from 2016 to 2020.

Our analysis found that 32% of all crashes in the County involved an injury, possible injury, or fatality. Between 2016 and 2020, there were nearly 1,600 recorded crashes involving a pedestrian or bicyclist. Notably, over half of all injury and fatal crashes occurred on arterial roads, despite making up just 15% of the County's road network. Arterial roads like Ritchie Highway, Mountain Road, or Baltimore-Annapolis Boulevard are typically large, high-speed roads, and they carry a larger number of vehicles and people, which creates more exposure for anyone traveling on them. Thus, these locations should be a key focus in improving the county's active transportation system. It also identified areas that have a high risk of being crash hot spots, which confirm what previous analyses of already recorded crashes show us.

Among areas with a high risk of pedestrian crashes, two are town centers (Glen Burnie and Parole) while others are busy commercial corridors with many destinations, such as Laurel-Fort Meade Road. Areas with a high risk of bicycle crashes are more diffuse, including arterial and collector roads in more residential areas, such as Harmans Road in Hanover and Catherine Avenue in Pasadena. Aligning with historical crash data in Move Anne Arundel!, the top pedestrian crash corridors are primarily state roads like Crain Highway North in Crofton, while

the top bicyclist crash corridors are county-owned roads like Catherine Avenue in Pasadena or Harmans Road in Severn. One possible explanation is that state roads are home to a significant amount of commercial activity that attracts people on foot, while local roads that are more residential in character may be common locations for recreational bicyclists.

The following are lists of the top high-risk pedestrian, bicyclists, and motorist-related crash corridors. Maps of these high-risk facilities can be found in the Appendix.

Top High Risk Pedestrian Crash Corridors

	Location
Baltimore-Annapolis Boulevard (MD 648)	Glen Burnie Town Center
Crain Highway North (MD 3)	Glen Burnie Town Center
Ritchie Highway (MD 2)	Glen Burnie
Forest Drive (County)	Parole Town Center
Hog Neck Road (MD 607)	Pasadena
Laurel-Fort Meade Road (MD 198)	Laurel
East Ordnance Road (MD 710)	Glen Burnie
East Furnace Branch Road (MD 270)	Glen Burnie

Top High Risk Bicyclist Crash Corridors

	Location
Mountain Road (MD 177)	Pasadena
Central Avenue (County)	Glen Burnie
Doris Avenue (County)	Brooklyn Park
Catherine Avenue (County)	Pasadena
Boulter's Way (County)	Arnold
Harmans Road (County)	Severn

Top High Risk Motorist Crash Corridors

	Location
Crain Highway South (MD 3)	Crofton
Annapolis Road (MD 175)	Odenton Town Center
Crain Highway North (MD 3)	Gambrills
Telegraph Road (MD 170)	Severn
Davidsonville Road (MD 424)	Crofton
Fort Smallwood Road (MD 173)	Pasadena
Mountain Road (MD 177)	Pasadena

Sincerely,

Dan Reed | Project Planner I

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Appendix C: Network Inventory Memorandum and Maps

MEMORANDUM

February 22, 2022

To: Tanya Asman, Bicycle and Pedestrian Planner
Organization: Anne Arundel County Office of Transportation
From: Dan Reed, Project Manager
Project: Pedestrian and Bicycle Master Plan

Re: Network Inventory

To find out where we should go, we must first understand where we are. Today, Anne Arundel County stands on the edge of a significant opportunity.

For most of the 20th century, the County's transportation network centered on the automobile. Today, the County's town centers and commercial districts are starting to evolve into walkable, bikeable places. Not only does this give people have a real alternative to driving, but it supports the County's environmental and economic development goals. However, there are still significant barriers to pedestrian and bicycle travel in other areas as well as gaps in the town centers and commercial districts themselves. This memo discusses the influence land use and street connectivity have on pedestrian and bicycle travel, the state of the County's pedestrian and bicycle networks, pedestrian and bicycle access to transit, and pedestrian and bicycle facilities in the County's three town center areas: Glen Burnie, Odenton, and Parole.

Each section in this memo refers to maps which can be found in the accompanying appendix.

Land Use and Street Connectivity

Anne Arundel County has a unique geography, with approximately 817 miles of shoreline,¹ sought-after suburban neighborhoods, an extensive highway network, and large institutional facilities, such as Fort Meade and BWI Thurgood Marshall Airport. As seen in Figure 1, this geography also creates some barriers to pedestrian and bicycle mobility. In particular, the County's:

- Many peninsulas, some of which are densely populated, often rely on just a single or few roads or bridges for access.
- Hierarchical street network, with cul-de-sacs and smaller roads feeding into larger roads with limited parallel alternatives, can require people to take long, circuitous routes to reach local destinations.
- Major highways, such as Route 50 and I-97, restrict local connections to a limited number of underpasses and overpasses.
- Main arterial corridors such as Routes 2, 3, 214, 177 and others are very congested and extremely difficult to travel along or across for pedestrians and cyclists

- The County's dominant single-use zoning pattern means homes can be far away from shopping, jobs, or public amenities like parks.
- The County's large institutional facilities sometimes require travelers to detour around them, a relatively minor inconvenience for drivers but often a significant barrier for slower-moving, distance-averse pedestrians and bicyclists.

These barriers mean there are few through connections, so those connections that do exist must be safe and comfortable for active transportation, as many trips will have no alternative. A neighborhood may have a network of low-speed, low-stress streets, but if a single inhospitable bridge or overpass or arterial road lies between them and a popular destination – like a school or a shopping center – residents are more likely to drive or avoid the trip entirely. These missing connections also make it difficult for the County's most vulnerable populations, such as households without cars or those living below the poverty line, to access daily needs or economic opportunities.

The street network provides the “bones” for much of the pedestrian and bicyclist network, but separated pedestrian and bicyclist facilities are critical for the safety and comfort of people who are walking, bicycling, or rolling with a wheelchair, scooter, or other small, wheeled device, particularly on streets with higher motor vehicle speeds and volumes.

Pedestrian Network

Pedestrian Network Overview

The County's pedestrian network consists of sidewalks and other pedestrian paths, shared use trails and sidepaths, and safe, comfortable pedestrian crossings. This network is used for a wide range of activities, including access to transit, travel to work and school, access to parks, libraries, and other public facilities, shopping, exercising, walking the dog, children's play, and many others. While the pedestrian network is most functional in urban areas where the potential for pedestrian travel for transportation purposes is highest, it is also important in other areas and for other purposes, including social and recreational purposes.

The need for safe pedestrian infrastructure is increasing in suburban and exurban areas, as people with lower incomes and less access to private cars increasingly move from more urban to more suburban areas in search of cheaper housing, a phenomenon known as the “suburbanization of poverty.”^{2,3} This phenomenon has increased pedestrian demand in places where pedestrian conditions are often more challenging and may be a contributing factor in the spike in pedestrian fatalities nationally since approximately 2010⁴.

Sidewalks

Sidewalks are essential to Anne Arundel County's transportation system and quality of life. Almost every Anne Arundel County resident is a pedestrian and almost every Anne Arundel County resident uses a sidewalk at some point in their day. For a map of sidewalks, see Figure 2. Sidewalks are often the only safe place for people traveling by bike and in 2021 the County passed an ordinance making safe bike riding on sidewalks legal.

Sidewalks are particularly important on arterial and collector streets in urban and suburban areas where pedestrian travel is concentrated. Arterial and collector streets have higher motor vehicle volumes and speeds, which increases the importance of sidewalks for safety. At the same time, pedestrian destinations, such as transit stops, retail areas, parks, and schools, are usually clustered on arterial and collector streets, which increases their importance for access.

² *Suburbanization of Poverty: Trends in Metropolitan America, 2000-2008*, Brookings Institution, Metropolitan Policy Program, p. 9. https://www.brookings.edu/wp-content/uploads/2016/06/0120_poverty_profiles.pdf

³ *Suburbanization of poverty impacts Baltimore region* [Editorial] - Baltimore Sun

⁴ *Pedestrian Deaths: A 'Silent Epidemic' Fueled By Race, Class And Car Design* | News | Ideastream Public Media

¹ Maryland Geological Survey--[Land Areas, Inland-Water Areas, and Length of Shorelines of Maryland's Counties \(md.gov\)](http://www.maryland.gov/land_areas_inland-water_areas_and_length_of_shorelines_of_maryland_s_counties)

Anne Arundel County has approximately 2,492 miles of sidewalks and other pedestrian pathways, according to the County's GIS data. Sidewalks are concentrated in urban areas of the County, which is also where the most complete sidewalk networks are found, although these networks often have significant gaps. Outside urban areas, sidewalks are less connected and consistent. In suburban areas, relatively complete sidewalk networks may exist within a subdivision but not along an arterial or collector street that connects the subdivision to places where people want to walk and roll, such as transit stops, schools, parks, and retail areas.

Crossings

Safe, comfortable pedestrian crossings are another critical component of Anne Arundel County's pedestrian network. On arterial and collector streets, a form of traffic control, i.e., traffic light, pedestrian hybrid beacon, or STOP sign, is typically required to make a crossing safe and comfortable for pedestrians. At intersections, signs and signals that regulate motor vehicle turning movements also play an important role, with free right turns and permissive lefts generally undermining pedestrian safety and comfort. Other important features include good lighting, high-visibility crosswalk markings, pedestrian signals with countdown timers, and pedestrian crossing islands. For a map of marked and unmarked crosswalks,⁵ see Figure 3.

The spacing of these safe crossing opportunities, and where they are located relative to pedestrian destinations, also affects safety, particularly on arterial and collector roads. In Anne Arundel County safe crossing opportunities on arterial and collector roads are concentrated in urban areas. However, in some cases they are widely spaced, not well-coordinated with transit stops and other significant pedestrian attractors or require pedestrians to wait a long time for their turn to cross. Additionally, it is common practice in Anne Arundel County not to mark all crosswalks at an intersection, which makes crossings less convenient for pedestrians, who if they were to stick to the marked crosswalks would in some cases have to make two or three crossings where one or two would suffice if all crosswalks were marked, and which also increases pedestrian exposure to motor vehicle traffic. The more inconvenient safe pedestrian crossings are, the more likely pedestrians are to either engage in unsafe behaviors, such as crossing midblock or against the light, to drive instead of walking, or to dispense with the trip entirely.

The accessibility of pedestrian crossings is important for all pedestrians, particularly pedestrians with disabilities. In Anne Arundel County, while most intersections with sidewalks have curb ramps, very often only one diagonal or apex ramp is provided on each corner, rather than two curb ramps aligned with the crosswalks they serve. Curb ramps that are misaligned with crosswalks by more than 30% make it more likely pedestrians with vision disabilities will veer outside the crosswalk into moving traffic.⁶ Misaligned crosswalks may also expose people in wheelchairs, parents pushing strollers, and others to moving traffic.

⁵ An unmarked crosswalk is a legal crosswalk without crosswalk markings. MD Transp Code § 21-101 (2016) defines a legal crosswalk as follows:

- i) Crosswalk. -- "Crosswalk" means that part of a roadway that is:
- (1) Within the prolongation or connection of the lateral lines of sidewalks at any place where 2 or more roadways of any type meet or join, measured from the curbs or, in the absence of curbs, from the edges of the roadway;
 - (2) Within the prolongation or connection of the lateral lines of a bicycle way where a bicycle way and a roadway of any type meet or join, measured from the curbs or, in the absence of curbs, from the edges of the roadway; or
 - (3) Distinctly indicated for pedestrian crossing by lines or other markings.

Unmarked crosswalk is defined as ""that part of a roadway that is within the [continuation] or connection of the lateral lines of sidewalks at any place where 2 or more roadways of any kind meet or join, measured from the curbs, from the edges of the roadway." In this situation, a legal crosswalk exists even if it is not marked.

⁶ [Nonvisual Cues for Aligning to Cross Streets \(nih.gov\)](https://www.nih.gov)

Bicycle Network

Bicycle Network Overview

Since the passage of its 2013 Pedestrian and Bicycle Master Plan, the County has made significant progress in building out its bicycle network. Today, the County has nearly 100 miles of off-road and on-road bicycle facilities, as well as another 20 miles of on-road bicycle supportive infrastructure, according to GIS data (Table 1). There are approximately 2,600 miles of roads in the County⁷. Facilities that are physically separated from motor vehicle traffic are particularly important on streets with higher motor vehicle speeds and volumes, since otherwise the average bicyclist is unlikely to use the facility. For a map of current and proposed bicycle facilities, see Figure 4.

Table 1 Bicycle Facilities and Supportive Infrastructure (Existing, Programmed, Planned)

Facility Type	Miles Existing	Miles Under Construction or Programmed	Miles Planned
Off-Road Bicycle Facilities			
Trails/sidepaths	60.4	7.9	38.9
On-Road Bicycle Facilities			
Separated Bike Lane	0.1	0.0	0.0
Bike Lane	36.6	3.2	78.5
On-Road Bicycle Supportive Infrastructure			
Shared Lane Markings (Sharrows)	3.9	3.1	11.3
Signed Bicycle Routes	15.3	0.0	87.2

Trails and Sidepaths

A significant part of the County's bicycle network are paved trails and sidepaths, which comprise more than half of all bicycle facilities. Trails are bicycle facilities that are physically separated from motor vehicle traffic by an open space or barrier and within an independent right of way. Sidepaths are similarly separated but within the roadway right of way. Trails and sidepaths may also be used by people who are walking, rolling, or jogging. The County may use these terms interchangeably; for example, the Broadneck Peninsula Trail is next to a roadway, making it functionally a sidepath.

Anne Arundel County currently has over 60 miles of trails and sidepaths, several of which have become regional attractions. Many County trails are part of national trail routes, such as the East Coast Greenway, September 11 Memorial Trail, and the American Discovery Trail. Major County trails include:

- The B&A Trail, which is 13.7 miles and stretches from Boulders Way in Annapolis to Dorsey Road in Glen Burnie, Maryland.
- The BWI Trail, which is a 10.8-mile loop trail around the BWI Thurgood Marshall Airport.
- The Broadneck Peninsula Trail, which is a planned 9 mile trail from the B&A Trail to Sandy Point State Park. 2.4 miles are completed along College Parkway from Bay Dale Drive to College Parkway East.
- The planned South Shore Trail from Annapolis to Odenton with one phase done and the second to begin construction in 2022.

⁷ Move Anne Arundel! identifies approximately 1,211 miles of roads maintained by MDOT SHA, 1,317 miles of roads maintained by the County, and 92 miles of roads maintained by the City of Annapolis.

On-Road Bicycle Facilities

On-road bicycle facilities are facilities dedicated for bicycle use within the roadway. This can include conventional bike lanes, separated bike lanes, and bike boulevards, or streets where bicycles share space with motor vehicles.

Anne Arundel County has almost 37 miles of conventional bike lanes, which also include road shoulders that have been marked as bike lanes. The County's bike lanes often connect with County trails, leveraging the value of the trail network. There are two separated bike lanes: a one-way bike lane on the Ridgely Avenue bridge over Route 50, and a new, two-way bike lane on Jennifer Road in Parole Town Center.

On-Road Bicycle Supportive Infrastructure

On-road bicycle-supportive infrastructure is intended to support bicycling that does not provide dedicated space for use by people who are bicycling in the street right-of-way. This category of infrastructure includes streets with shared lane markings or "sharrows", and signed bicycle routes. Anne Arundel County currently has approximately 20 miles of bicycle supportive infrastructure. Much of it is on arterial and collector roadways, where the average bicyclist is likely to feel uncomfortable absent physical separation from motor vehicle traffic, and most of it consists of signed bicycle routes.

Crossings

Crossings are an important aspect of Anne Arundel County's bicycling network, especially on arterial and collector streets. Such infrastructure includes controlled crossings, bike boxes to facilitate left turns, bike lane continuation markings, and protected intersections.

Other than controlled crossings, this infrastructure is currently largely absent from Anne Arundel County. Controlled crossings are relatively infrequent on arterial and collector roadways outside the town centers and other more urban areas of the County. The lack of such crossings can be a significant barrier to bicycle travel, particularly in cases where the arterial or collector lacks a separated bicycle facility or parallel neighborhood street that would enable the bicyclist to travel to a protected crossing and continue their trip.

Connections to Transit

Anne Arundel County has a wealth of transit options that connect it to the rest of the Baltimore-Washington region and beyond, as shown in Figure 5

Two MARC lines serve the County. There are two stations on the Penn Line, Odenton and BWI Rail. On the Camden Line, there are five stations that serve the County: Laurel, Laurel Park, Savage, Jessup, and Dorsey. Amtrak also provides service to the BWI Rail and Odenton stations.

Several commuter buses serve the County, providing service to downtown Baltimore, downtown Washington, DC, the I-270 Technology Corridor in Montgomery County, and Kent Island. These buses primarily stop at park-and-ride lots, though they also serve major destinations within the County, including downtown Annapolis and Arundel Mills Mall.

MTA's LightRailLink has seven stations in the County, with service between BWI Airport, Glen Burnie, and points north in Baltimore City and Baltimore County. Additionally, there are three providers of local bus service. The Maryland Transit Administration, whose CityLink runs several routes, primarily in the northern part of the County with connections to Baltimore City. Some of these routes form a trunk line along Ritchie Highway (Route 2) between Brooklyn Park and Annapolis, while other lines serve major job centers in the western part of the County, including BWI Business District and Arundel Mills Mall. The RTA of Central Maryland provides service in the western part of the County, with connections to Howard and Prince George's counties. Annapolis Transit provides service within the City of Annapolis and to surrounding areas, including Parole and Edgewater.

The County has invested in transit-oriented development around some stations that allows people to walk, roll, or bike to the station easily, such as at the Odenton MARC Station, and ongoing trail projects will improve connections to several stations, such as the Baybrook Connector between Brooklyn and Nursery Road Light Rail and BWI Trail extensions to Nursery Road and Odenton.

There are many opportunities to improve connections to transit service in the County. Whether isolated or proximate to homes and businesses, many rail stations and commuter bus stops are difficult to reach without a car. Bus stops along major roads such as Route 2 can be challenging to walk to due to a lack of sidewalks or crosswalks or ADA-accessible facilities. Short connections between existing trails like the WB&A and B&A and transit stops could dramatically increase accessibility by giving people across a wide part of the County a low-stress route to the station. Along major roads, intersection improvements could make it easier to access local bus service, incentivizing its use.

Town Centers

Anne Arundel County's three town centers are among its most pedestrian and bicycle friendly places. This section provides an overview of pedestrian and bicycle conditions in and near the Glen Burnie, Odenton, and Parole town centers. In these communities, Move Anne Arundel! sets specific goals for encouraging walking, biking and transit use. Table 2 below summarizes sidewalk coverage by functional classification on arterial, collector, and local streets within a half-mile buffer of each of the town centers.

Table 2: Percent Street Miles with Sidewalk on Both, One, or No Sides in Glen Burnie, Odenton, and Parole within a Half-Mile Buffer of Town Center⁸

Town Center Area	Street mileage within 0.5 mi. buffer of Town Center	Arterial Streets			Collector Streets			Local Streets		
		% of street length with sidewalks			% of street length with sidewalks			% of street length with sidewalks		
		2 Sides	1 Side	0 Sides	2 Sides	1 Side	0 Sides	2 Sides	1 Side	0 Sides
Glen Burnie	32.3	28%	24%	47%	51%	39%	9%	33%	16%	51%
Odenton	97.4	8%	49%	43%	21%	25%	54%	37%	18%	46%
Parole	87.6	18%	44%	37%	20%	20%	60%	17%	26%	57%

Glen Burnie Town Center

Pedestrian Access

Glen Burnie is the County's largest town center (see Figure 6) and has the most extensive sidewalk network, with many local and collector streets having sidewalks on both sides. However, there are significant gaps in Glen Burnie's sidewalk network, particularly on Ritchie Highway north of Glen Burnie Town Center. There are also gaps in the sidewalks along Baltimore Annapolis Boulevard between Glen Burnie and the Cromwell Light Rail station that make it difficult for pedestrians to access the station. The Glen Burnie Heights neighborhood, south of the town center, lacks sidewalks entirely.

Controlled crossings are provided on arterials and collectors in and near Glen Burnie Town Center; however, there are sometimes long distances between these crossing opportunities. For example, on Crain Highway south of the town center, there is a gap of ¾ miles between the signal at Quarterfield Road and the signal at Central

⁸ The percentages provided in this table are approximate. They were developed by classifying each street segment as having sidewalk on 2 sides, 1 side, or 0 side. A street segment was considered to have sidewalk on a given side if the length of sidewalk falling within a 50-foot buffer of that side was at least 75% of the street segment length. Once classified, the segments lengths were then summed and divided by the total length or roadway for each functional classification category within the buffered area to determine the percentages by functional classification.

ald's, and other uses that generate pedestrian trips.

ist Access

urnie Town Center is served by the B&A trail, which parallels Baltimore Annapolis Boulevard and Ritchie y, the BWI Trail, and the John Overstreet Connector Trail, which provides a connection between them. are no on-street bicycle facilities, and there are few comfortable connections to the B&A Trail.

on Town Center

rian Access

ewer developments in the Odenton Town Center area (see Figure 7) have complete or near complete lk networks internally, including The Village at Odenton Station. However, sidewalks are less consistently le outside these developments. Among arterials and collectors, Town Center Boulevard has continuous lks on both sides, but there is a gap on Annapolis Road between the Baldwin Road and Town Center ard that affects access to the Odenton MARC Station and other destinations from the 322 Baldwin ent Homes and Echelon at Odenton as well as gaps east of Town Center Boulevard, particularly east of rchard Road, where neither side of the street features continuous sidewalk, which also affect access.

lks are also missing on the east side of Piney Orchard Road south of Odenton Road, as well as on n Road itself east of Piney Orchard (north side) and west of Piney Orchard (both sides). Finally, although king lots flanking the Odenton MARC station do include some sidewalks, pedestrian access could be ed by providing additional sidewalks to accommodate pedestrian desire lines, such as from Duckens Becknel Avenue, and Hale Street.

gh controlled pedestrian crossing opportunities on arterial and collector streets are relatively closely spaced e Odenton MARC Station, they are less frequent outside of this area. For example, on Annapolis Road re no controlled pedestrian crossing opportunities between Town Center Boulevard and the Patuxent y access ramps, where there are also no marked crosswalks or pedestrian signals, a distance of nearly ½ ere are no controlled pedestrian crossing opportunities on Town Center Boulevard north of Annapolis ard, including at Hale Street, which provides a connection between an existing neighborhood and the station parking lot. There are also no protected crossing opportunities on Piney Orchard Road between n Road and Waugh Chapel Road, a distance of over 1 mile, or on Odenton Road between Annapolis nd Piney Orchard Road, a distance of almost 1 mile.

ist Access

n Town Center is served by the WB&A trail from the south, a sidepath on Town Center Boulevard from the and the Odenton Bike Path to the east. The primary barriers to bicycling are Route 175 (Annapolis Road) ute 170 (Telegraph Road) north of Route 175, both of which lack bicycle facilities. Route 175 is particularly nt connection for bicyclists, because it is one of the only streets near the Town Center that provides an est connection over the CSX railroad tracks. Route 32 (Patuxent Freeway) is also a major barrier, because mited access freeway with high-speed motor vehicle traffic. There is, however, a wide shoulder on Route has bicycle markings south of Route 175.

Parole Town Center

Pedestrian Access

Parole (see Figure 8) is home to the Annapolis Town Center at Parole development, one of the County's premier shopping and nightlife areas. The sidewalk network inside Annapolis Town Center is near complete, and sidewalks are largely continuous on the streets adjacent to it, except for gaps the on south side of West Street across a gas station property, on the east side of MD 2 (Solomons Island Road), and on the south side of Forest Drive near MD 2.

Sidewalk coverage on West Street is generally good on both sides, but sidewalks are largely absent on MD 2, especially south of Forest Drive, affecting pedestrian access from neighborhoods such as Dorsey Heights to the south and Parole Manor to the east.

Once you get outside the core area on major roads, controlled pedestrian crossings are few and far between. There is no safe crossing from the residential area south of Bestgate Road to Bestgate Park, St. Phillips Episcopal Church, and other destinations on the north side of Bestgate Road. To cross safely at a signalized intersection, residents from the neighborhood must walk west to Admiral Drive, a distance of ¾ mile.

Bicyclist Access

Parole Town Center is served by a sidepath along Route 2 from Forest Drive to Jennifer Road and by the County's first separated two-way bike lane, on Jennifer Road near Anne Arundel Medical Center. However, there are no comfortable bike routes to Parole Town Center or the developments that border it, including Festival at Riva, Harbor Center, Forest Plaza, and Westfield Annapolis Mall. The planned extension of the Poplar Trail westward to Route 2 and eastward to downtown Annapolis will help. Connections are also needed to Harbor Center and between the mall area and Town Center.

US 50 and MD 665, which are controlled access highways, present barriers to pedestrians and bicyclists, with crossings limited to interchanges.

Conclusion

Despite the challenges described in this memo, significant opportunities remain, and this plan will lay out a process by which we can take advantage of the opportunity to create a safe, connected, and accessible community.

Sincerely,

Dan Reed | Project Planner I

TOOLE DESIGN

8484 Georgia Avenue, Suite 800 | Silver Spring, MD 20910
Email@tooledesign.com | 301.927.1900 x143

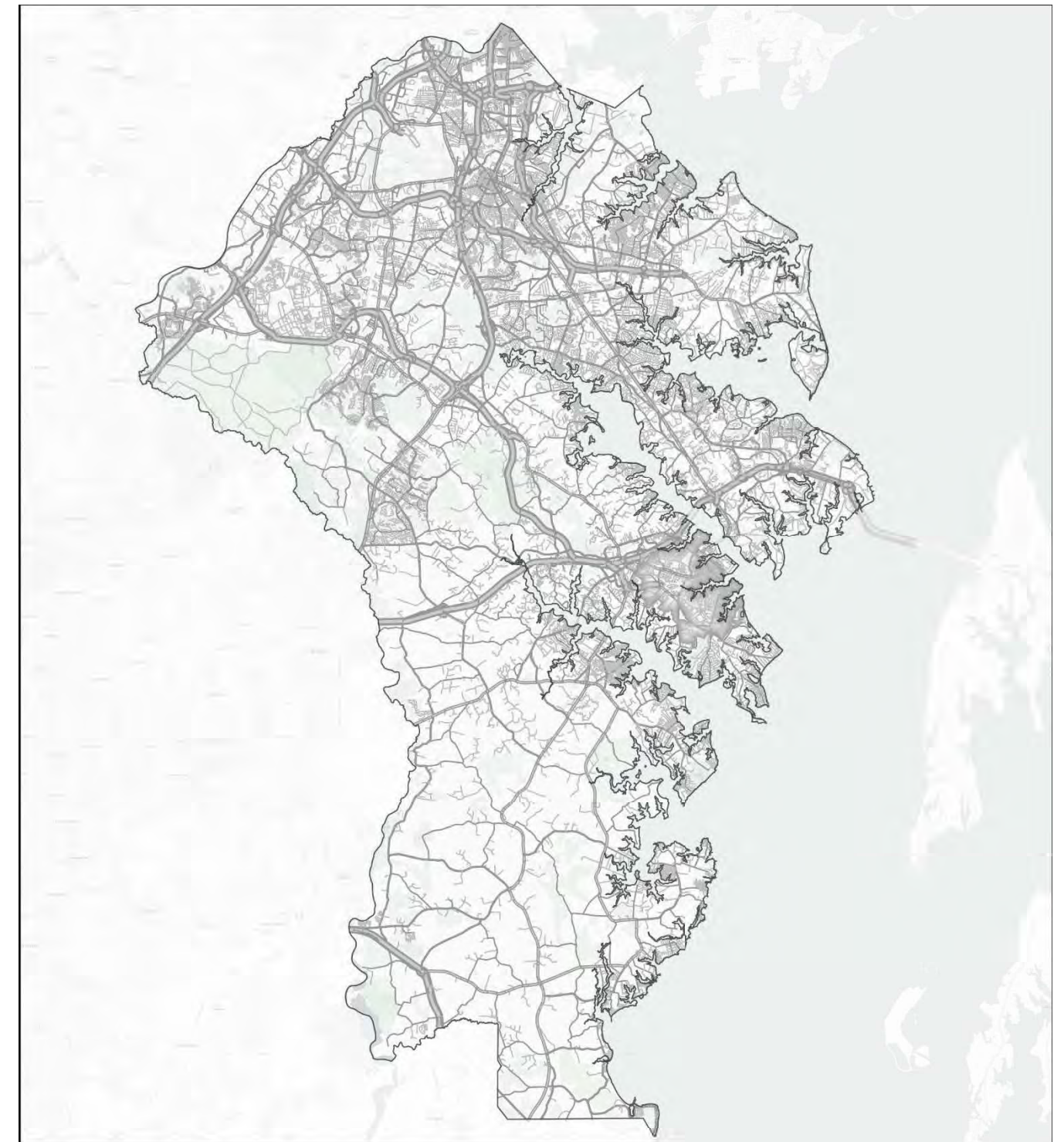
MEMORANDUM

February 22, 2022

To: Tanya Asman, Bicycle and Pedestrian Planner
Organization: Anne Arundel County Office of Transportation
From: Dan Reed, Project Manager
Project: Pedestrian and Bicycle Master Plan

Re: Appendix

On the following pages are the maps referenced in the Task 1 Network Inventory memo.



Anne Arundel County Street Types

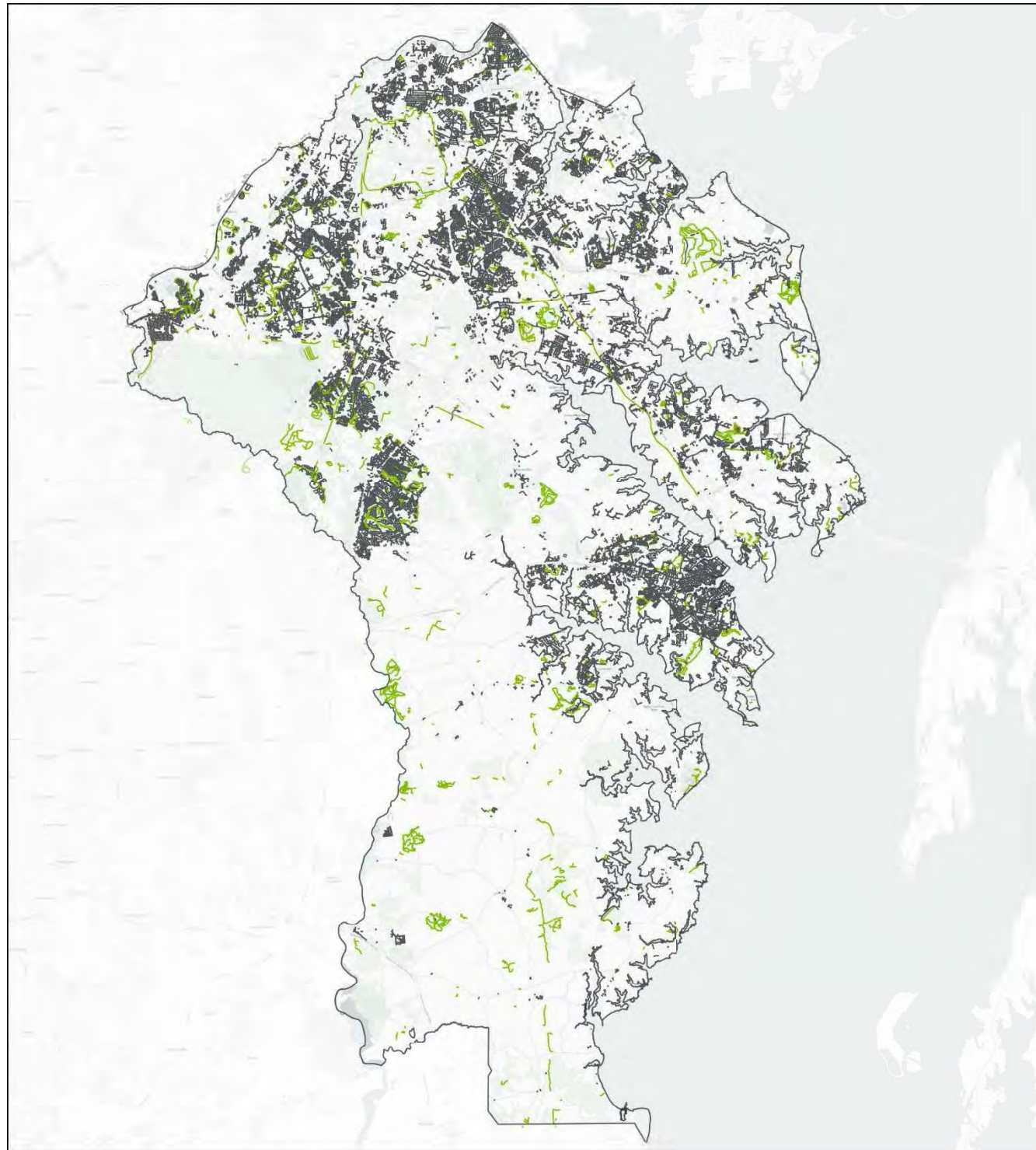
Street Network

0 1 2 3 4 5 mi



- Freeway
- Principal Arterial
- Minor Arterial
- Local

Figure 1 Street Network



Anne Arundel County

Pedestrian Network

0 1 2 3 4 5 mi

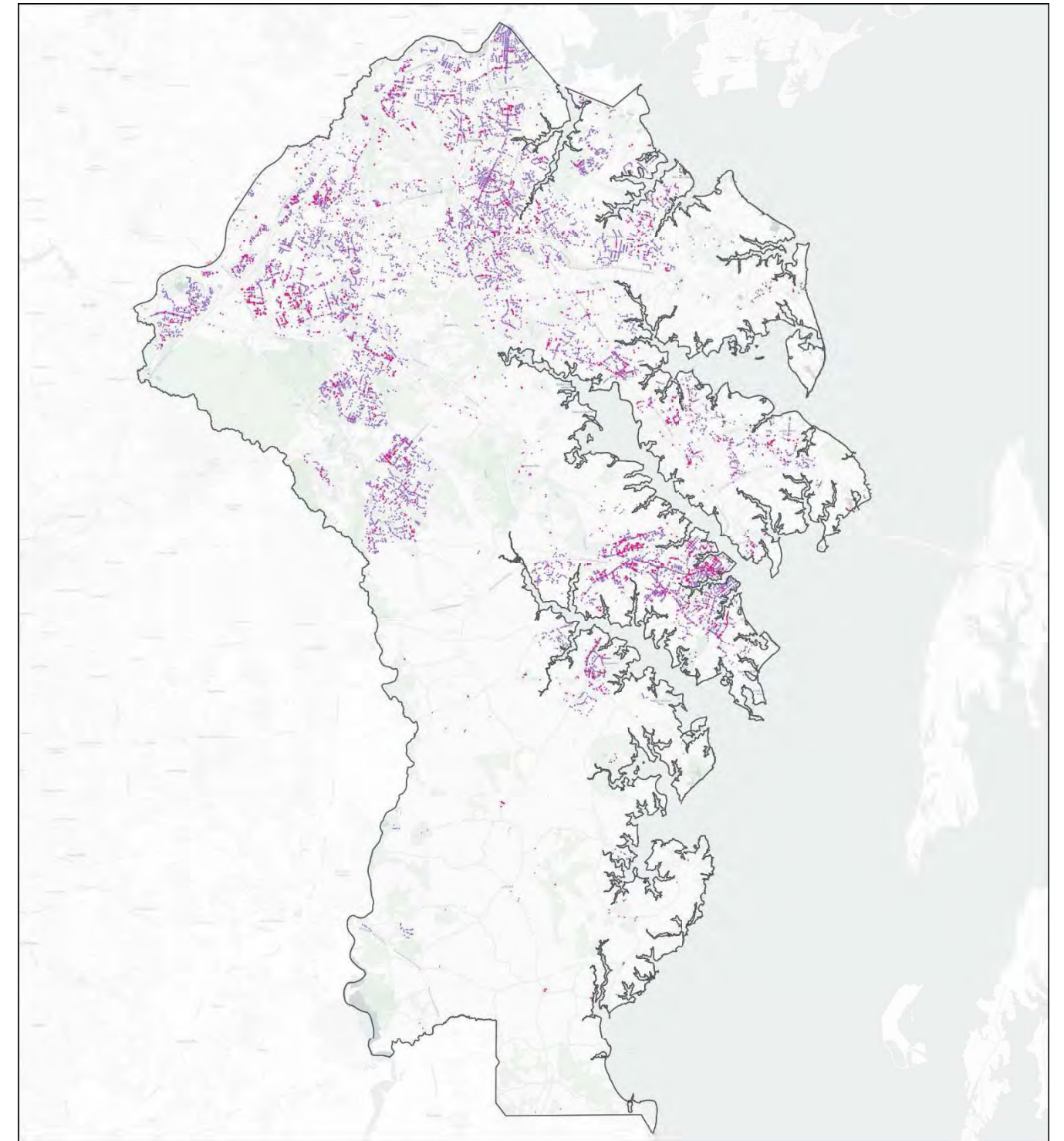


Pedestrian Facilities

- Sidewalk
- Path



Figure 2 Sidewalks + Paths



Anne Arundel County

Pedestrian Network

0 1 2 3 4 5 mi



Crosswalks

- Marked
- Unmarked



Figure 3 Crosswalks

Note: Unmarked crosswalks are crossings at intersections where no markings are present. Under Maryland state law, pedestrians have the right-of-way in unmarked crosswalks only when located at traffic lights.

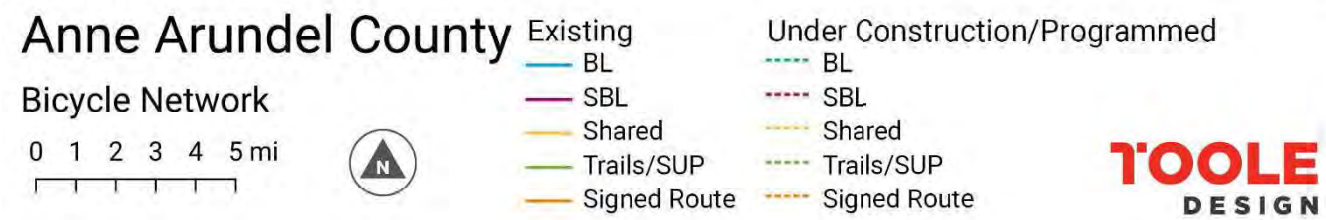
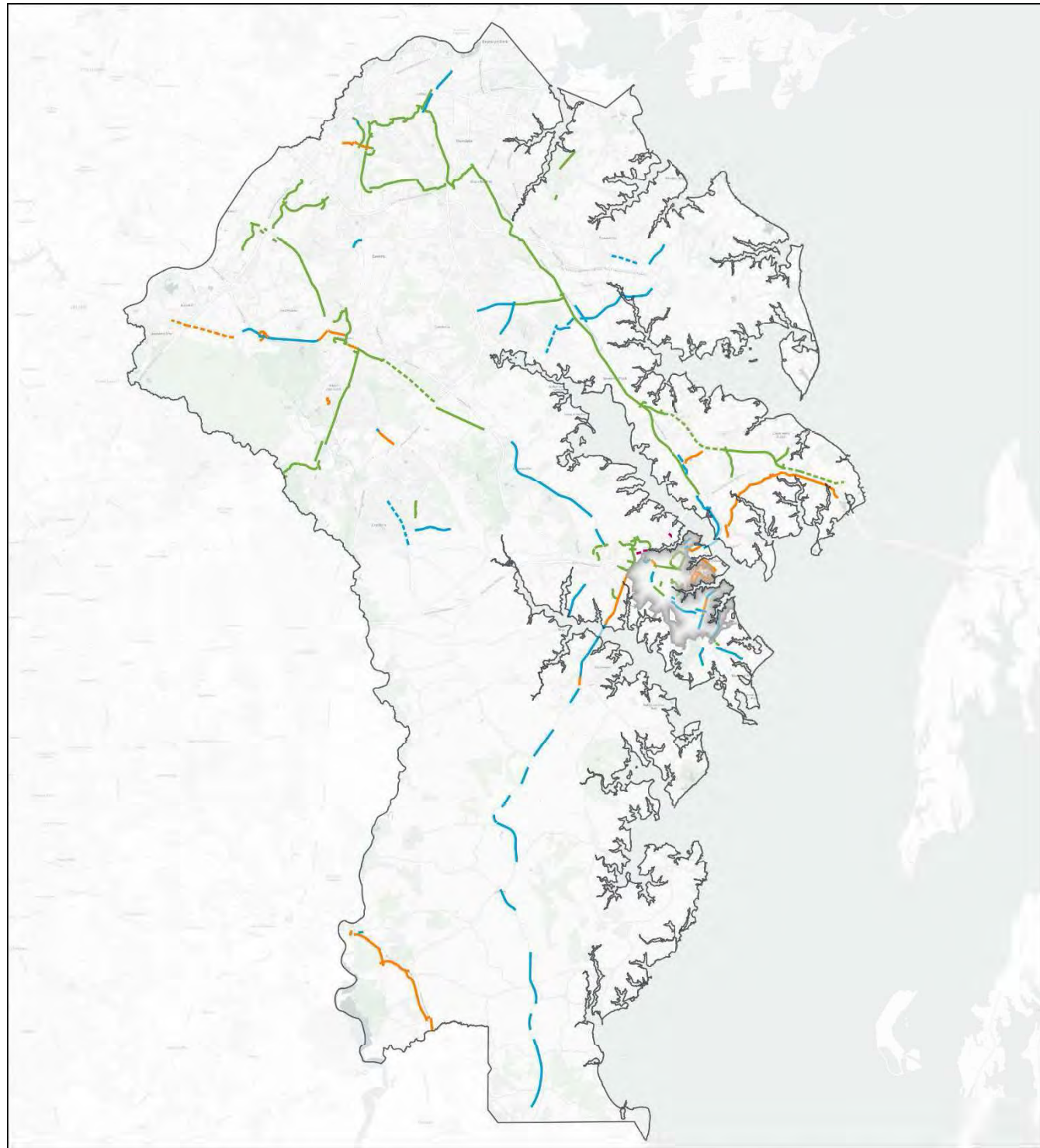


Figure 4 Bicycle Network

Note: Signed routes are streets that are designated bike routes where bicyclists are encouraged to ride, but do not have dedicated bicycle facilities.

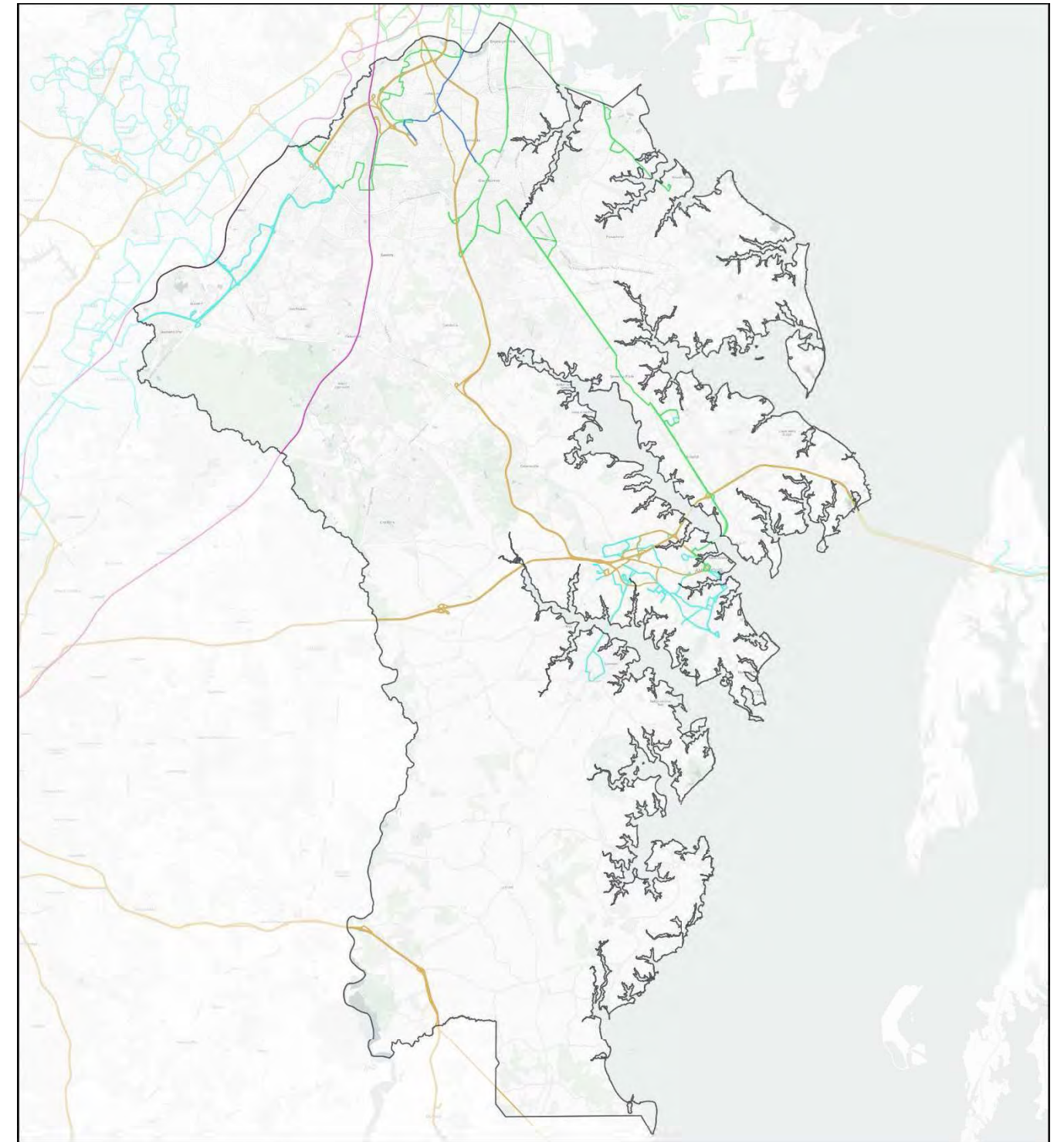
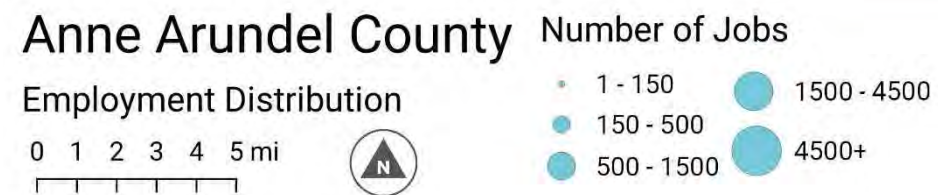
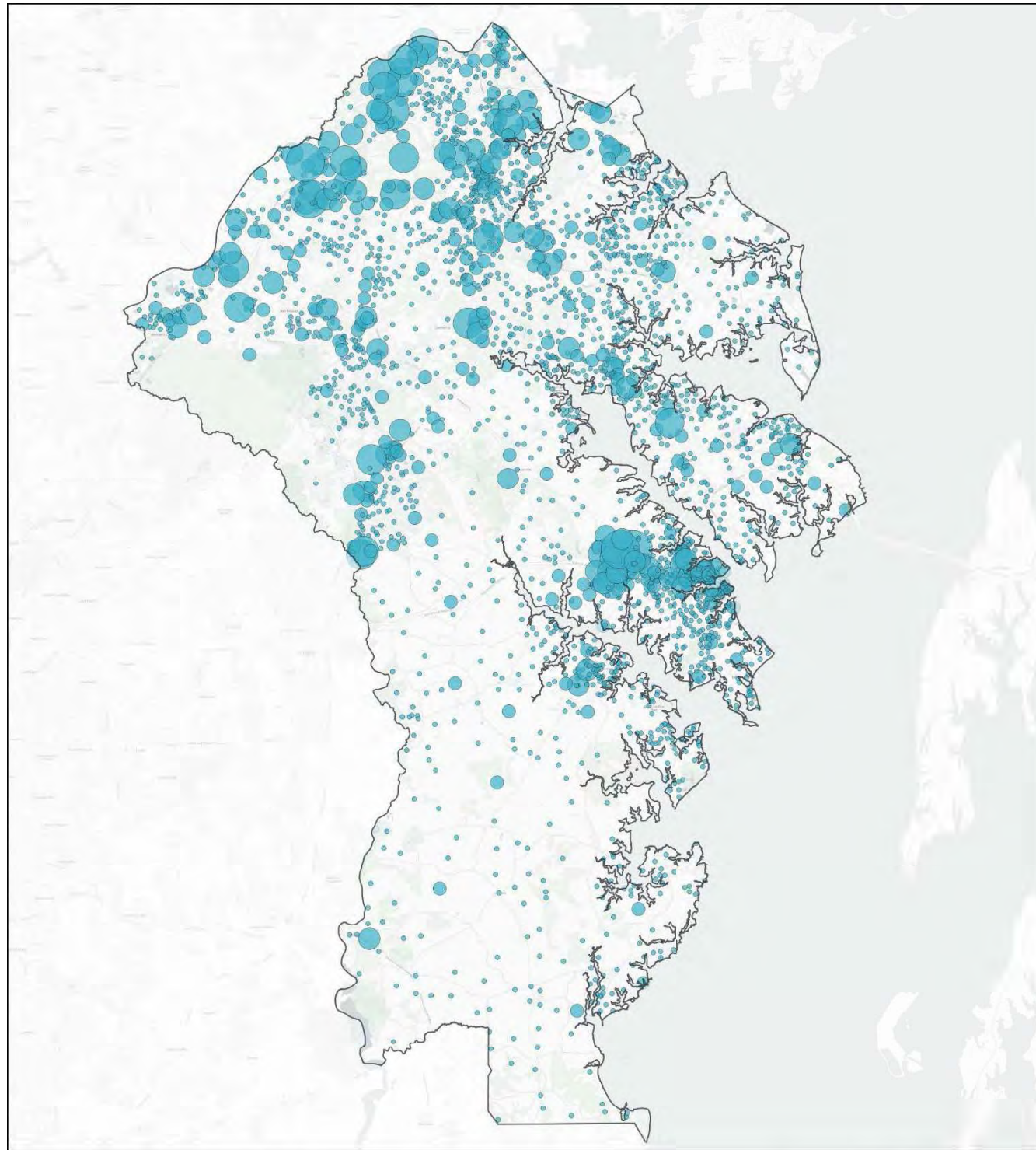


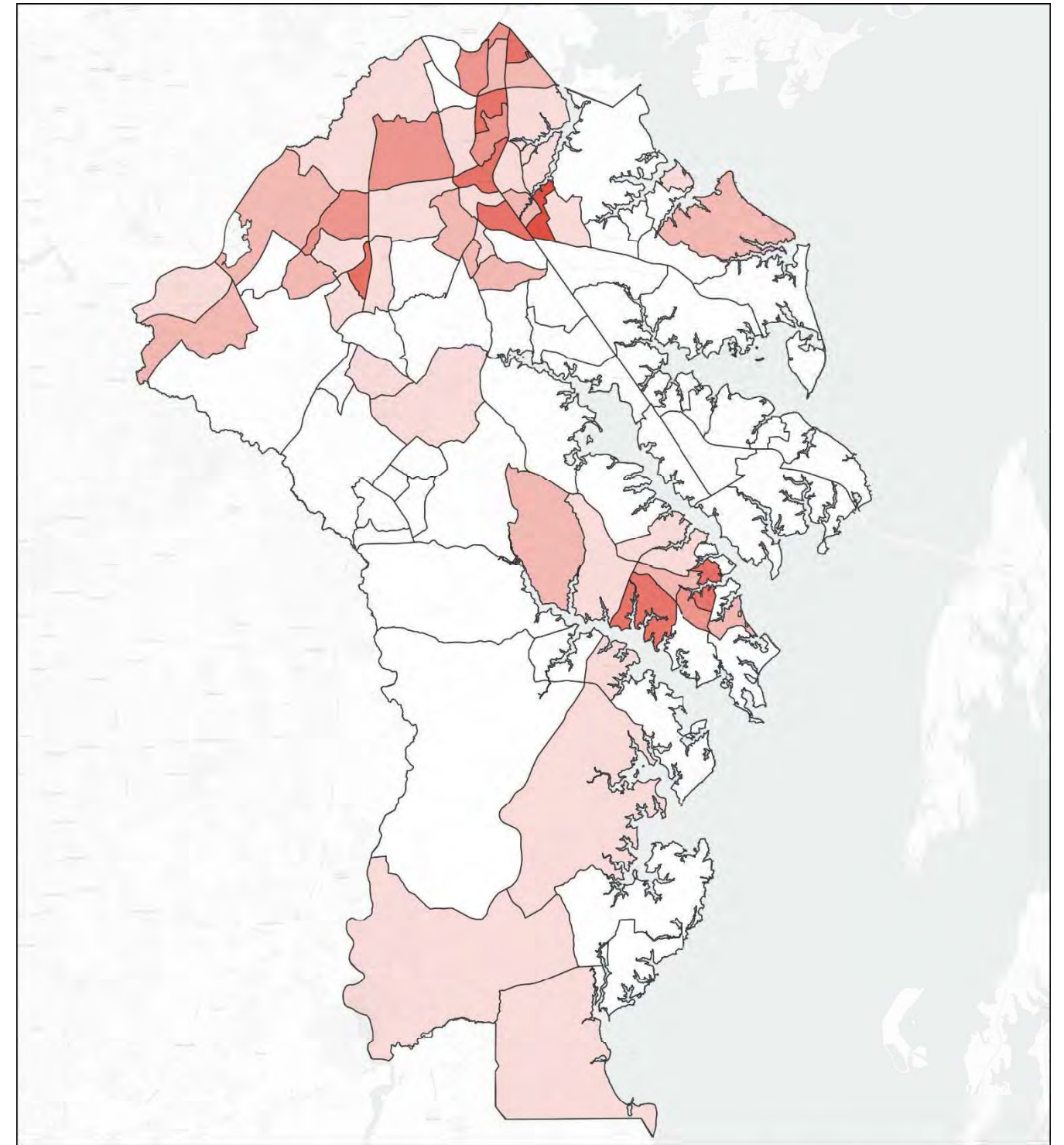
Figure 5 Transit Network



Source Data: US Census Bureau
2019 LEHD Origin-Destination
Employment Statistics (LODES)



Figure 6 Employment Distribution

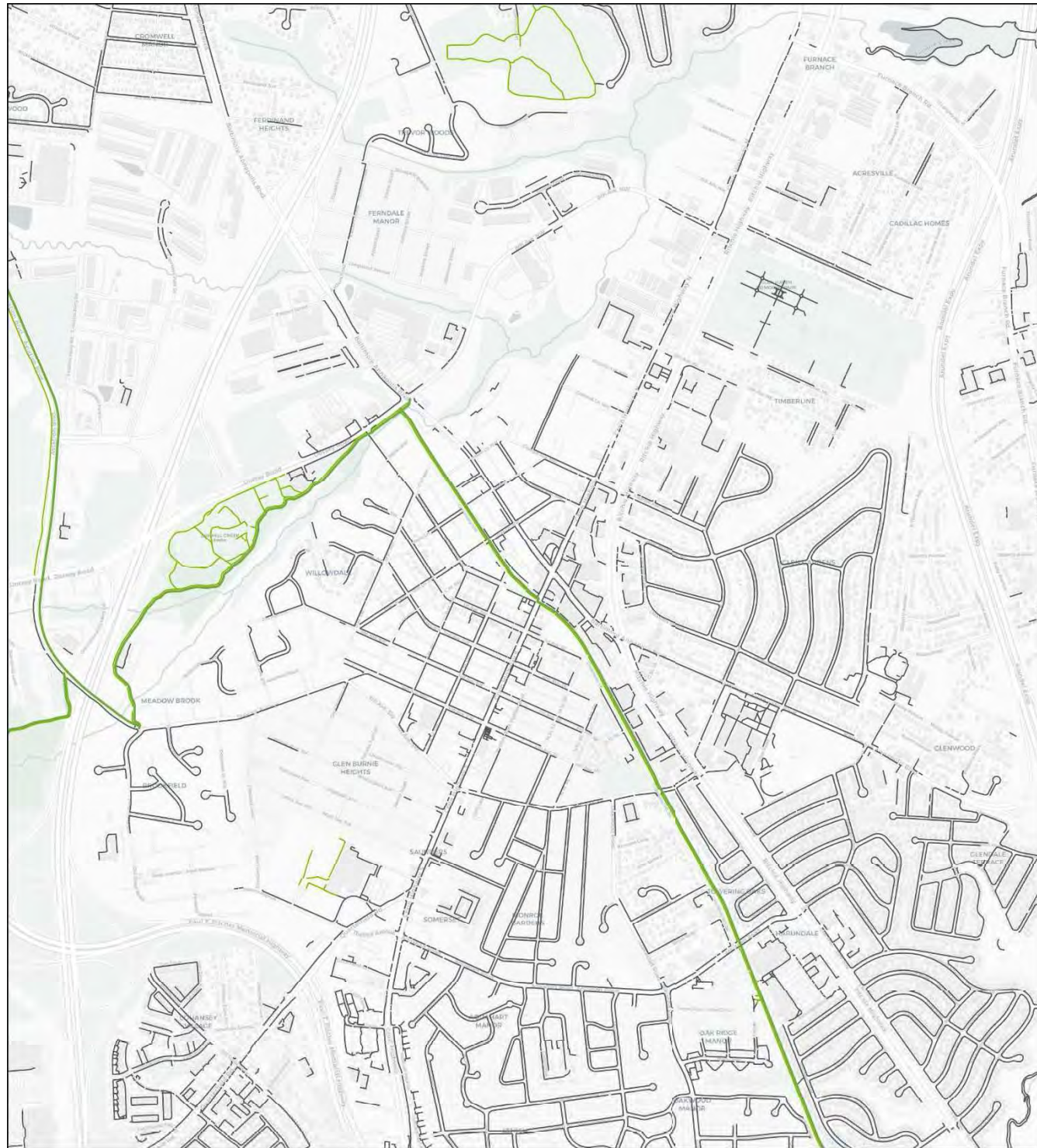


VPI is a composite score developed by BMC to identify census tracts with higher concentrations of 7 determinants of transportation vulnerability. Maximum VPI score is 14, and the maximum in Anne Arundel County is 6.



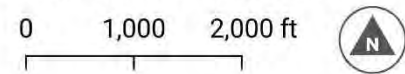
Figure 7 Vulnerable Population Index

Note: VPI is a composite score developed by the Baltimore Metropolitan Council to identify census tracts with a high concentration of vulnerable populations, including car-free households and households living below the poverty line.



Anne Arundel County

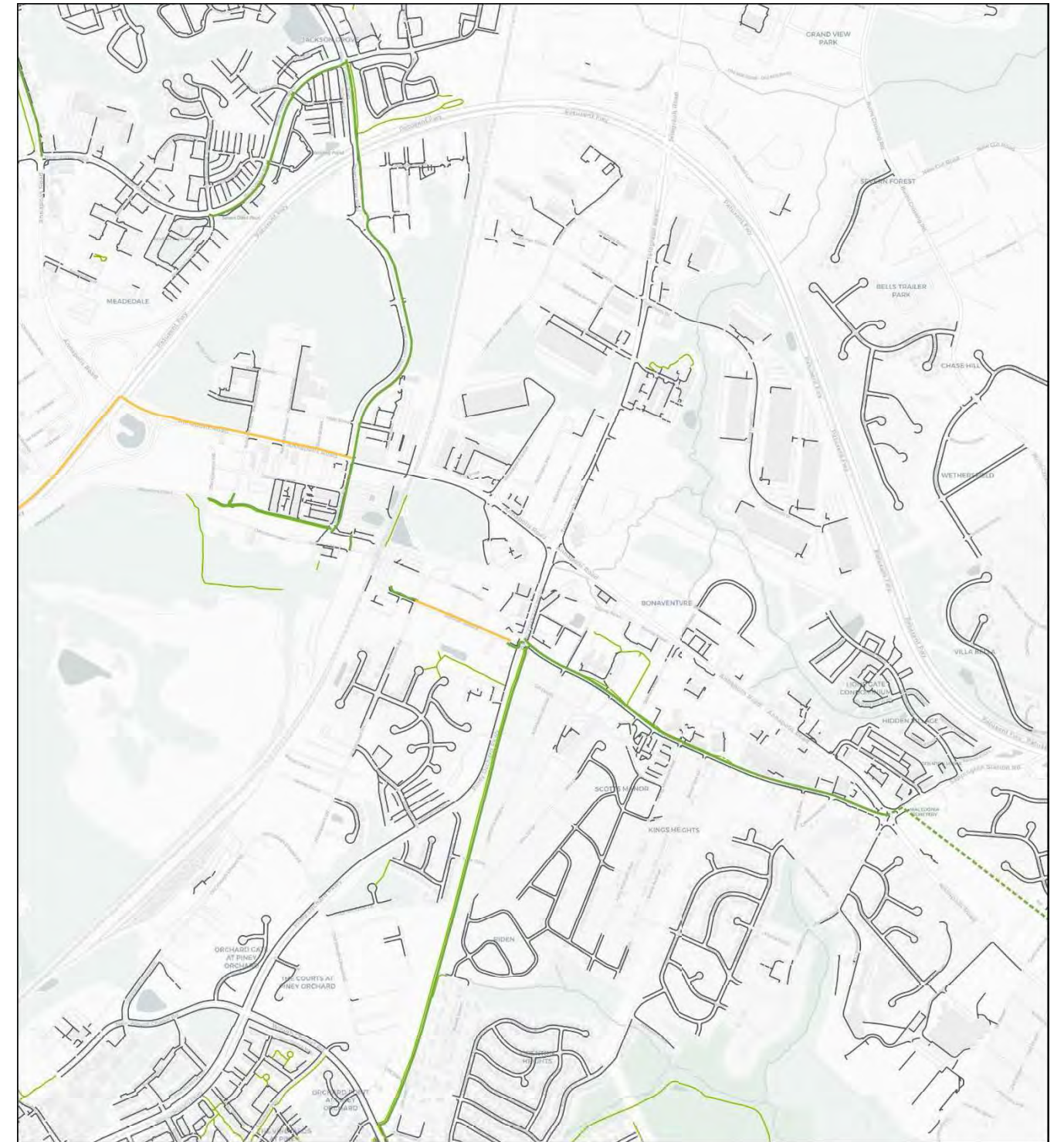
Glen Burnie Town Center



- | | |
|---------------------------|------------------------------|
| Bicycle Facilities | Pedestrian Facilities |
| — BL | — Sidewalk |
| — SBL | — Path |
| — Shared | |
| — Trails/SUP | |

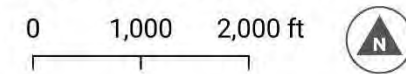


Figure 8 Glen Burnie Town Center



Anne Arundel County

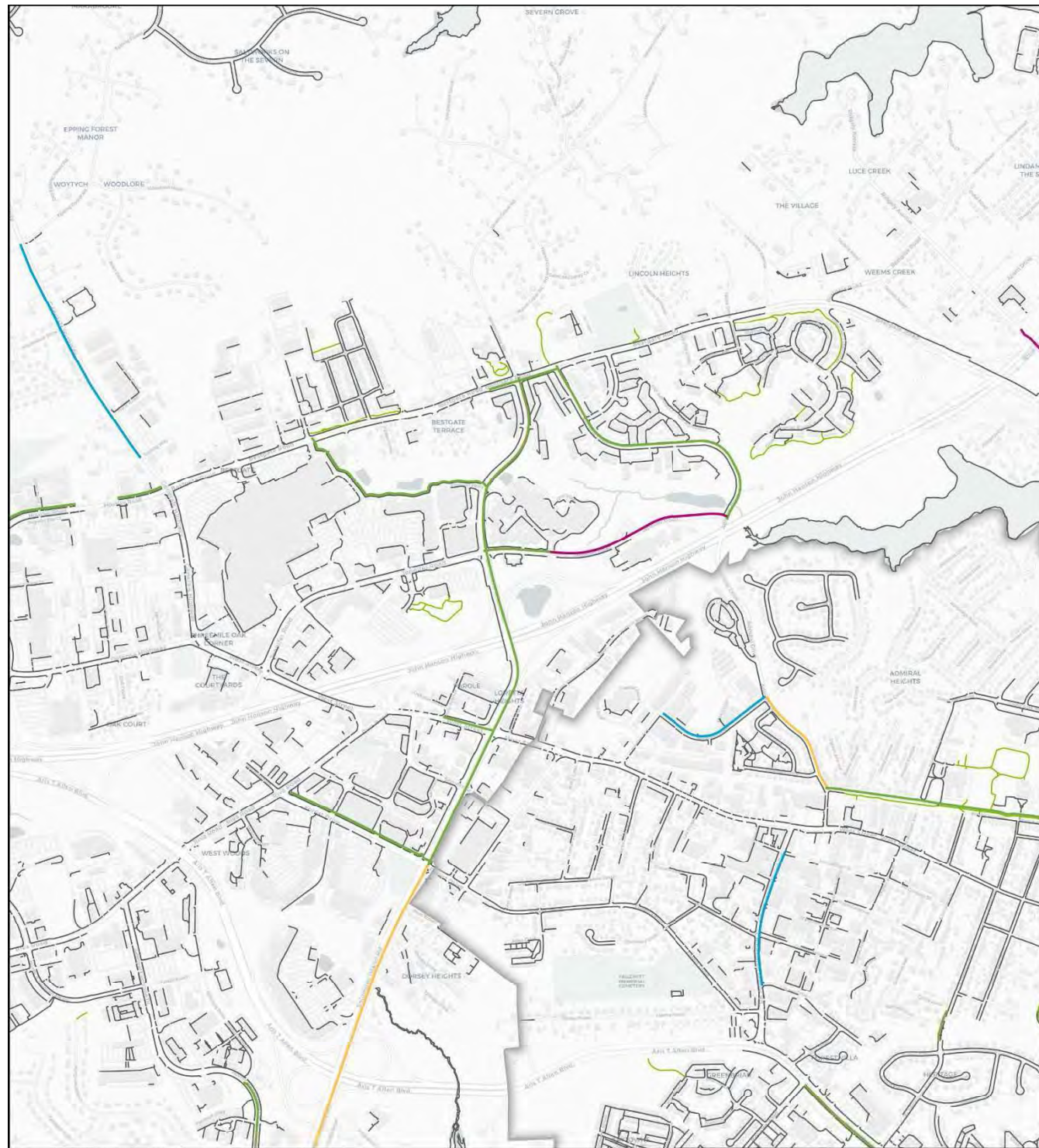
Odenton Town Center



- | | |
|---------------------------|------------------------------|
| Bicycle Facilities | Pedestrian Facilities |
| — BL | — Sidewalk |
| — SBL | — Path |
| — Shared | |
| — Trails/SUP | |



Figure 9 Odenton Town Center



Anne Arundel County

Parole Town Center

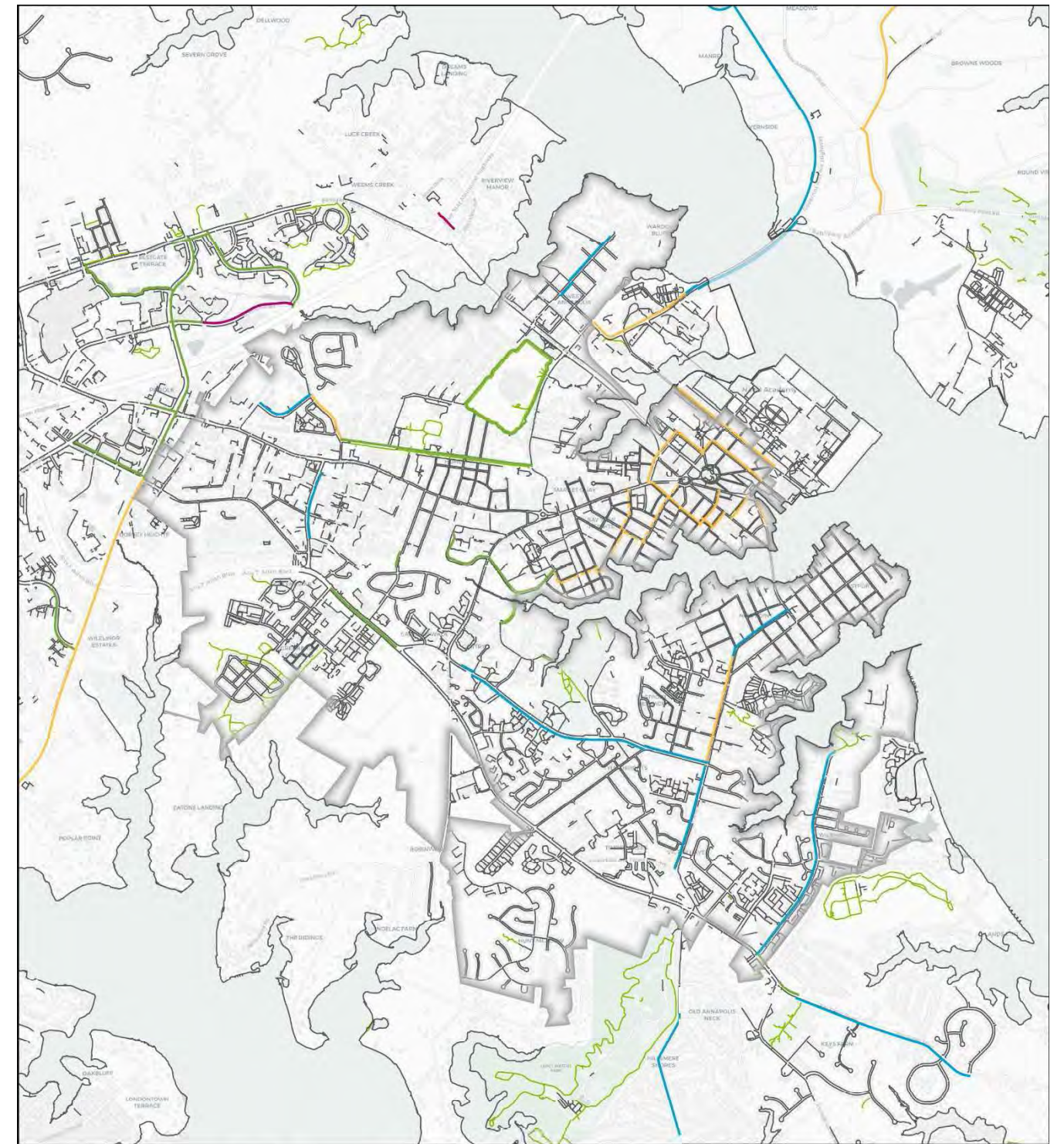
0 1,000 2,000 ft



- | | |
|---------------------------|------------------------------|
| Bicycle Facilities | Pedestrian Facilities |
| — BL | — Sidewalk |
| — SBL | — Path |
| — Shared | — City of Annapolis Boundary |
| — Trails/SUP | |



Figure 10 Parole Town Center



Anne Arundel County

City of Annapolis

0 0.25 0.5 0.75 1 mi



- | | |
|---------------------------|------------------------------|
| Bicycle Facilities | Pedestrian Facilities |
| — BL | — Sidewalk |
| — SBL | — Path |
| — Shared | — City of Annapolis Boundary |
| — Trails/SUP | |



Figure 11 City of Annapolis

Appendix D: Network Gap Analysis Memorandum

MEMORANDUM

June 23rd, 2022

To: Tanya Asman, Bicycle and Pedestrian Planner

Organization: Anne Arundel County Office of Transportation

From: Rochelle Carpenter

Project: Anne Arundel County Bicycle and Pedestrian Master Plan Update: *Walk & Roll Anne Arundel!*

Re: Walking and Rolling Network Gap Analysis Memorandum

Where are the places in Anne Arundel County where people are more likely to walk, bicycle or otherwise actively move for transportation? Where are people already walking or rolling, even without the safe infrastructure to do so? Where is this infrastructure lacking throughout the County? This memorandum presents answers to, or guidance to answering, these critical questions.

The goal of *Walk & Roll Anne Arundel!* is to connect residents and visitors using their own power to myriad communities throughout the County. Currently, many communities are “islands” with some internal low-stress streets, but they are bounded by high-stress roads, making each community feel insular and inaccessible to each other. On peninsulas, a high-stress road can be the only way in and out, meaning that these areas are effectively cut off from the walking and rolling network. The County needs connectivity between existing neighborhoods to improve multimodal safety and access.

In order to make these improvements, we first need to know what the barriers are to walking, bicycling, using a wheelchair, or using another micromobility device such as an electric scooter (the latter referred to generally as a “rolling device”) to get from place to place. Toole Design developed and used the following analyses to determine the primary barriers to multimodal travel:

- 1) Bicycle Network Analysis;
- 2) Level of Traffic Stress Analysis;¹
- 3) Conversations with Anne Arundel County staff responsible for ensuring access for people with disabilities; and
- 4) Feedback from residents through prior community engagement activities.

A connected pedestrian and bicycle network will increase accessibility for residents, giving them more freedom in how they move around. Several areas around the County are home to people who do not have a motor vehicle; an auto-oriented network effectively isolates them from access to employment opportunities or essential goods and services.

¹ The LTS analysis provides information about the location and proportion of roadways in the County that are high and low stress. It was not directly used to prioritize low-stress walking and bicycling facilities on the proposed bicycling/SUP network.

POLICY AND DESIGN BARRIERS

Decades of auto-oriented planning and design throughout the United States have left fewer choices and more dangerous conditions for people who travel on foot or rolling device. Over the past twenty years, industry best practices in bicycle and pedestrian planning have evolved. While Anne Arundel County and the State of Maryland have made progress updating policies to standardize the design and construction of multimodal transportation facilities, there remain deficiencies in transportation planning, policies, programs, and procedures. Improvements to County and State processes can ensure that high-quality walking and rolling infrastructure is seamlessly built throughout the County.

Toole Design reviewed current County planning documents, including:

- 1) *Move Anne Arundel!* the County's first Transportation Functional Master Plan²
- 2) Anne Arundel County's "Complete Streets Policy"³
- 3) Maryland State Highway Administration's "Complete Streets Policy"⁴
- 4) Anne Arundel County Code: "Article 13 - Public Works"⁵
- 5) Anne Arundel County Code: "Article 17 – Subdivision and Development"⁶
- 6) Anne Arundel County Code: "Article 18 – Zoning"⁷
- 7) "Anne Arundel County Engineering Design Manual"⁸

We identified a variety of policy and design barriers that hinder multimodal transportation construction, do not prioritize high-quality walking and rolling infrastructure, or present barriers to people with disabilities. We describe those barriers and their coinciding documents below.

Pedestrian Network

The pedestrian network in Anne Arundel County consists of sidewalks and other pedestrian paths, shared use paths/sidepaths, and pedestrian crossings. The need for this kind of infrastructure is rapidly increasing in suburban and exurban areas across Anne Arundel County as people with lower incomes, often without access to cars, move to more suburban areas searching for housing at a lower cost. The pedestrian network is critical to ensuring people can access a variety of activities including transit, work, school, parks, public facilities, and more. It is also an important part of people's daily exercise and recreation.

Sidewalks and Crossings

Sidewalks are a vital piece of infrastructure for the County's transportation system, connectivity, and quality of life. However, there are a variety of policy and design barriers in existing planning documents and legislation that prevent the construction of sidewalks and safe crossings outright, as well as pedestrian infrastructure with adequate design standards. These include:

- Minimum sidewalk width is only 5', with no incentives for the County or developers to build wider sidewalks that meet the industry standard. It is preferred for sidewalks to be 6', with an absolute minimum of 5'. A sidewalk directly adjacent to moving traffic should be 8' with a 2' buffer.⁹
 - The County passed an ordinance in 2021 making bicycle riding on sidewalks legal, making it even more important for sidewalks to be wider and allow for safe and comfortable access for all travelers.
- In addition to the lack of incentives, the County's current policy requires developers to provide bicycle, pedestrian, or transit design or infrastructure at a cost cap of a certain amount, respective to the development's quantity of units or square footage. If determined that such infrastructure cannot be constructed, the developer is allowed to pay a fee-in-lieu, amounting to that same cost cap. In addition, the County does not have a requirement that developers install sidewalks or bicycle infrastructure on roadways adjacent to their property.
- Pedestrian and bicycle projections, nor infrastructure, are not sufficiently studied through the Adequate Public Facilities or Traffic Impact Study review processes.
- On State Highways, the County has a standard template response to "defer ROW dedication and frontage improvements to SHA"; however, the State is unable to require improvements unless an access permit is required. The County plans for improvements on State roadways but often does not ask for those improvements to be made at the time of development. For a variety of reasons, bicycle and pedestrian improvements are frequently not constructed as part of the private site development process.
- Also relevant to the County's Bicycle Network, the "Complete Streets Policy" states that exceptions can be made that would prohibit providing favorable conditions for all modes of transportation. There are no specifications or prioritizations for different modes, making it easier to not construct industry-standard sidewalks, bicycle lanes, and safe crossings.

Bicycle Network

Anne Arundel County has been increasingly building its bicycle network, with the completion of about 30 miles of on-road infrastructure. This includes a combination of trails, sidepaths/shared use paths, separated bicycle lanes, conventional bicycle lanes, shared lanes, and signed bicycle routes.

Trails and Sidepaths

A large portion of the County's bicycle network consists of paved trails and sidepaths, comprising over half of all bicycle facilities. Trails are bicycle facilities that are physically separated from motor vehicle traffic by an open space or barrier and have their own right of way. Trails are managed by the Anne Arundel County Department of Recreation and Parks; they are typically – but inconsistently – maintained by the department. Additionally, other trails are managed by the City of Annapolis. Sidepaths are separated but within the roadway right of way; they are managed and inconsistently maintained by the Public Works Department. While County code requires adjacent property owners to maintain sidewalks, maintenance responsibilities of sidepaths are not the responsibility of

² <https://www.aacounty.org/departments/transportation/move-anne-arundel/#:~:text=The%20Transportation%20Functional%20Master%20Plan,local%20and%20state%20fiscal%20constraints.>

³ <https://www.aacounty.org/departments/transportation/bike-pedestrian/complete-streets/>

⁴ [SHA Complete Streets Policy \(maryland.gov\)](https://www.maryland.gov/sha/complete-streets-policy/)

⁵ https://codelibrary.amlegal.com/codes/annearundel/latest/annearundelco_md/0-0-0-111531#JD_Article13Title2

⁶ www.aacounty.org/departments/planning-and-zoning/forms-and-publications/Article_17.pdf#:~:text=Anne%20Arundel%20County%20MD%20Article%2017%3A%20Subdivision%20and,of%20suitable%20size%20to%20ensure%20protection.%28Bill%20No.%203-05%29

⁷ www.aacounty.org/our-county/county-code/forms-and-publications/Article18.pdf

⁸ <https://www.aacounty.org/departments/public-works/engineering/design-manual/>

⁹ National Association of City Transportation Officials. "Urban Street Design Guide." [Sidewalks | National Association of City Transportation Officials \(nacto.org\)](https://www.nacto.org/sidewalks/)

adjacent property owners. In addition, some small connectors are built by community associations. This split in ownership and responsibility leads to inconsistencies in how trails and sidepaths are designed, built, and maintained.

Furthermore, most of the County's regulatory and planning documents surrounding the pedestrian and bicycle network only account for on-road facilities. Trails do not benefit from the same caliber of guidance. In fact, trails are able to connect people where they need to go when located within bicycling distance between essential destinations. The Capital Crescent Trail in Montgomery County, MD, for example, is an integral part of the community's transportation system with over 1,000,000 people using the trail every year.¹⁰ Trails and sidepaths should be definitively coded in County legislation as transportation facilities.

Trails and sidepaths provide the most separation from the dangers of motor vehicle traffic. Additionally, they can be completed separate from roadway projects, which can allow for the gaps in the County's bicycle network to be filled in more quickly and at the industry's best standards.

On-Road Bicycle Facilities and Supportive Infrastructure

In addition to the County's "Complete Streets Policy," the State of Maryland has a "Complete Streets Policy"; however, neither provide specific, industry-standard requirements. While the County has a "Complete Streets Policy" and a County Council Resolution, it has not yet implemented Complete Streets in County code and design manuals.

The "Complete Streets Policy" has a minimum travel lane requirement for cars, but it does not have any sort of specification for bicycle travel lanes. This further emphasizes the auto-centric approach in the policy, as well as other planning documents for the area.

Connections to Transit

An important piece of providing a safe, connected public transportation system in Anne Arundel County is providing the infrastructure that creates safe first and last mile pedestrian and bicycle connections to transit. These connections are necessary to fill the gaps in areas that may not be served by transit. Looking at the Anne Arundel County Code and Zoning Ordinances, the county has a designation of "Transit Mixed Use" for areas surrounding transit and rail stations such as the Odenton MARC station. This designation allows for more high-density mixed-use development and additional pedestrian and bicycle paths, but it does not require it. Practitioners have come to recognize that the minimum standard is what will likely be implemented, especially if there is no incentive to develop facilities that would meet industry-best practices.

Anne Arundel County should consider the benefits of developing a more in-depth policy for bicycle and pedestrian facilities in Transit Mixed Use areas and Transit-Oriented Developments such as in Odenton, Glen Burnie, Linthicum, Jessup, and Brooklyn Park. Without strict, specific requirements the County will continue to leave transit-dependent people with unsafe connections to transit in the County.

¹⁰ Kraut, Aaron (4 September 2015). "New Numbers Show How Many People Are Using the Capital Crescent Trail". Retrieved 6 September 2017.

Access for People with Disabilities

In talking with County staff and researching existing County policies, Toole Design recommends the following to improve transportation access for people with disabilities:

- 1) Anne Arundel County should establish an ADA Self-Evaluation and Transition Plan. Through this process, the County would review the policies, programs, services and facilities for each department, including the Department of Public Works and the Office of Planning and Zoning, and identify changes needed to comply with ADA. Note that the Baltimore Metropolitan Council and the Maryland Department of Transportation have adopted Transition Plans. For guidance, refer to "ADA Transition Plans: A Guide to Best Management Practices" by the National Cooperative Highway Research Program.¹¹
- 2) Develop guidance for transportation planning and engineering to ensure that the transportation system meets the needs of people with disabilities. Maryland State law allows local governments to go beyond ADA standards and minimum State standards. The County should adopt transportation design standards to ensure that people with disabilities can functionally get from place to place. ADA should be treated as a minimum and not a standard for the County, given that people with disabilities still have difficulty navigating a transportation system that complies with ADA. Preliminary ideas for this guidance include:
 - a. Designing public transportation stations and stops that make it easy for people with disabilities to travel by following universal design guidelines described in:
 - i. "Transit Universal Design Guidelines" by the American Public Transportation Association¹²
 - ii. "Design Guidelines for Accessible Bus Stops" by the BC Transit Municipal Systems Program¹³
 - iii. "Universal Design & Accessible Transit Systems: Facts to Consider When Updating or Expanding Your Transit System." by Easter Seals.¹⁴
 - b. Of note, due to emerging challenges, designing floating bus stops to be accessible for pedestrians with vision and other disabilities.
 - c. Designing bicycle facilities that work for bicyclists with disabilities by following:
 - i. "Designing for All Ages & Abilities: Contextual Guidance for High-Comfort Bicycle Facilities" by the National Association of City Transportation Officials.¹⁵
 - d. Make design decisions with the needs of people with autism and other behavioral conditions by learning from emerging research by entities such as Rutgers University and the Governor's Council for Medical Research and the Treatment of Autism, which has begun to investigate policies and procedures that will help people with autism spectrum disorder (ASD).¹⁶
- 3) Increase staff capacity at the Department of Public Works and the Disability Commission to evaluate all engineering plans for roadway, pedestrian, and bicycle facilities to ensure comprehensive access for people with disabilities is designed and constructed.

¹¹ National Cooperative Highway Research Program. "ADA Transition Plans: A Guide to Best Management Practices" by the National Cooperative Highway Research Program." May 2009. [NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM \(dot.gov\)](https://www.nchrp.org/files/2009/ADA-Transition-Plans-A-Guide-to-Best-Management-Practices.pdf).

¹² American Public Transportation Association. "Transit Universal Design Guidelines: Principles and Best Practices for Implementing Universal Design in Transit." July 28, 2020. <https://www.apta.com/wp-content/uploads/APTA-SUDS-UD-GL-010-20.pdf>.

¹³ BC Transit Municipal Systems Program. "Design Guidelines for Accessible Bus Stops." https://nacto.org/docs/usdg/design_guidelines_accessible_bus_stops_bc_transit.pdf.

¹⁴ Easter Seals. "Universal Design & Accessible Transit Systems: Facts to Consider When Updating or Expanding Your Transit System." [NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM \(dot.gov\)](https://www.easterseals.com/usa/~/media/Files/2017/07/UD-GL-010-20.pdf).

¹⁵ National Association of City Transportation Officials. "Designing for All Ages & Abilities: Contextual Guidance for High-Comfort Bicycle Facilities." Dec. 2017. https://nacto.org/wp-content/uploads/2017/12/NACTO_Designing-for-All-Ages-Abilities.pdf.

¹⁶ <https://cait.rutgers.edu/autism-project/>.

- 4) Assess project designs for accessibility at multiple stages before construction, especially for nonconventional designs. For example, for transportation projects that affect pedestrians and involve a nonconventional design, an accessibility audit should be considered. Audits should be performed upon development of a 15% design and again upon development of a 65% design. For an example see the “Accessible Design Guide” by the City of Chicago Department of Transportation.¹⁷
- 5) Assess projects post construction. Build a project’s budget to allow for revisions to improve accessibility.
- 6) Systematize engagement of people with disabilities in the planning and design processes, such as by following:
 - a. “Engaging People with Disabilities in Street Planning and Design: 11 Tips for Getting it Right” by Toole Design Group¹⁸
 - b. “Designing Streets for PVD Toolkit” by Montgomery County.¹⁹
- 7) Provide regular trainings to County staff on accessible planning and design.
- 8) Develop a proactive sidewalk repair and maintenance program for sidewalks on County and State-owned roadways.

The findings from this investigation provide important groundwork for the policy recommendations that will be presented in the 2022 update of Anne Arundel’s Bicycle and Pedestrian Master Plan. Although there are challenges, the County has significant opportunities to create safe, connected, and accessible communities. In order to do so, the County needs to define strict, definitive standards for Complete Streets and bicycle and pedestrian facilities that are inclusive of all modes and abilities.

Recommended Changes to the County’s “Complete Streets Policy”

The overarching goal of *Walk and Roll Anne Arundel* is to provide the framework to ensure that Anne Arundel County provides a complete network of transportation options for all travelers, regardless of their mode, age, or ability.

The County should formally adopt legislation to codify Anne Arundel County’s “Complete Streets Policy” to promote better walking, bicycling, and rolling infrastructure in the County. In February 2014, Anne Arundel County Council passed “Resolution 45-14,” adopting a “Complete Streets Policy” for Anne Arundel County. This resolution included language promoting the implementation of Complete Streets Guidelines, as well as other guiding principles surrounding design regulations and standards.

While the adoption of the County’s “Complete Streets Policy” provided a foundation to integrate Complete Streets design principles into transportation projects, it stopped short of establishing exact procedures and requirements that would consistently yield projects with walking and bicycling infrastructure. The following recommendations should be incorporated in Anne Arundel County’s formalized Complete Streets Policy:

1. Add a section to establish the County’s commitment to Complete Streets design and construction in all phases of project development.

Consider adopting the following language found in the City of Cleveland Heights’s “Complete and Green Street Policy” of 2018:

All [County]-owned new construction, reconstruction/retrofit, resurfacing, repaving, restriping and rehabilitation of transportation projects in the public right-of-way including, but not limited to, streets and all other connecting pathways shall be designed, constructed, operated, and maintained so that all modes of transportation allow all users to move safely, comfortably, conveniently and independently.

Privately constructed streets and parking lots shall adhere to this policy.

The [County] shall foster project partnerships with [State and local agencies], neighboring communities and business and school districts to develop facilities and accommodations that further the [County’s] Complete Streets policy for travel between cities.

The [County] shall approach every street project and each project’s phase as an opportunity for transportation improvements for all users and as an opportunity for water quality improvements. These phases include, but are not limited to: planning, programming, design, right-of-way acquisition, construction, construction engineering, reconstruction, operation and maintenance. Other changes to transportation facilities on streets and rights-of-way, including capital improvements and major maintenance, must also be included.

The [County] shall provide accommodations for all modes of transportation to continue to use the road safely and efficiently during any construction or repair work that infringes on the right-of-way and/or sidewalk.

2. Add a section to clearly indicate jurisdictions in which the County’s Complete Streets policy applies. Consider adopting the following language found in the City of Cleveland policy:

All transportation infrastructure and street design and construction projects requiring funding or approval by [Anne Arundel County] shall adhere to this Policy. [County Commissioners] shall require the staff of Public Works, Long Range Planning, and Transportation to implement the Complete Streets improvements in a unified and coordinated manner through consistent and thorough communication, including Complete Streets topics at senior staff meetings and staff reports.

Projects funded by non-County governmental agencies such as state, federal, and city governments, shall adhere to this policy. To the greatest extent, coordination between County and non-County staff responsible for planning and engineering of Complete Streets shall receive special attention.

Projects funded by non-County agencies and organizations, such as the Maryland Transit Administration, shall adhere to this policy.

Private developments including street design and construction components shall adhere to – and fully support – this policy. The Planning Director or his or her designee with the developer shall agree upon applicable requirements early in the approval process. The County shall verify compliance of the development during inspections prior to the use of such improvements by the general public.

To the fullest extent possible, State of Maryland streets including the design, construction, reconstruction and maintenance of such streets, shall comply with this policy.

¹⁷ City of Chicago Department of Transportation. “Accessible Design Guide.” Aug. 2012. www.chicago.gov/content/dam/city/depts/cdot/Public%20Way%20Regulations/CDOT_AccessibleDesignGuide_Aug2012.pdf.

¹⁸ Toole Design Group. “Engaging People with Disabilities in Street Planning and Design: 11 Tips for Getting it Right.” https://tooledesign.com/wp-content/uploads/2020/07/Toole-Design-Resource-Guide-vol2_20200727_ADA2.pdf.

¹⁹ See page 11 of Montgomery County, MD’s “Designing Streets for PVD Toolkit” at www.montgomerycountymd.gov/DOT/Resources/Files/MC%20Designing%20Streets%20for%20PVD%20Toolkit_20211007_ADA.pdf.

For all streets and trails that connect to neighboring cities, Complete Streets principles shall be encouraged across borders so that accommodations are continuous to destinations in other streets.

To align with the *Walk & Bicycle* secondary map recommendations, specify that context-sensitive walking and bicycling facilities shall be designed and constructed on all arterial and collector roadways in the County.

3. List specific design guides that public agencies and private developers shall use to develop walking and bicycling facilities, including:

AASHTO Guide for Planning, Designing and Operating Pedestrian Facilities

AASHTO Guide for the Development of Bicycle Facilities

FHWA Bikeway Selection Guide

FHWA Designing for Pedestrian Safety

ITE Designing Walkable Urban Thoroughfares: A Context Sensitive Approach

ITE Policy on Geometric Design of Highways and Streets

NACTO Urban Bikeway Design Guide

NACTO Urban Street Design Guide

4. Specify that private developers shall design and construct context-sensitive walking and bicycling facilities in the frontage zone of their property.
5. Establish a modal hierarchy when planning, designing, and constructing roadway projects: (1) walking 2) cycling/public transit/micromobility 3) taxi/commercial transit/shared vehicles 4) single occupancy vehicles.

NETWORK GAP ANALYSIS TOOLS

Key questions guiding the network gap analysis: Does the low-stress network provide adequate coverage and access?

Bicyclists report discomfort and reluctance riding on streets with higher volumes of traffic, traveling at faster speeds. Bicycling can become a routine mode of transportation in Anne Arundel County if a low-stress network is developed that enables people to comfortably travel to the places they need to go. While Toole Design found that about 74% of the roads in the County are already low-stress, they are often surrounded by high speed and high volume roads, which connect to essential destinations. The County also has significant challenges with wide intersections and gaps in safe crosswalks, which contribute to inaccessible islands. Major roads like MD routes 2, 3, 177 and others are barriers to walking and rolling even short distances because they are treacherous to cross. Where feasible, reductions in traffic lanes and speeds can link these islands. Where these reductions are not possible, low-stress facilities such as sidepaths, bridges, separated bicycle lanes, and conventional bike lanes are needed.

Toole Design created a Level of Traffic Stress (LTS) map for Anne Arundel County in order to demonstrate the location and volume of streets where most bicyclists do not feel comfortable riding today. This map may also be used to identify streets where new bicycling infrastructure should be significantly separated from the roadway. The LTS map is displayed below with overlays of job centers, schools, transit stops, and more.

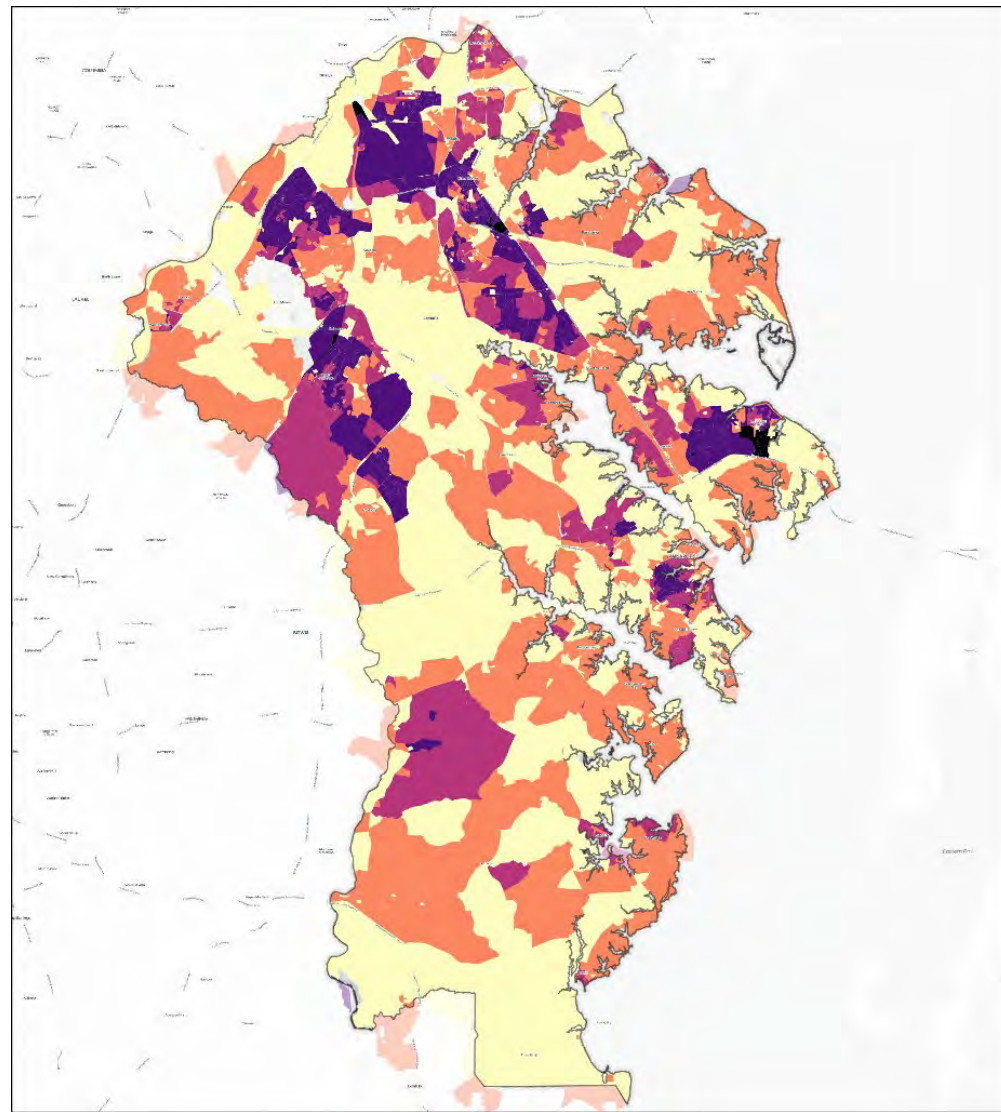
Does the low stress network go where people want to go?

To help assess how the bicycling and shared use path network connects people to destinations, Toole Design created a Bicycle Network Analysis (BNA) map. Network connectivity can be a difficult concept to describe, understand, and – crucially – to measure. Traditionally, jurisdictions attempting to quantify the usefulness of their bicycle networks have fallen back on easily-measured attributes like the mileage of bicycle lanes, or an as-the-crow-flies distance to the nearest bicycle facility.

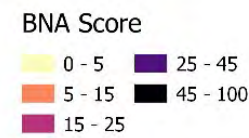
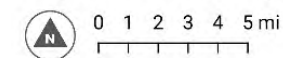
Although there may be some correlation between “bicycle friendliness” and these basic measures, they fail to capture the importance of having an *interconnected* network of comfortable bicycle routes, in addition to the role that *destinations* play in connecting people to places. The BNA aims to capture the importance of the interconnectedness of bicycle routes by measuring access to destinations.

Toole Design created Anne Arundel County’s BNA map by integrating the following spatial analyses:

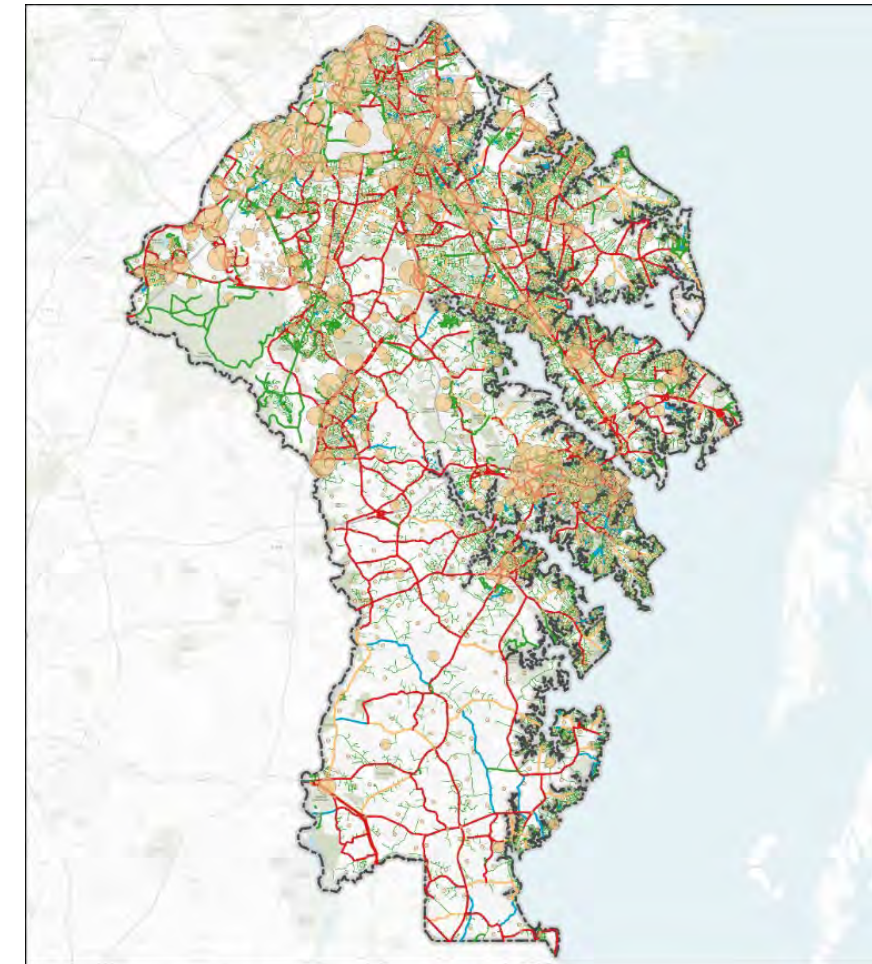
- 1) Level of Traffic Stress
- 2) Block-to-block connectivity analysis
- 3) Aggregation of essential destinations



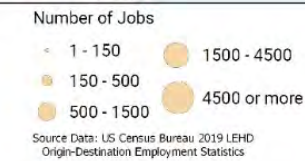
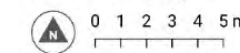
Anne Arundel County
Bicycle Network Analysis



Job and Shopping Centers



Anne Arundel County
LTS and Employment Distribution



This map identifies locations with shopping centers relative to roadways with varying levels of traffic stress, with:

Red = LTS 4 (highest level)

- Bicyclists mix with traffic moving at moderate speeds or are in close proximity to high-speed traffic
- Acceptable only to “strong and fearless” bicyclists

Orange = LTS 3

- Bicyclists must interact with traffic traveling at moderate speeds or in multiple lanes or are in close proximity to higher speed traffic
- Suitable for “enthused and confident” bicyclists

Blue = LTS 2

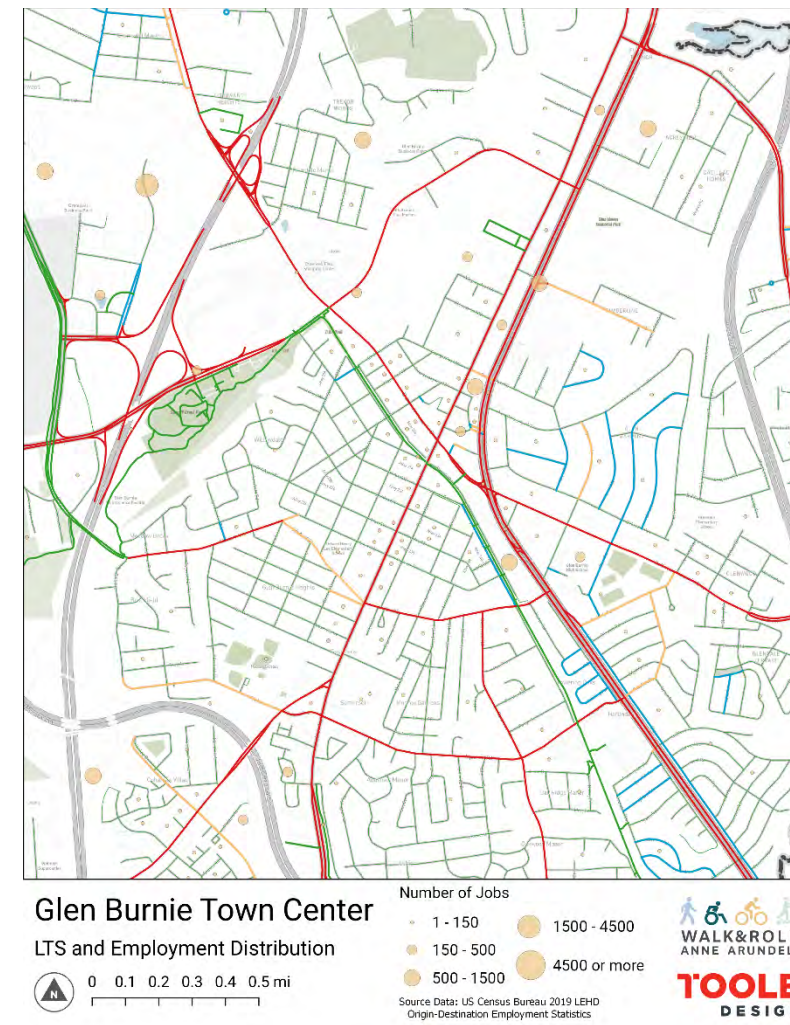
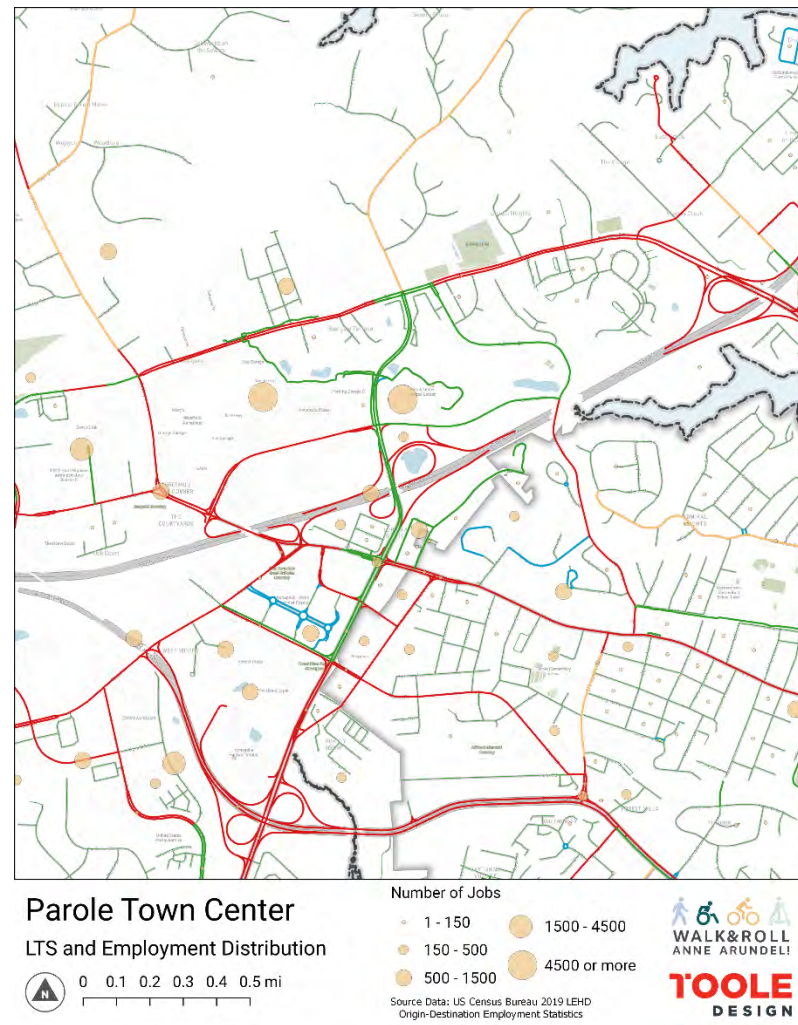
- Bicyclists have their own space to ride without the need to share lanes with traffic
- Physical separation exists on streets with higher speeds and multiple travel lanes
- Easy-to-use crossings
- Suitable for adult “interested but concerned” bicyclists

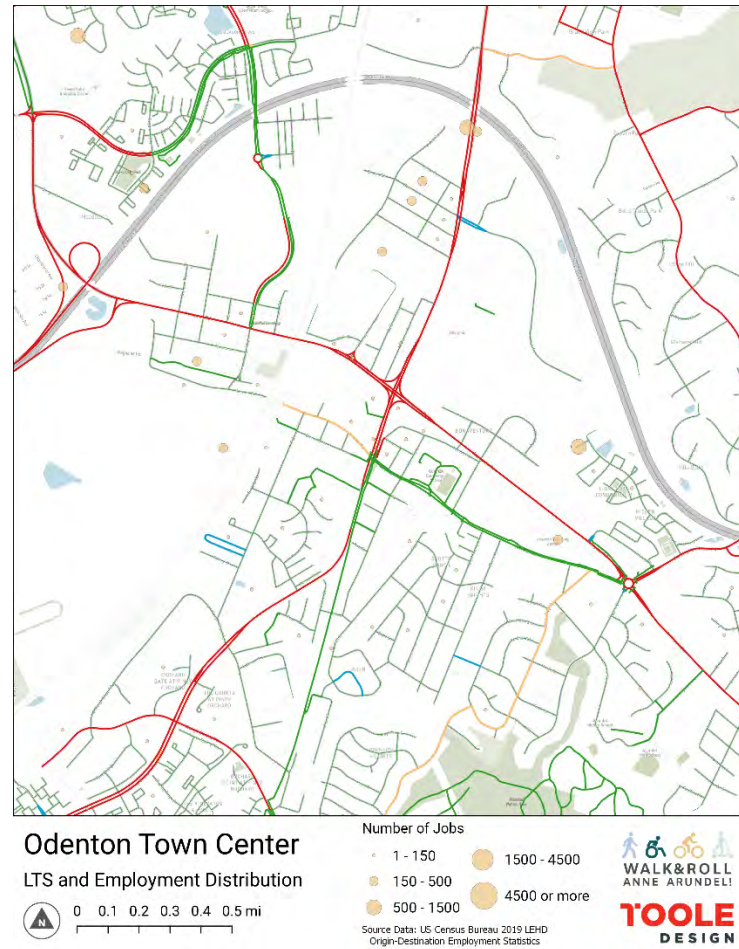
Green = LTS 1

- Significant separation exists between bicycle lanes and travel lanes
- Easy-to-use crossings
- Suitable for children²⁰

²⁰ <https://peterfurth.sites.northeastern.edu/2014/05/21/criteria-for-level-of-traffic-stress/>

We also provide this same spatial analysis for the Town Centers of Parole, Glen Burnie, and Odenton:





We observe that:

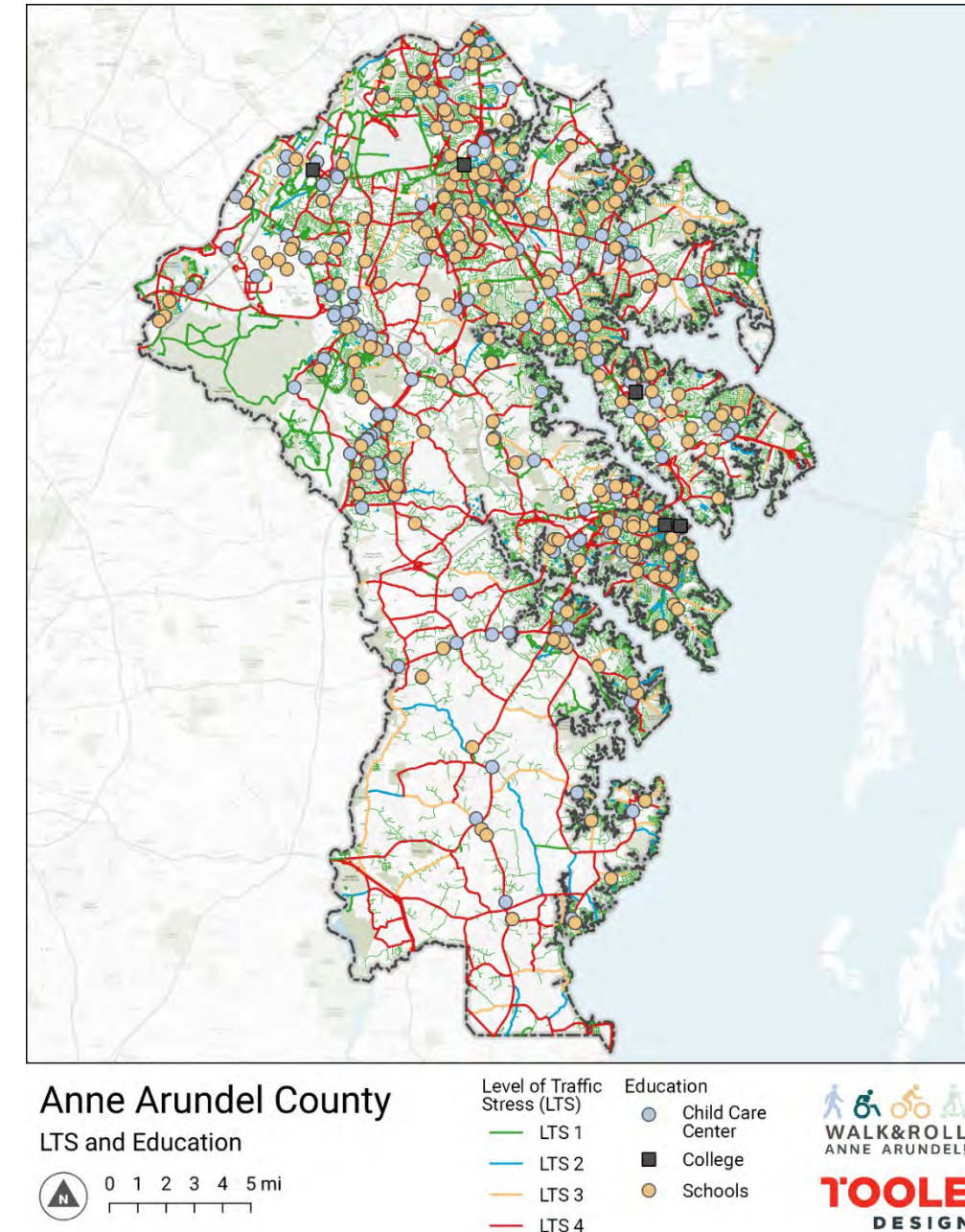
- The County's largest commercial centers grew around major roads; the result is they are hard to reach by walking, bicycling or wheelchair.
- The County's largest concentrations of jobs are in the Parole, Linthicum Heights, and Arundel Mills areas; all three are difficult to reach by multimodal transportation.
- Major retail destinations – Parole, Glen Burnie, Waugh Chapel, Arundel Mills – are also difficult to reach; nearly all shopping centers are on a high-stress road.
- That said, there is a nascent network of high-quality facilities – such as sidepaths around Arundel Mills Mall and the BWI Trail – that can form the spine of an expanded network connecting these places. For example, connecting Parole to the B&A Trail opens access to a large swath of the County. Connecting the BWI Business District to the BWI Trail would also significantly increase access.

Public Transportation

Many of the County's transit stops, from MARC stations, light rail stations, and bus stops, are on roadways with a high level of traffic stress.

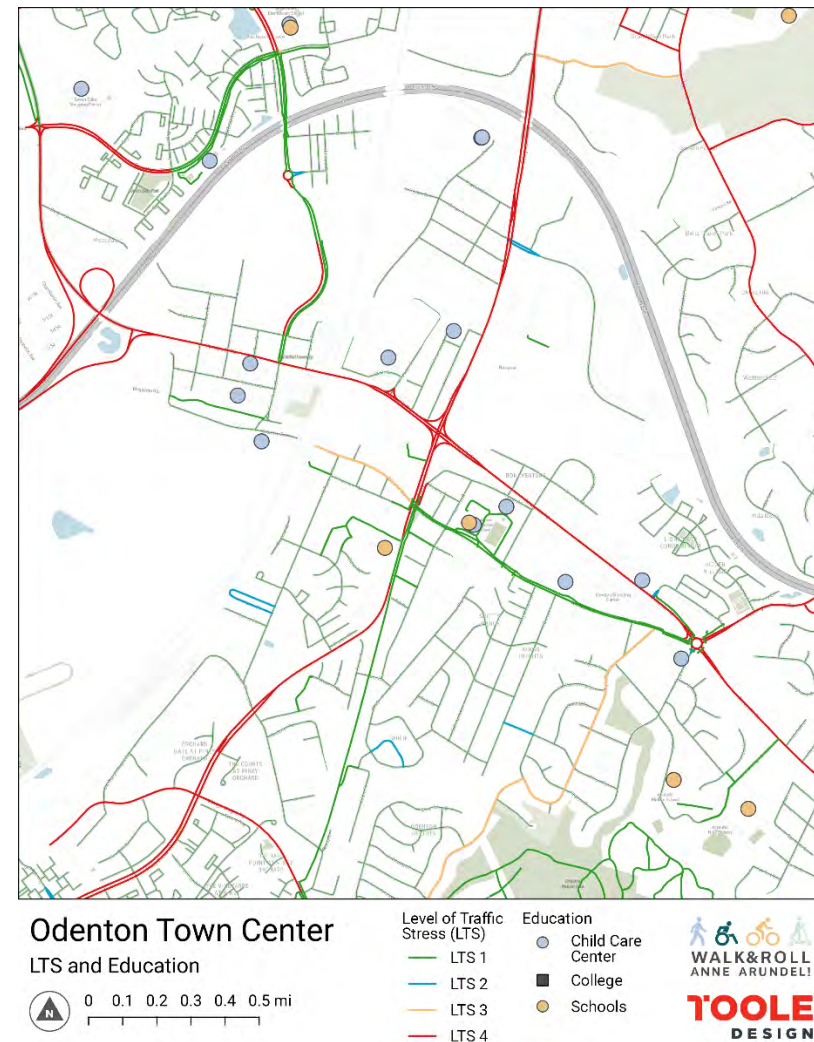
It is difficult to reach public transportation stops in the County's 3 Town Centers of Odenton, Glen Burnie, and Parole.

Schools and Child Care Centers



We observe that many schools are located on residential streets that are generally comfortable; however, some are on busy streets, or busy streets that form barriers in the walkshed.

This is true, for example, in Odenton:

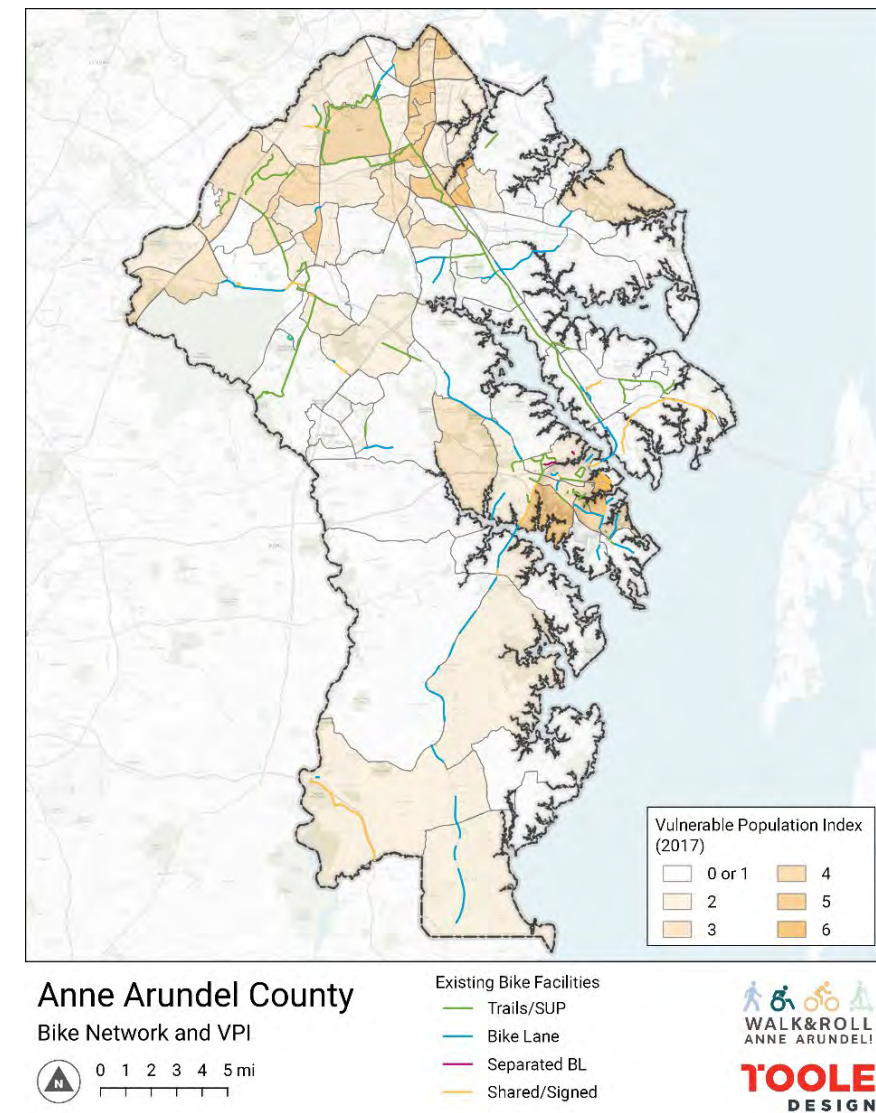


Does the network serve those who need low-cost transportation options the most?

This plan centers vulnerable populations by recommending walking and rolling projects that provide more direct and closer connections between neighborhoods with higher numbers of underserved people and essential destinations, particularly job centers, schools, and transit stops. According to a Harvard University study, commute time is the leading indicator of a family's ability to climb the economic ladder.²¹ If people living in poverty could easily walk or roll to job centers, schools, child care centers, grocery stores, and public transportation stops, in 30 minutes or less, they would be more likely to escape poverty.

²¹ Chetty, Raj and Nathaniel Hendren. "Impacts of Neighborhoods on Intergenerational Mobility I: Childhood Exposure Effects." *The Quarterly Journal of Economics*, Vol. 133(3). Aug. 2018, pp. 1107 – 1162. <https://doi.org/10.1093/qje/qjy007>.

Existing Bicycle Network – Access and Gaps to Vulnerable Residents



Multimodal transportation projects should be prioritized in neighborhoods with concentrations of traditionally underserved people. The Vulnerable Population Index (VPI) index includes higher averages of census tracts where people do not own a vehicle and where people are in poverty, making it disproportionately challenging for people to travel to jobs, schools, services, social networks, and recreation. When looking more closely at the data in Anne Arundel County:

- VPI areas are close to job centers in some areas. Most of the jobs in the County are in North County and Annapolis. Large concentrations of VPI populations reside along the Route 2 corridor between Brooklyn Park and Marley Station, west of the City of Annapolis, and on the northwest side of the County between Maryland City and Hanover. Safe and direct walking and rolling connections should be constructed linking these neighborhoods with job centers.
- VPI areas in South County are not near job centers. Some of these residents are near destinations that help them meet their daily needs (schools, shopping, etc.). Providing direct and safe connections to these

centers are important. Providing public transportation services within walking or bicycling distance to their homes is also important to improve their ability to travel to work.

- While there are barriers to transportation all over the County, they are especially a problem in areas with high concentrations of vulnerable populations, cutting them off from opportunity in the rest of the County. Prioritizing safe multimodal transportation for traditionally underserved people would help ensure people can get to work, school, and other essential destinations, supporting both individuals and the broader economy.

TAKEAWAYS AND GOALS FOR NETWORK DEVELOPMENT

Takeaways

In reviewing the results of these mapping analyses, we observe that:

- The County has a hierarchical road network with limited direct bicycling connections. For example, a shared use path only exists on a segment of College Parkway in Arnold, isolating people from being able to safely walk or roll to Anne Arundel Community College from neighborhoods on the Sandy Point peninsula.
- Major destinations – such as job centers to residential areas – are at distances not conducive to comfortable walking or rolling. For example, a major concentration of jobs at the Westfield Annapolis Mall and the Annapolis Towne Center in Parole is approximately 2-3 miles from neighborhoods in western Parole off of Riva Road. This is outside of practical walking distance. They are within bicycling distance but lack a direct bicycling connection.
- Water and large roads form barriers to walking and rolling. For example, the Baltimore and Annapolis Trail is disconnected from communities on the eastern side of Route 2 in Glen Burnie, Pasadena, Severna Park, and Arnold because of the lack of safe crossings on Route 2.
- High-quality, popular trails, like the BWI Trail, the B&A Trail, and the WB&A Trail, lack connections to each other, residential areas, and essential destinations.
- The County's Town Centers lack a complete sidewalk network, especially in Parole.

Goals for Walk & Roll Anne Arundel!

Goals for the Pedestrian Network:

- Focus on improving high-speed, high-volume roads in urban areas where people are more likely to walk for transportation purposes.
- Connect to transit, jobs, and daily needs; public feedback clearly indicates a desire to meet this goal.
- Reduce pedestrian crashes by installing infrastructure proven to improve safety, including:
 - High-visibility midblock crosswalks with pedestrian hybrid beacons or rectangular rapid flashing beacons
 - Pedestrian refuge islands
 - Leading pedestrian intervals
 - Pedestrian signal timing that give people with disabilities enough time to cross the street.
- Implementation of road diets and other methods of utilizing road space for the pedestrian network.

- Complete the sidewalk network to meet industry standards established in *Walk & Roll Anne Arundel!* in the Town Centers, including 8-10' sidewalks with landscaped buffers between sidewalks and travel lanes.
- Promote safe routes to school, especially with safe walking paths ¼ - ½ mile from schools, and bicycling paths ½ - 3 miles from schools.

Goals for the Bicycle Network:

- Provide low-stress bicycling infrastructure, including sidepaths and protected bicycle lanes, and associated crossings, to allow people to comfortably travel to jobs, transit, shopping centers, schools, and other essential destinations. This direction was prominent in public feedback.
- Provide context-sensitive facilities for urban, suburban, and rural roads.
- Connect and improve existing bicycling infrastructure.
- Prioritize connections to existing trails.

In the next phase of work, we will show how these goals provide the framework for our recommended walking and rolling network.

Sincerely,

Rochelle Carpenter | Senior Planner

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Appendix E: Network Recommendations by Fifteen Priority Improvement Areas

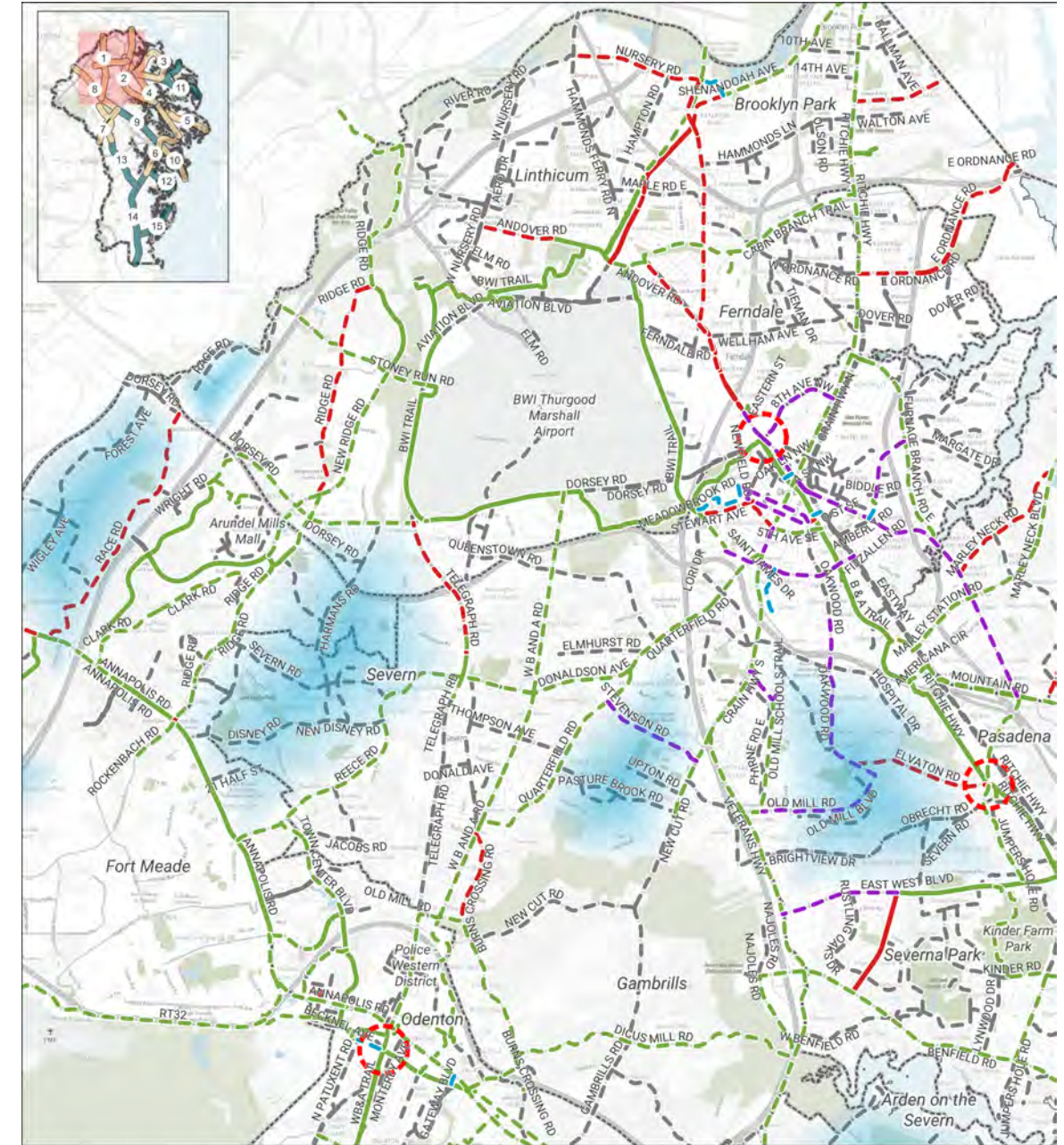
Northwest County

Description and Destinations:

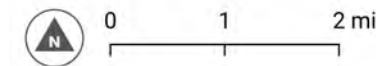
The Northwest County PIA is one of the newer sections of the county, with large developments from the 1990s and later and with many sidepaths. Additionally, there are many office parks, logistics warehouses, and major shopping centers. Some of these major destinations and developments include Seven Oaks, Arundel Preserve, Piney Orchard, BWI Business District, and Arundel Mills Mall.

There are more than a dozen elementary schools located in this proposed improvement area. This area also houses a high concentration of the County's vulnerable population.

Within this PIA, there is one large gap between the BWI Trail and the network of facilities around Odenton. The County is currently pursuing a new facility on Burns Crossing Road and WB&A Road between these two areas. There is narrow ROW, no curb and gutter, and relatively low speeds.



1 - Northwest County Proposed Bike Network



- Facility Types**
- Proposed**
 - Shared-use Path
 - Separated Bike Lane
 - Bike Lane
 - Low Stress Connection
 - Secondary Network
 - Existing**
 - Shared-use Path
 - Bike Lane
 - Areas for Future Study
 - Key Trail Intersections



The focus for this area is connecting the existing shared-use paths throughout the different developments and major destinations. The largest issue is pedestrian crossings; there are many large suburban roads with high speeds that lack appropriate pedestrian crossings. Other issues for the area include tying the BWI Trail into surrounding areas and identifying the small connections between shopping centers and residential areas that already have sufficient paths.



A pedestrian walks on Reece Road in Severn

Proposed Improvements:

The following are recommended improvements in the Northern Section of the Northwest County PIA:

- Shared-use paths on the following roads in Hanover:
 - » Ridge Rd between the Stoney Run Creek Trail and Corporate Center Dr
 - » New Ridge Rd between Stoney Run Rd Corporate Center Dr and Arundel Mills Blvd
 - » Ridge Rd from Arundel Mills Blvd to Fort Meade
 - » Hanover Rd between Race Rd and the BWI Trail in Hanover
 - » From Dorsey Rd to Ridge Rd over MD 100, connecting the BWI Trail to the Arundel Mills Shopping Center, following Alternative 3 in the County’s BWI Trails Connector Study: BWI to Arundel Mills from 2017
 - » From Dorsey Rd to Wright Rd over MD 100, connecting the BWI Trail to the Arundel Mills Shopping Center, per Alternative 4 in the BWI Trails Connector Study: BWI to Arundel Mills from 2018
 - » Wright Rd from Dorsey Rd to Dorchester Blvd
 - » Clark Rd from between Milestone Pkwy to Watts Ave.
- Shared-use paths:
 - » Along Stoney Run Creek in Patapsco Valley State Park

- Safer crossings for pedestrians and bicyclists at multiple high-risk intersections.

The following are recommended improvements in the Southern Section of the Northwest County PIA:

- A bicycle lane on Ridge Road from Ridge Rd and New Ridge Rd to Ridge Rd and Corporate Center Dr, following existing County plans
- Shared-use paths on the following roads:
 - » WB&A Rd from the BWI Trail to the end of the road, where a new trail connection would begin to Odenton Rd; extension of the WB&A Trail beyond the Odenton Road trailhead
 - » Safer pedestrian and bicyclist crossings along the WB&A trail at Waugh Chapel Road, Odenton Elementary School, and other high-risk intersections
 - » Quarterfield Rd from WB&A Rd to Donaldson Ave
 - » Donaldson Ave from Quarterfield Rd to Telegraph Rd
 - » Reece Rd from Telegraph Rd to Annapolis Rd
 - » Najoles Rd and Dicus Mill Rd from Benfield Blvd & Najoles Rd to Dicus Mill Rd & Burns Crossing Rd Rd
 - » Jacobs Rd W from Reece Rd to Town Center Blvd
 - » Town Center Blvd from Jacobs Rd W to Blue Water Blvd
 - » Telegraph Rd from Old Mill Rd to Odenton Rd
 - » On Old Mill Road, Burns Crossing Road, and Sappington Station Rd from Old Mill Rd & WB&A Rd to Burns Crossing Rd & Old Mill Rd to Sappington Station Rd & Annapolis Rd
 - » Along the Patuxent Freeway to Annapolis Rd.
- Shared-use paths through Fort Meade that connect residential areas north of Fort Meade to the Patuxent Research Refuge North Tract
- Bicycle lanes on Burns Crossing Rd from WB&A Rd into downtown Odenton
- Shared-use paths through Fort Meade that connect residential areas north of Fort Meade to the Patuxent Research Refuge North Tract
- Additional recommendations in the Odenton Town Center are described in the Town Center section below.

Previous Study Recommendations:

Ridge Road Study:

A 2017 report, entitled “Future Conditions Report, Transportation Facilities Planning: Ridge Road” evaluated alternatives to reduce congestion, improve safety, and improve walking and bicycling conditions on:

- New Ridge Rd (MD 713) between Ridge Road and Corporate Center Drive (MD 758)
- Ridge Road (MD 713) between Dorsey Road (MD 176) to the Access Control Point (ACP) of Fort George G. Meade (FGGM) at Rockenbach Road (MD 713).

The resulting preferred design concept includes:

1. Maintaining a two-lane roadway
2. Constructing a new continuous sidewalk on both the east and west sides of Ridge Road from Dorsey Road to Corporate Drive
3. Adding continuous on-road bicycle lanes along the east and west sides of Ridge Road between New Ridge Road and Corporate Center Drive
4. Adding turn lanes at the intersection with Hanover Road, adding two-way stop control at the intersection with Stoney Run Road, and lengthening a southbound turn lane at the intersection with New Ridge Road.

The estimated construction cost is \$8 million.

In order to provide a low-stress facility for both pedestrians and bicyclists, this plan recommends a separated shared-use path on Ridge Road between Dorsey Road and Corporate Center Drive.

Stewart Avenue & Old Stage Road Intersection Study:

A 2016 study evaluated intersection safety improvement options at Stewart Avenue and Old Stage Road. The existing intersection includes a crosswalk for travelers of the BWI Trail/John Overstreet Connector pedestrian and bicycle path. Researchers also noted challenging turns for vehicles and excessive speeds, despite the installation of a speed bump around 450 feet east of the intersection. The study recommends either a roundabout or the removal of the eastbound right-turn lane coupled with traffic calming measures. The latter would improve safety for those traveling on foot because of the reduced crossing distance for pedestrians and because it could contribute to slower motor vehicle speeds. The study suggests installing a pedestrian refuge island in the middle of the crosswalk to further improve safety.

Telegraph Rd (MD 170) and Reece Road (MD 174) Concept Development Study:

A 2016 study evaluated highway capacity modifications at the intersection of Telegraph Road and Reece Road, and extending along Telegraph Road for a total of .82 miles. Modifications include an additional southbound through lane along Telegraph Road, a raised median along Telegraph Rd to prevent left turns, an extension of dual northbound through lanes along Telegraph Road, and the addition of a 6’ bicycle lane along Telegraph Road going in the northbound direction and 5’ sidewalks with a 3’ buffer along both sides of the roadway.

MD 168 (Nursery Road) Sidewalk Feasibility Study:

A 2013 “purpose and need” study evaluated the need for modifications to West Nursey Road, a minor arterial from Hammonds Ferry Road to Elkrige Landing Road. The report determined that pedestrians and bicyclist infrastructure should be built on the corridor.

A 2014 study determined the feasibility and cost of a sidewalk and bicycle lane on Nursey Road from Hammonds Ferry Road to MD 648E (Baltimore Annapolis Boulevard). The estimated cost was \$6.1 million, excluding ROW cost.

BWI to Arundel Mills Trails Connector Study:

A study was conducted to identify the preferred route between the BWI Trail, Arundel Mills, and nearby destinations. The study determined that the preferred route would be building a shared-use path on Stoney Run Road, New Ridge Road, and Arundel Mills Boulevard, which would connect the BWI Trail to Arundel Mills.

The report describes the route as follows: “Connect to existing BWI Trail at northern edge of Stoney Run Road Bridge and continue trail to intersection of Stoney Run Road at New Ridge Road. Trail continues along the west side of New Ridge Road to intersection with Ridge Road. Trail then crosses to New Ridge Road to continue along east side as New Ridge Road becomes Arundel Mills Blvd. Trail follows Arundel Mills Boulevard along east side under MD 100, down to Arundel Way where it crosses Arundel Mills Boulevard. There is also a proposed spur down Ridge Road to Dorsey Road.”

The estimated construction costs, including ROW acquisition, is \$18,731,000.

BWI to Odenton Trail

This includes a study of a SUP on both Telegraph Road between the BWI Trail and Odenton Road in Odenton and WB&A Road between the BWI Trail (with connections to the BWI Trail via Old Telegraph Rd and Queenstown Rd) and Odenton Road. The preferred alignment is: the preferred alignment is a new buffered side path along the east side of WB&A Road, with the northern end terminating at the intersection of WB&A Road at MD 176 (Dorsey Rd) and the southern terminus consisting of a new side path from WB&A Road to the North Side of Old Mill Road, then traversing the east side of Telegraph Road and then south to MD 32, where the side path transitions to the west side of Telegraph – ending at the Trailhead for the WB&A Trail at Odenton Road. The estimated construction cost for this preferred alignment is about \$6.8 million.

Corridor Growth Management Plan:

On MD 176, the CGM Plan includes a recommendation to “improve bicycle and pedestrian facilities such as sidewalks and bicycle lanes/signed routes, implement access management, implement new site design guidelines/overlay district to provide a more walkable streetscape/building frontage; implement shared parking requirements.”

If recommendations from the CGM Plan are implemented to begin all-day weekday high quality transit service along I-97, low-stress walking and bicycling infrastructure should be constructed to stops at BWI Airport and Arundel Mills Mall (as well as Parole Town Center, Benfield Blvd, Glen Burnie Town Center, and Glen Burnie Light Rail Station, covered in other PIAs).

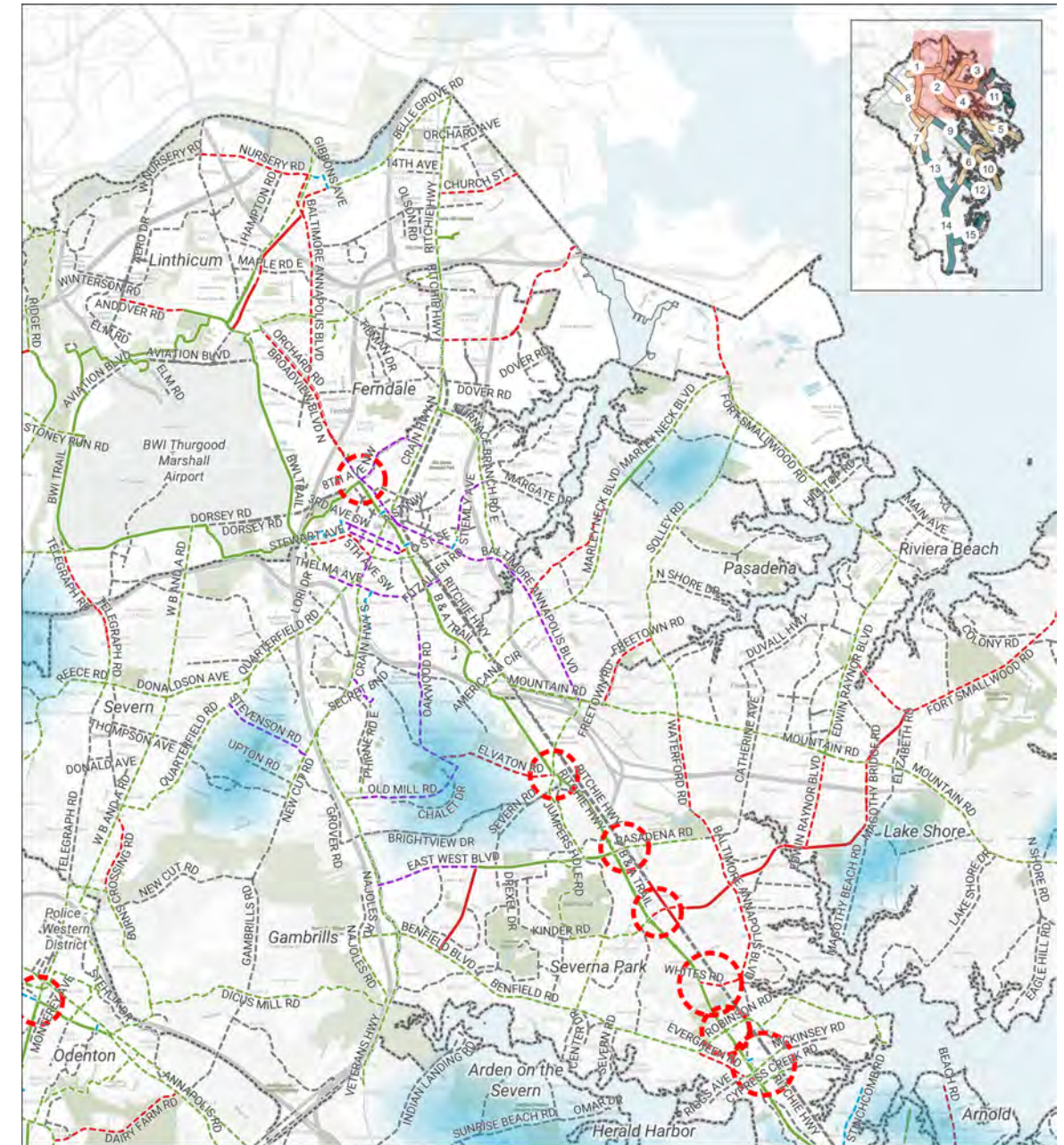
North County

Description and Destinations:

This Priority Improvement Area in northern Anne Arundel County connects the Brooklyn Park area (and neighboring Baltimore City) to Glen Burnie Town Center roughly following Route 2 (Ritchie Highway); and Glen Burnie to Severna Park following Route 3 (Crain Highway), Veterans Highway, and Benfield Road to reach neighborhoods and commercial areas that are farther away from the B&A

Trail. Brooklyn Park and Glen Burnie are some of the most diverse and vulnerable areas in Anne Arundel County according to BMC’s Vulnerable Population Index. The area has a high concentration of jobs, public facilities, and people within the county.

The area currently has two of the county’s signature trails within it – the BWI Trail and B&A Trail. This plan utilizes them as regional spines for the bicycle and pedestrian network and proposes facilities that will extend the reach



2 - North County Proposed Bike Network

Facility Types

- Proposed**
 - Shared-use Path
 - Separated Bike Lane
 - Bike Lane
 - Low Stress Connection
 - Secondary Network
- Existing**
 - Shared-use Path
 - Bike Lane
- Areas for Future Study
- Key Trail Intersections



of the network into neighborhoods and commercial areas that are further away from the trails. This will connect the three communities closer together and allow people of all ages, backgrounds, and abilities the opportunity to travel to nearby destinations via walking and biking, thereby improving access to employment opportunities as well everyday needs such as shopping, school, and recreation. There are 14 elementary schools, four middle schools (Brooklyn Park, Cockran, Old Mill, Severna Park), and four high schools (Glen Burnie, North County, Old Mill, Severna Park) in the area.

Proposed Improvements:

Proposed improvements to connect Brooklyn Park to Glen Burnie include:

- Shared-use paths on these roads in the northern part of this PIA:
 - » Baybrook Connector, which is currently being studied: Bidirectional shared-use path on Belle Grove Rd to connect Brooklyn (Baltimore) and Brooklyn Park to the Nursery Road light rail station and the BWI Trail extension to the light rail
 - » Bidirectional shared-use path on Ritchie Hwy between the proposed SUP along the Cabin Branch and the northern County line
 - » Bidirectional shared-use path on Route 2 and 3 (Crain Hwy) to connect Brooklyn Park and Glen Burnie to shopping centers and the proposed SUP along the Cabin Branch
 - » E Ordnance Rd from Ritchie Hwy to the eastern County line
 - » New multi-use trail following Cabin Branch, from BWI Trail to Route 2 (Ritchie Hwy) south of I-695, to connect to North County High School and Hilltop Elementary.
- Bicycle lanes on:
 - » Nursery Rd and Baltimore Annapolis Blvd for more direct connections from Linthicum Heights, Pumphrey, and Ferndale to Glen Burnie
 - » Andover Rd between W Nursery Rd and the BWI Trail
 - » Church St from Crain Hwy to eastern County line
 - » North Broadview Blvd from Baltimore Annapolis Blvd to the proposed SUP along the Cabin Branch.

Proposed improvements to connect Glen Burnie to Severna Park include:

- Shared-use paths on:
 - » Route 3 (Crain Hwy/New Cut Rd), as a continuation of the recommended SUP on the northern segment of Crain Hwy, over MD-100 and I-97 to connect to North Arundel Aquatic Center, Glen Burnie Park Elementary, East Park Plaza Shopping Center, Veterans Highway, and ending at Archbishop Spalding High School
 - » Quarterfield Rd from Donaldson Ave to Crain Hwy S
 - » Quarterfield Rd from WB&A Rd to Donaldson Ave
 - » Marley Station Rd from Baltimore Annapolis Blvd to B&A Trail
 - » Veterans Highway from Route 3 to MD-178 for access to commercial areas
 - » Jumpers Hole Rd from Route 2 to Benfield Rd, with a connection through Kinder Farm Park for ease of design and construction (without the need to acquire ROW or move utilities on the adjacent section of Jumpers Hole Rd)
 - » Earleigh Heights Connector from Jumpers Hole Rd to WB&A Trail
 - » Benfield Blvd from Veterans Hwy to the B&A Trail (where Benfield Blvd has already transitioned to Robinson Road) and beyond, serving Benfield Elementary and Severna Park Middle and High Schools.
 - » Benfield Rd from Veterans Hwy to Najoles Rd
 - » New Old Mill Schools Trail along the west side of Rippling Woods Elementary and Old Mill Middle/High School grounds
 - » Connect to broader bike network using Hidden Brook Dr at northern end and Old Mill Rd at the southern end.
 - Bidirectional cycle tracks on the following roadways:
 - » Old Mill Rd/Blvd from Veterans Highway to Oakwood Road
 - » Oakwood Rd from Old Mill Blvd to Oakwood Station Road
 - » East West Blvd from Veterans Hwy in Millersville to Route 2/Governor Ritchie Hwy in Pasadena.

- Separated bicycle lane on Stevenson Rd from Quarterfield Rd to New Cut Rd
- Bicycle lane on:
 - » Burns Crossing Rd from Sappington Station Rd to WB&A Rd, with a separated bicycle lane connection on Sappington Station Rd between Burns Crossing Rd and the South Shore Trail
 - » Elvaton Rd from Oakwood Rd to Jumpers Hole Rd
 - » Waterford Rd from Mountain Rd to Baltimore Annapolis Blvd.

Previous study recommendations:

Corridor Growth Management Plan:

If recommendations from the CGM Plan are implemented to begin all-day weekday high quality transit service along MD-100 between I-95 and I-97, low-stress walking and bicycling infrastructure should be constructed to stops in Marley Station, BW Medical Center, MID 170 (potential future MARC Station), Arundel Mills, Dorsey MARC Station, Snowden River Park & Ride, and Long Gate Park & Ride/Ellicott City.

If recommendations from the CGM Plan are implemented to begin local transit service along parallel corridors of MD-295/Baltimore-Washington Parkway, MD-176 and MD-713, low-stress walking and bicycling infrastructure should be constructed to stops.

If recommendations from the CGM Plan are implemented to begin all-day weekday high quality transit service along I-97, low-stress walking and bicycling infrastructure should be constructed to stops in Parole Town Center, Benfield Blvd, Glen Burnie Town Center, Glen Burnie Light Rail Station, BWI Airport, and Arundel Mills Mall.

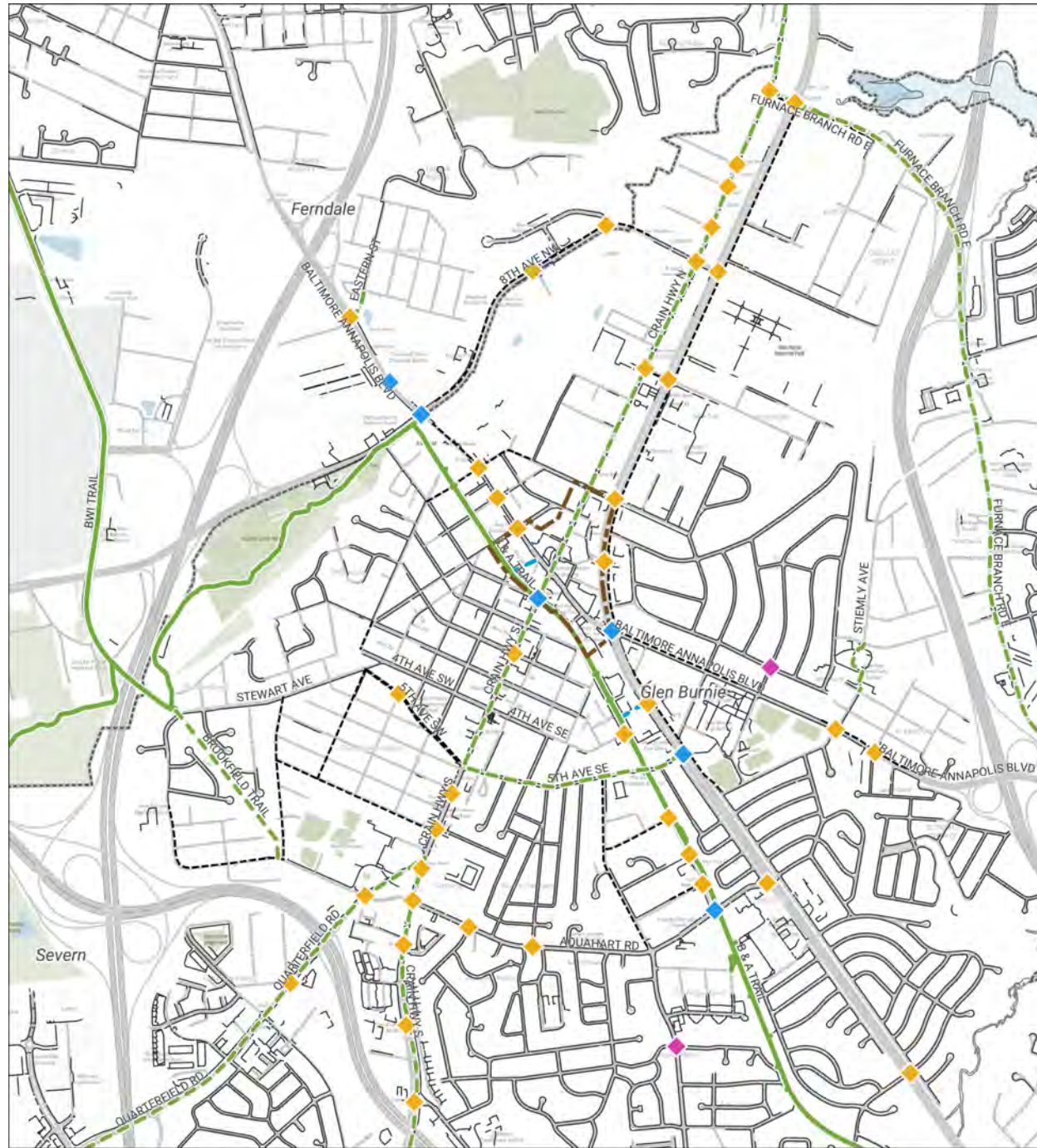
The CGM Plan recommends “improving the cross-section to accommodate bicycles and pedestrians, including designated bike lanes/route signing”; “implementing access management/driveway consolidation in the more commercial area” “implement a demand-responsive shuttle service between the Benfield Park & Ride and the Jones Station Road Park & Ride”; “implement bike shares and car shares at the Benfield Park & Ride and the Jones Station Road Park & Ride.”



A bicyclist on Benfield Road in Severna Park. Taken by Toole Design Staff.

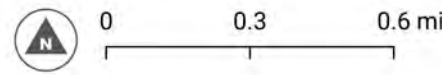
Glen Burnie

Glen Burnie Town Center Pedestrian Network

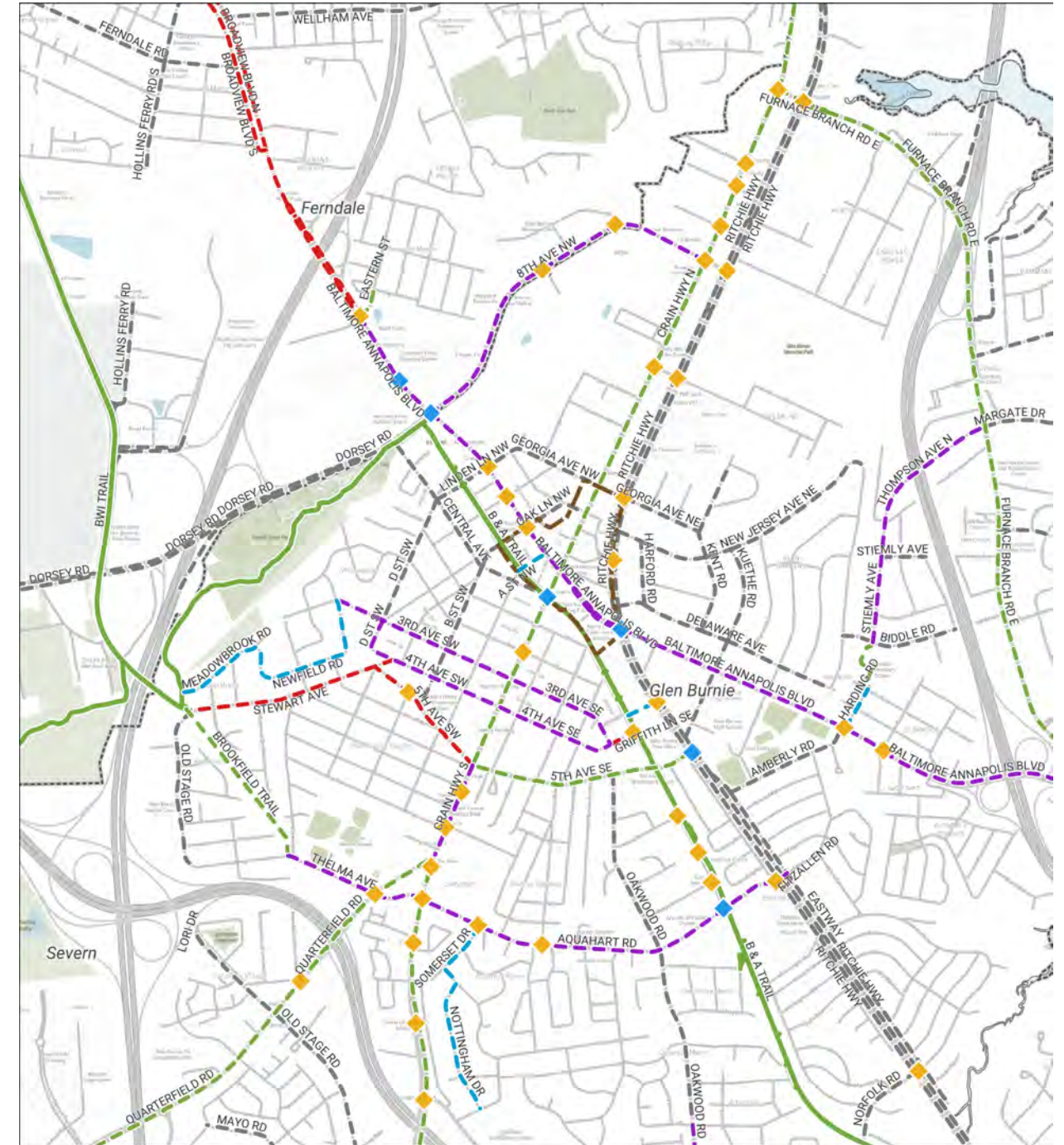


Glen Burnie Town Center
Proposed Pedestrian Network

- | Facility Types | | Proposed Crossing Improvements | |
|---------------------------------|----------------------|---|---|
| Proposed | Existing | Proposed Crossing Improvements | |
| Shared-use Path | Shared-use Path | Upgrade Pedestrian Crossing Elements | ◆ |
| Potential Multimodal Connection | Sidewalk | Upgrade Pedestrian and Bike Crossing Elements | ◆ |
| New Sidewalk | Town Center Boundary | New Pedestrian and Bike Crossing | ◆ |

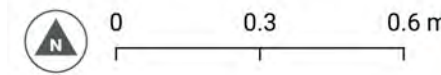


Glen Burnie Town Center Bicycle Network



Glen Burnie Town Center
Proposed Bike Network

- | Facility Types | | Proposed Crossing Improvements | |
|-----------------------|----------------------|---|---|
| Proposed | Existing | Proposed Crossing Improvements | |
| Shared-use Path | Shared-use Path | Upgrade Pedestrian and Bike Crossing Elements | ◆ |
| Separated Bike Lane | Bike Lane | New Pedestrian and Bike Crossing | ◆ |
| Bike Lane | Town Center Boundary | | |
| Low Stress Connection | | | |
| Secondary Network | | | |



Description and Destinations:

The Glen Burnie Town Center is organized around the Glen Burnie Town Center zoning district at the intersections of Baltimore Annapolis Boulevard (Route 648), Crain Highway (Route 3), and Ritchie Highway (Route 2). The Town Center zoning district has a mix of commercial, institutional, and government land uses, including the District Courthouse and the Anne Arundel Community College. Commercial activity extends out from the Town Center zoning district to the north to the Baltimore City line, transitioning to industrial uses west and north of Glen Burnie Bypass, and south on Crain Highway past Route 100. Outside of the commercial areas, the land use is characterized by low-medium density, single-family detached residential subdivisions (R5). The BWI Thurgood Marshall Airport is located less than a mile to the northwest of the Glen Burnie Town Center.

Within the Glen Burnie Town Center, Corkran Middle School, Glen Burnie High School, Anne Arundel Community College, and Monsignor Slade Catholic School are key destinations for students in the community. In addition, two private schools and two elementary schools are located south of the Town Center but north of Route 100, accessible by Oak Manor Drive and Oakwood Road. Other important multimodal connections include the commercial and medical facilities located south of Route 100 off Quarterfield Road.

The Cromwell Light Rail station is located just north of the Town Center and is not well connected for people walking or bicycling. The Baltimore and Annapolis Trail (B&A Trail) runs south from Dorsey Road (north of the Town Center zoning district) to Annapolis, serving as a major, regional multimodal route. At its northern terminus, the B&A Trail connects west to the BWI Trail that loops around the airport. Sidewalk coverage throughout Glen Burnie is generally comprehensive with some exceptions, including Ritchie Highway north of the Town Center, Crain Highway



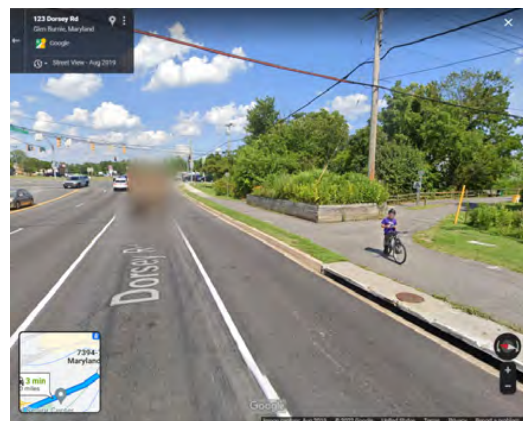
Students bicycling on a Dorsey Road shared-use path in a school zone in Glen Burnie

north of the Town Center, Quarterfield Road, and the Glen Burnie Heights neighborhood south of the Town Center.

Proposed improvements:

Needed walking and rolling improvements include:

- Continuous connections:
 - » From the Town Center zoning district and the B&A Trail to Cromwell Station
 - » On Route 2 (Crain Highway) as part of North County Corridor.
- Sidewalk gaps filled, prioritizing connections to economic opportunities and services and prioritizing major roads like Route 3 (Ritchie Highway), Route 2 (Crain Highway), Route 648 (Baltimore Annapolis Boulevard), and 8th Avenue.
- Community connections across major barriers such as Route 100 and Arundel Expressway (Rt 10).
- Shared streets, calm enough for bicyclists to share the road with drivers, on Somerset Drive/Nottingham Drive and Meadowbrook Road/Newfield Road as shorter-term alternatives to traveling on Crain Highway south of Aquahart Road and Stewart Avenue respectively.
- More frequent pedestrian crossings, especially across Route 2, Route 3, Route 648, and on Greenway Road to access the B&A Trail.

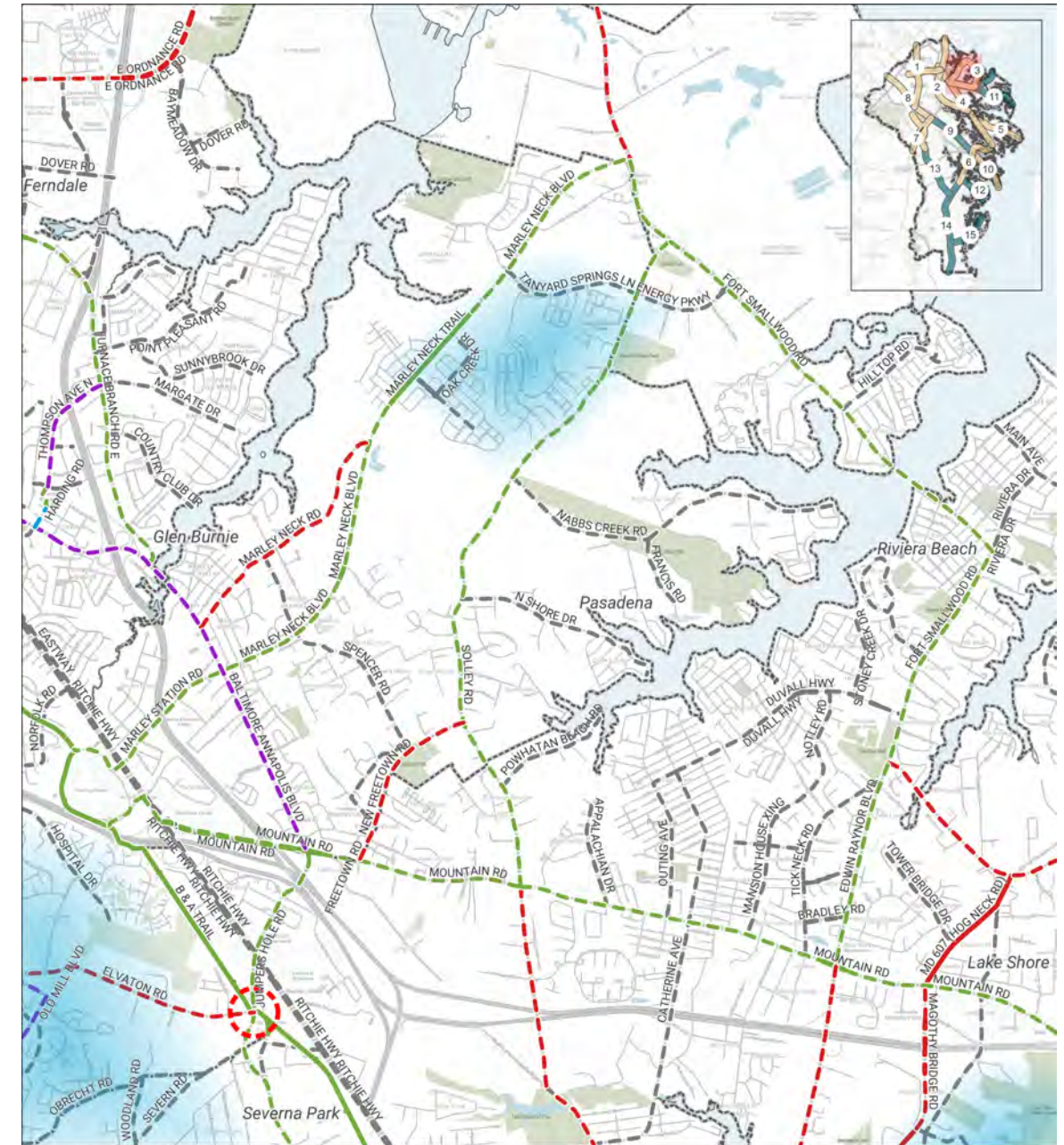


Marley Neck Loop

Description and Destinations:

The Marley Neck Loop connects schools, day care centers, health care facilities, industrial areas, residential areas, grocery stores, and shopping centers. Specifically, schools and education centers within walking distance of this perimeter include: Marley Elementary and Middle Schools, Freetown Elementary School, Northeast Middle and High Schools, High Point Elementary School, Sunset

Elementary School, Riviera Beach Elementary School, a YMCA Head Start at Freetown, and Anne Arundel Workforce Development. Business corridors like Route 173, Edwin Raynor Blvd, and Mountain Road in Pasadena follow the proposed route. The Marley Station Mall, also on the route, offers redevelopment potential. The peninsulas formed by Nabbs Creek, Stoney Creek, and Rock Creek offer unique recreation, social, cultural, and business opportunities with improved access.



3 - Marley Neck Loop Proposed Bike Network

Shared-use Path	Key Trail Intersections	Areas for Future Study
Separated Bike Lane	Existing Shared-use Path	Existing Bike Lane
Bike Lane	Low Stress Connection	Secondary Network

Scale: 0, 0.6, 1.2 mi

WALK & ROLL ANNE ARUNDEL!

Proposed improvements:

Needed walking and rolling improvements include:

- Continuous shared-use path connections:
 - » From where the B&A Trail reaches the Marley Station Mall eastward over Route 2;
 - » Eastward along Marley Neck Blvd;
 - » Southeastward along Fort Smallwood RD;
 - » Southward on Edwin Raynor Blvd
 - » Westward on Mountain RD until connecting with Route 2
- A direct and safe connection across Route 2 between the Loop and Marley Station Mall and the B&A Trail
- Bicycle lane on Edwin Raynor Blvd from Mountain Rd to Magothy Bridge Rd
- Bicycle lane on Marley Neck Rd between Baltimore Annapolis Blvd and Marley Neck Blvd
- Bicycle lane on Freetown/New Freetown Rd between Solley Rd and Mountain Rd

Previous Study Recommendations:

Major Intersections and Important Facilities (MIFF):

The MIIF proposes both bicycle lanes and sidewalks where there are gaps in the network. Due to the industrial nature of Fort Smallwood Road/MD-173, *Walk & Roll Anne Arundel!* recommends completing a shared-use path along the route for both pedestrians and bicyclists.

The MIFF recommends the following on Mountain Road/MD-177:

- 5' bicycle lanes, and sidewalks of 5' with a 3' buffer, on Mountain Road from Jumpers Hole Road to Waterford Road/MD-648
- 10' bicycle lanes on MD-177 between MD-100 and South Carolina Avenue
- 10' bicycle lanes east of South Carolina Ave

Edwin Raynor Boulevard Study:

In 2018, Anne Arundel County Department of Public Works assessed the constructability of walking and bicycling facilities on Edwin Raynor Boulevard from Old Crown Drive to Fort Smallwood Road in the "Edwin Raynor Boulevard Concept Study." The purpose of the study was to assess a design that would cost less and have fewer impacts than a previous design from 2007. The 2018 study evaluated the following components:

1. A 10' wide multi-use trail on one side of Edwin Raynor Boulevard with a 3' grass buffer or a 12' wide multi-use trail when no grass buffer strip is possible between the trail and the curb
2. A 5' sidewalk on the other side of Old Crown Drive to Fort Smallwood Road

The study evaluated right-of-way, utilities and environmental impacts and developed a preliminary construction cost estimate. The study was favorable for the proposed design, and further determined that the east side of Edwin Raynor Boulevard was optimal for the multi-use trail. The estimated cost of the project was \$5,119,821.

This plan recommends a shared-use path on Edwin Raynor Boulevard, which is consistent with the 2018 study.

Solley Road Improvements Study:

A 2018 study evaluated vehicular, bicycling and pedestrian improvements to Solley Road from Mountain Road (MD 177) to Fort Smallwood Road (MD 173). The report is entitled, "Solley Road Improvements Study."

The recommendations to bicycling and walking infrastructure include:

1. New bicycle lanes, a shared-use path, and sidewalk improvements along the 4500 foot segment between 300 feet South of Chestnut Springs Lane to MD 173. Connectivity would be provided to growing residential areas, industrial areas, Solley Elementary School, Solley Park, and Orchard Beach Volunteer Fire Station.
2. New bicycle lanes, a shared-use path, and/or sidewalk improvements along the 7100 foot segment of the corridor between MD 177 to Willow Tree Drive/ Lincoln Drive. Connectivity would improve to multiple residential areas along Solley Road and the commercial areas along Mountain Road.
3. Roundabouts, which would slow vehicular speeds, at:
 - a. Freetown Road
 - b. Lincoln Drive/Willow Tree Drive
 - c. North Shore Road
 - d. Nabbs Creek Road
 - e. Energy Parkway/Tanyard Springs Lane.
4. Several minor roadway improvements were noted as near term improvements, such as completing a sidewalk to Solley Park, extending a sidewalk or

shared-use path along Southbound Roadway from Solley Elementary School to Tanyard Springs Lane, and providing bicycle lanes and/or a shared-use path along Solley Road between Tanyard Springs Lane and MD 173.

Mountain Road Commercial Corridor Study:

A 2015 study evaluated improvements to Mountain Road between Solley Road/Waterford Road and Edwin Raynor Boulevard. The study is entitled, "Transportation Facility Planning – Mountain Road (MD 177): Commercial Corridor Study (Solley Road/Waterford Road and Edwin Raynor Boulevard).

The study recommends the following typical section:

1. 12' travel lanes
2. 5' bicycle lanes in each direction
3. 5' grass strip buffer
4. 5' sidewalks in each direction
5. 16' two-way left-turn lane
6. Improvements at intersections with Solley Road/ Waterford Road, Catherine Avenue, and Edwin Raynor Boulevard.

To provide a low-stress walking and bicycling facility on Mountain Road, *Walk & Roll Anne Arundel!* recommends a shared-use path in each direction instead of unprotected bicycle lanes and buffered sidewalks.

Corridor Growth Management Plan:

The CGM Plan recommends constructing new sidewalks and evaluating the feasibility for bicycle infrastructure along Magothy Bridge, Hog Neck Road, and Fort Smallwood Road.

MD 177 Corridor Study:

A 2015 study analyzed modifications to reduce congestion on MD 177 from Magothy Beach Road to MD 2. The study recommended intersection modifications along the MD 177 corridor at Solley Road, Catherine Avenue, Edwin Raynor Boulevard, and MD 607. The study also analyzed removing shoulders that are currently used by bicyclists in order to add travel lanes on Catherine Avenue and Edwin Raynor Boulevard, which *Walk & Roll Anne Arundel!* does not recommend. The study did not analyze sidewalk construction.

Baltimore & Annapolis Trail Connections

Description and Destinations:

The Baltimore & Annapolis (B&A) Trail is a popular trail – a cherished amenity in Anne Arundel County that provides opportunities for exercise, connection to nature, and

access to essential destinations. The trail currently starts in Glen Burnie where it intersects with the BWI Trail and ends in Annapolis after crossing the Naval Academy Bridge. While primarily an off-road facility, the portion of the trail from US 50 to Annapolis is currently on Route 2, a busy highway. Additionally, given that its off-road portions still run parallel to Route 2/Governor Ritchie

Highway, access to the trail is not consistent, particularly to and from peninsular communities northeast of the trail. Providing safe and more direct connections to the trail would potentially open up the trail and its many benefits for tens of thousands of people from their homes and many of the County’s commercial centers, services, and even schools.

- Arnold Rd with a shared-use path to connect to Arnold community, its elementary school, and the Broadneck Trail

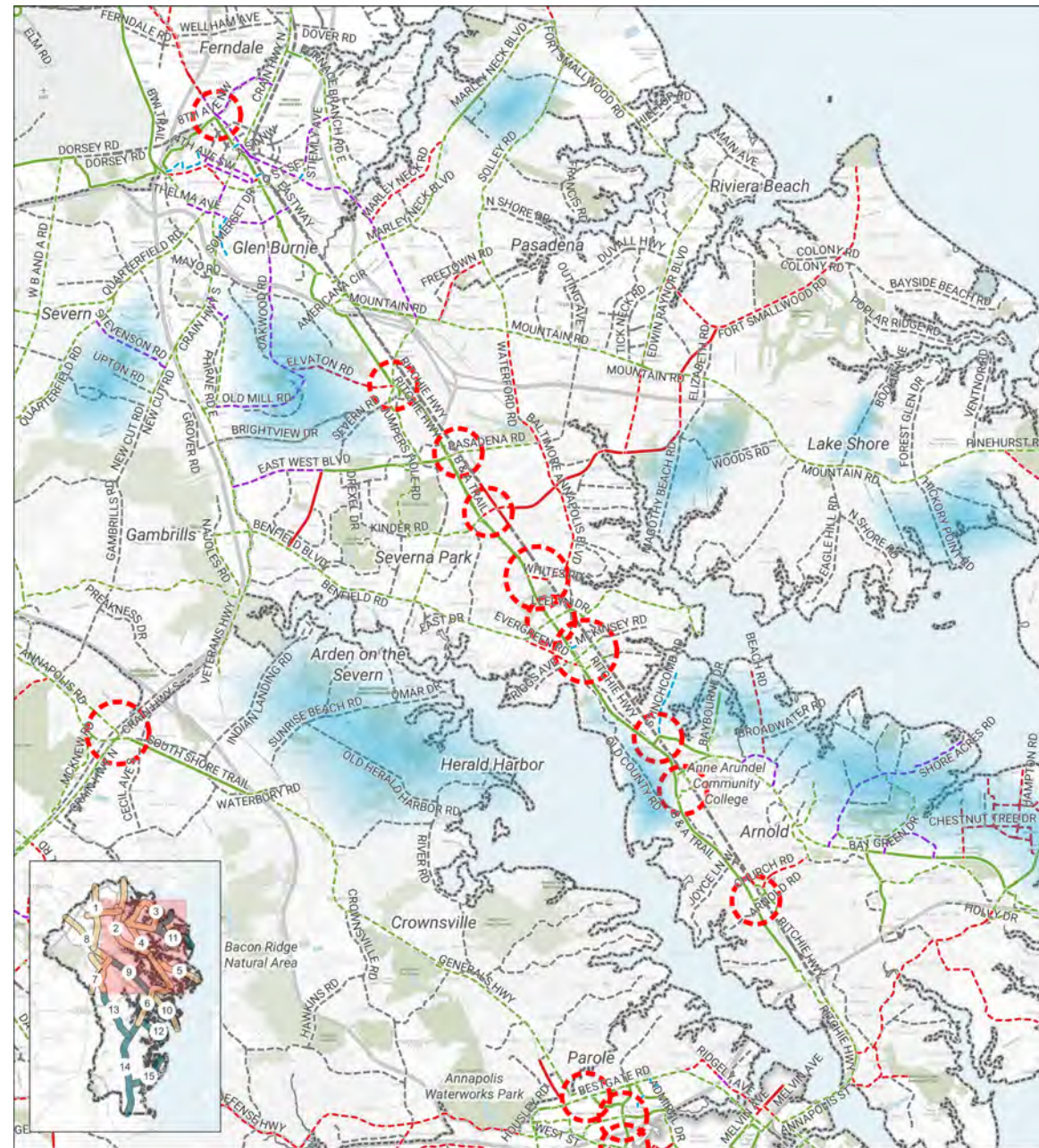
The currently shared on-street portion of the B&A Trail along Boulders Way and Ritchie Hwy should be upgraded to be a separated bike lane (with a walking lane if space allows) on Boulders Way and a shared-use path or sidepath along Ritchie Hwy leading to the Naval Academy Bridge. The bridge’s existing bike lanes, which double as shoulder lanes, should be upgraded to separated bike lanes or shared-use paths.

Previous Study Recommendations:

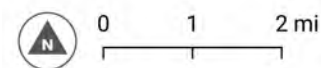
Corridor Growth Management Plan:

The recommendations in the CGM Plan for MD 2 North (page 1-7) include a new sidewalk on both sides of MD 2 and allow for transit-oriented development in Severna Park Marketplace, Harundale Plaza, Marley Station Mall and Glen Burnie Town Center.

Additionally, while a shared-use path along MD-2 North between US-50 and MD-10 is not recommended at this time because of the proximity to the parallel B&A Trail, if the roadway were to be widened to 6 lanes, a shared-use path should be constructed along MD-2 North to allow safe and more direct access to the essential destinations along the corridor. Crosswalks and other pedestrian safety features should also be installed at least every 1/8 mile along Route 2.



4 - B&A Connections Proposed Bike Network



Proposed improvements:

Access to the trail and essential destinations would open the most potential at the following points (from north to south):

- Baltimore Annapolis Blvd and Dorsey Rd, with a shared-use path extension of the B&A Trail to reach the Glen Burnie (Cromwell) Light Rail Link station
- Jumpers Hole Rd with a shared-use path to connect to Pasadena shopping centers and surrounding neighborhoods
- East West Blvd and Pasadena Rd with a shared-use path to connect to Pasadena Elementary and Baltimore Annapolis Blvd
- Earleigh Heights Rd with a shared-use path to connect to Kinder Farm Park and existing bike lanes on Magothy Bridge Rd
- Whites Rd with a bike lane to meet planned bike lanes on Baltimore Annapolis Blvd
- Leelyn Dr via improved crossing and shared-use path to connect to Severna Park schools on Benfield Rd and the Arundel Beach community
- Improved crossing on Baltimore Annapolis Blvd at trailhead close to Severna Park Elementary School to connect to Park Plaza Shopping Center
- Riggs Ave and McKinsey Rd via shared-use path to improve east-west connections within Severna Park, including the elementary school
- Cypress Creek Rd via bicycle lanes to reach Cypress Creek community
- Improved trail crossing on Route 2/Ritchie Hwy at Jones Station Rd, with bike boulevards towards Manhattan Beach community
- College Pkwy via new trail crossing to connect to Anne Arundel Community College more directly, as well as Phase 3 of the Broadneck Trail and Arnold-Sandy Point PIA destinations

Arnold-Broadneck Peninsula

Description and Destinations:

College Parkway serves as the main connection between Arnold and Sandy Point, with its peninsulas and their large residential areas, and the rest of the County. There are several schools along the corridor, notably Broadneck High School and Anne Arundel Community College as well

as two middle schools (Magothy River, Severn River) and five elementary schools (Arnold, Belvedere, Broadneck, Cape Saint Claire, Windsor Farm). The County is currently building a shared-use path along College Parkway, colloquially named the Broadneck Trail, with Phases 1 and 2 already completed.

Proposed improvements:

Proposed improvements along this PIA will use College Parkway's completed Broadneck Trail as a spine, with spurs leading to essential destinations on the peninsula:

- Shared-use paths on the following roadways:
 - » Peninsula Farm Road and Baybourne Dr, providing access to Magothy River Middle School
 - » Jones Station Rd and Church Rd, connecting Arnold Elementary School and the broader Arnold community
 - » Shore Acres Rd, providing access to Broadneck Elementary School
 - » Cape Saint Claire Rd from College Pkwy to River Bay Rd, connecting the Cape Saint Claire community and their access to the nearby elementary school
 - » Broadneck Rd from Bay Dale Dr to Benson Ln, providing access to Broadneck Park and Windsor Farm Elementary School
 - » St. Margarets Rd/MD 179 from E College Pkwy to MD 648, with a possible extension on Ritchie Rd and Sharpe Rd to Hwy 450
 - » St. Margarets/Whitehall Rd from MD 179, continuing along Interstate 50 until it transitions to Skidmore Dr, continuing along Skidmore Dr, then continuing along Old Ferry Slip Rd, then both segments of Oceanic Rd to reach the shoreline. Especially with any future development, people will be easily able to walk and bicycle to job centers along this corridor.
 - » S Beach Rd from E College Pkwy to Sandy Point Playground
- Bicycle lanes should be provided, if possible given limited space, on the following roadways:
 - » Green Holly Drive from Rolling View Dr to College Pkwy
 - » Rolling View Drive from Mariners Cir to Green Holly Dr
 - » Chestnut Tree Drive from Green Holly Dr to Hilltop Dr
 - » Blue Ridge Drive from Chestnut Tree Dr to Hilltop Dr
 - » Summit Drive from St. Margarets Dr to Blue Ridge Dr
 - » Hampton Road from River Bay Rd to Blue Ridge Dr

- » Hilltop Drive from Mountain Top Dr to Cape St Clair Rd
- » Mountain Top Drive from Hampton Rd to Cape St Clair Rd
- » Lake Claire Drive from River Bay Rd to Latrobe Dr
- » Crestview Drive from Lake Claire Dr to St. Margarets Dr
- » St. Margarets Dr from Broadview Dr to Southview Dr
- » Southview Drive from Round Top Dr to Chestnut Tree Dr
- » Shore Acres Road from Magothy Ave to College Pkwy
- » Mago Vista Rd from Beach Rd to Jones Station Rd, connecting to Belvedere Elementary School

These bicycle lanes would provide access to Broadneck High School and Cape St. Claire Elementary School, the library, jobs, community centers, parks, and more.

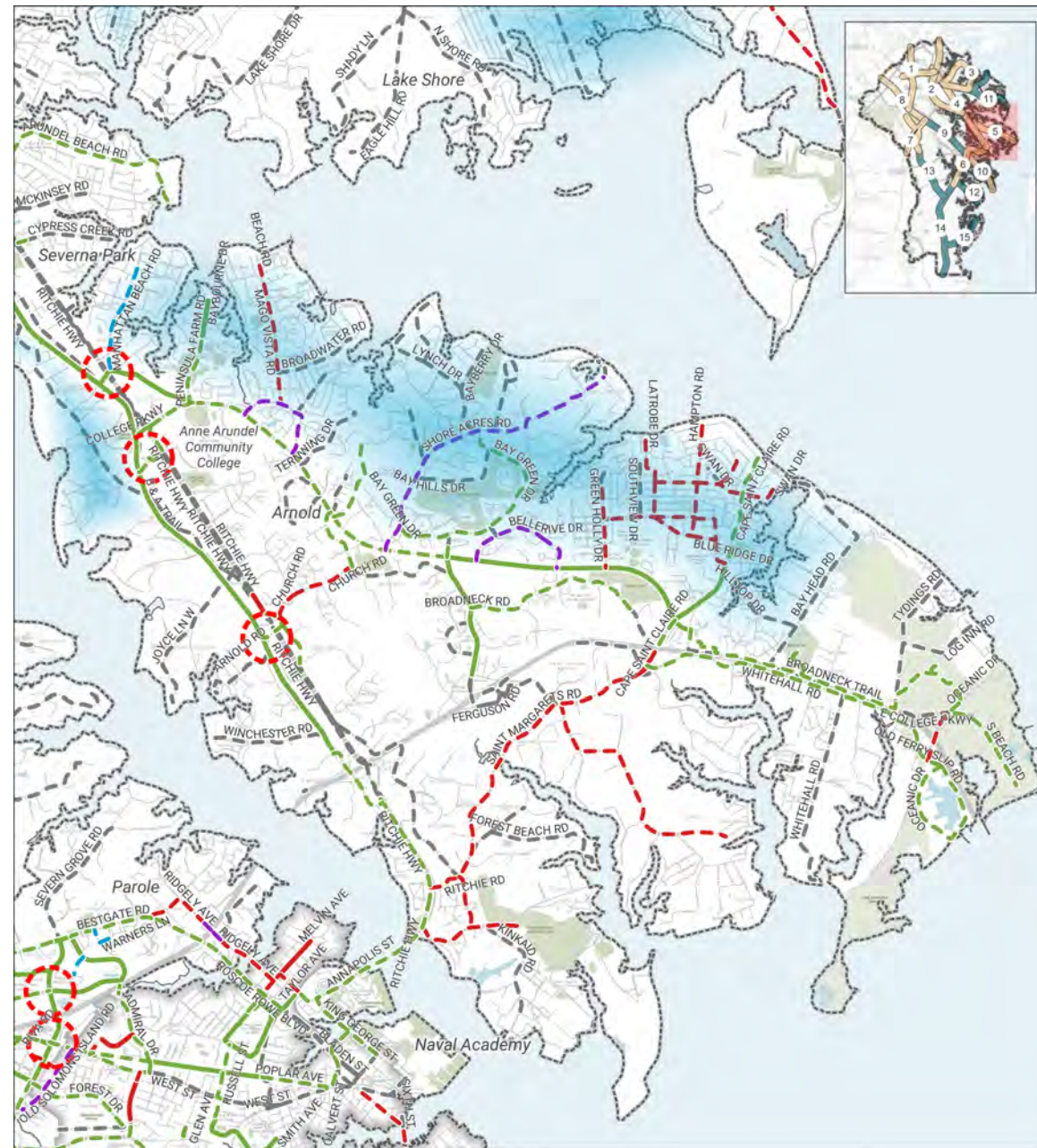
- Protected cycle tracks on:
 - » Bellerive Rd from College Pkwy to College Pkwy
 - » Jones Station Rd from College Pkwy to College Pkwy
- A new trail access point next to the community college to connect to the B&A Trail

Previous Study Recommendations:

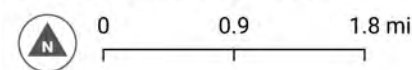
Major Intersections/Important Facilities:

Walk & Roll Anne Arundel!'s recommendations align with MIFF, which proposed:

- Evaluating an extension of the Annapolis Transit Gold Line
- Extending Broadneck Trail to the B&A Trail
- Adding bicycling lanes on College Parkway
- Completing the sidewalk network on College Parkway
- Promoting New Urbanism development with Complete Streets and no cul-de sacs.



5 - Arnold-Broadneck Peninsula Proposed Bike Network



- Facility Types**
- Proposed**
- Shared-use Path
 - Separated Bike Lane
 - Bike Lane
 - Low Stress Connection
 - Secondary Network
- Existing**
- Shared-use Path
 - Bike Lane
- Key Trail Intersections
- Areas for Future Study



Parole-Annapolis-Edgewater

Description and Destinations:

Improving walking and rolling conditions in this corridor is critical to connecting communities to the County's largest concentration of jobs at Annapolis Towne Center, Westfield Annapolis Mall, downtown Annapolis and surrounding areas, the County government center

(Heritage Complex on Riva Road), a major hospital, and Annapolis High School. The corridor is a key connection between Annapolis/Parole and South County, and it roughly follows Route 2. In addition to the previously listed malls and town center, other key destinations include the Heritage Complex, South River High School, Central Middle School, Central Special School, Woodland Beach, and surrounding communities.

In Parole, Route 2 and its extension north of Route 50 have shared-use paths. South of Route 50 there is also a proposed bike lane/shared-use path along Riva Road. Route 2 would be a more practical route as it is the major thoroughfare connecting Annapolis to Edgewater and is better connected to the key destinations.

Proposed Improvements:

There are opportunities to expand on existing nascent walking and rolling infrastructure in this area, particularly:

- A new shared-use path constructed on Solomons Island Road/Route 2 and Medical Parkway/Route 50 in Parole.
- The County's first separated bicycle lane on Jennifer Road.

Priority recommended improvements include:

- Completion of the South Shore Trail and connection to the Poplar Trail in the City of Annapolis.
- A shared-use path on Solomons Island Road/Route 2 south of Route 50. Route 2 is the main road in this area, has an activity center in Edgewater with shopping centers, South River Colony, the Woodland Beach Community, and multiple schools, such as South River High School, Central Middle School, and Central Special School.
- Upgraded bicycle and pedestrian crossing elements at signalized intersections within the improvement area.
- Construction of new pedestrian connections between Riva Shopping Center, Festival at Riva, Annapolis Harbour Center, and other Riva Rd retail destinations in Parole/Annapolis.
- Construction of a new pedestrian connection between Gateway Village and Westfield Annapolis Mall in Parole.
- Protected bicycle lanes on Riva Road over the South River.
- Protected bicycle lanes on Riva Road over the South River.
- A separated bicycle lane on Mayo Rd in Edgewater.
- A shared-use path on Stepneys Lane.
- Shared-use paths on Solomons Island Rd and Central Ave E in Edgewater.
- A shared-use path on Central Ave and Loch Haven Road in Selby-on-the-Bay.

- Addition of sidewalks along Harry S Truman Pkwy, Route 2, and Forest Dr. in Parole.
- Additional improvements within the Parole Town Center are described in the Town Center section below.

During the public engagement process, many residents voiced support for new facilities on Route 2 or Riva Road, which are the only connections across the South River and where protected bicycle lanes are nonexistent.

Recommendations were not made within the City of Annapolis boundary in this Plan; however, coordination with the City is needed to ensure an interconnected regional network.

Previous Study Recommendations:

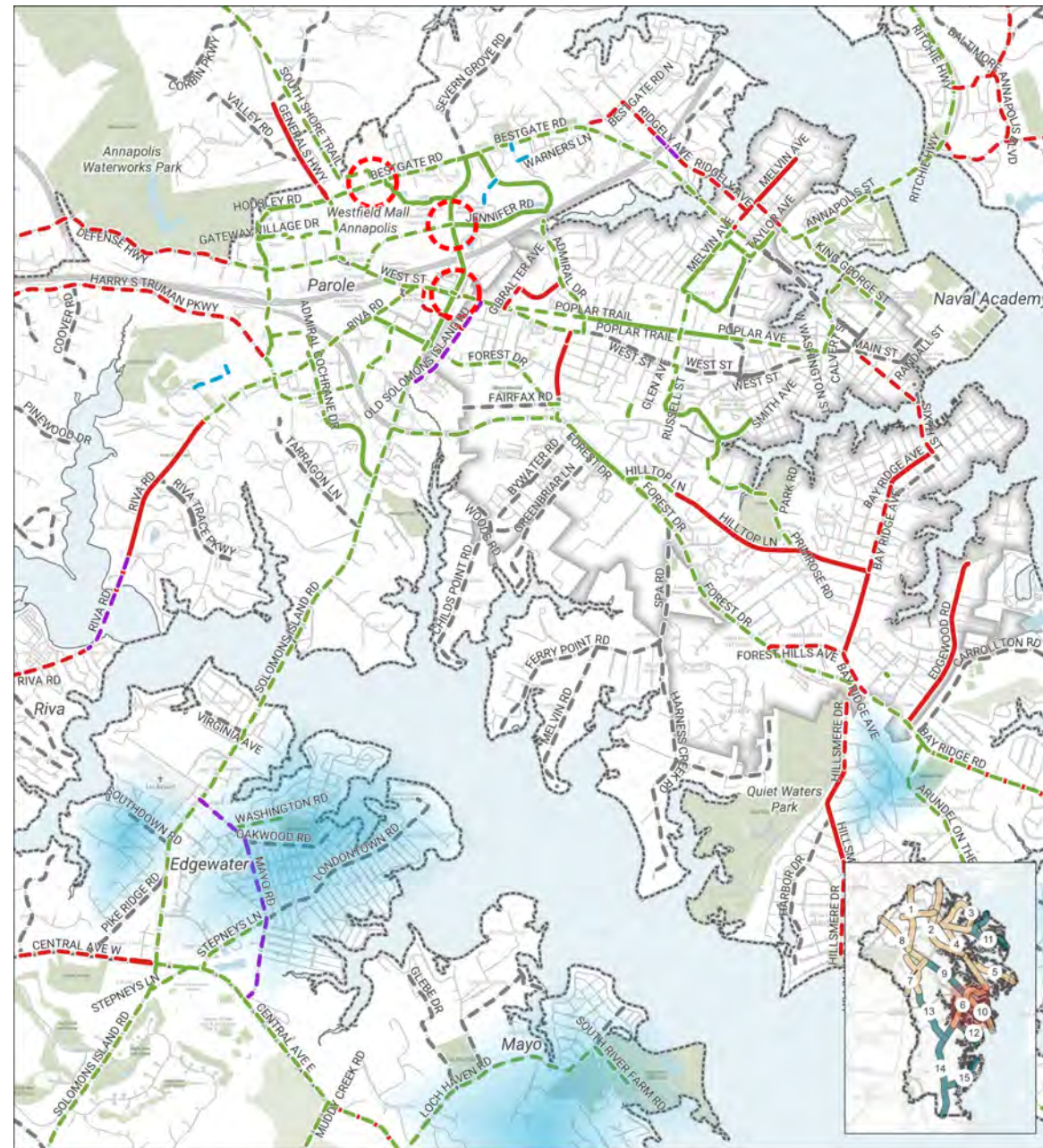
Parole Mobility Study:

Completed in 2021, the Parole Mobility Study looked to identify the necessary short- and long-term improvements within Parole Town Center to meet its future land use, urban design, and multimodal transportation needs. In doing so, Parole Town Center will work to provide a connected transportation network supported users of all modes, ages, and abilities. The report includes an analysis of existing conditions, future conditions, recommendations, and ways to incorporate these recommendations into the Parole Urban Design Plan.

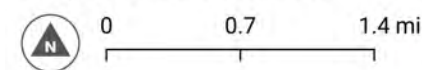
The study identified that Route 2, MD 178, and MD 665 all have at least one segment over its capacity, and additional roadways and segments will be over their capacity by 2045. This was one of the building blocks for the recommendations in the study. The recommendations, with a goal of improving conditions and connectivity by all modes, include the addition of new roadway extensions, construction of a shared-use path network, improving pedestrian facilities, reducing block sizes to increase walkability, increasing access to transit, and encouraging an increase in non-vehicular modes of transportation.

Major Intersections/Important Facilities:

MIFF recommends a multi-use path parallel to MD-665/ Aris T. Allen Boulevard. In addition, it recommends the following transit and TOD improvements: commuter rail with terminus at Annapolis Towne Center; or bus rapid transit or light rail with a focal point at Annapolis Towne Center; increased density at Annapolis Towne Center and adjacent areas to support high-quality transit service.



6 - Parole-Annapolis-Edgewater
Proposed Bike Network



Corridor Growth Management Plan:

The CGM Plan recommends on MD-2 South/Solomon's Island Road: construct missing sidewalks, evaluate feasibility to add bicycle lanes and/or signed routes along side streets, parallel routes or MD-2, and incorporate improved site design to orient new buildings to the street and encourage more walkable frontage, and develop improved access controls such as frontage road creation and streetscape treatments as redevelopment occurs to create a boulevard style cross-section.

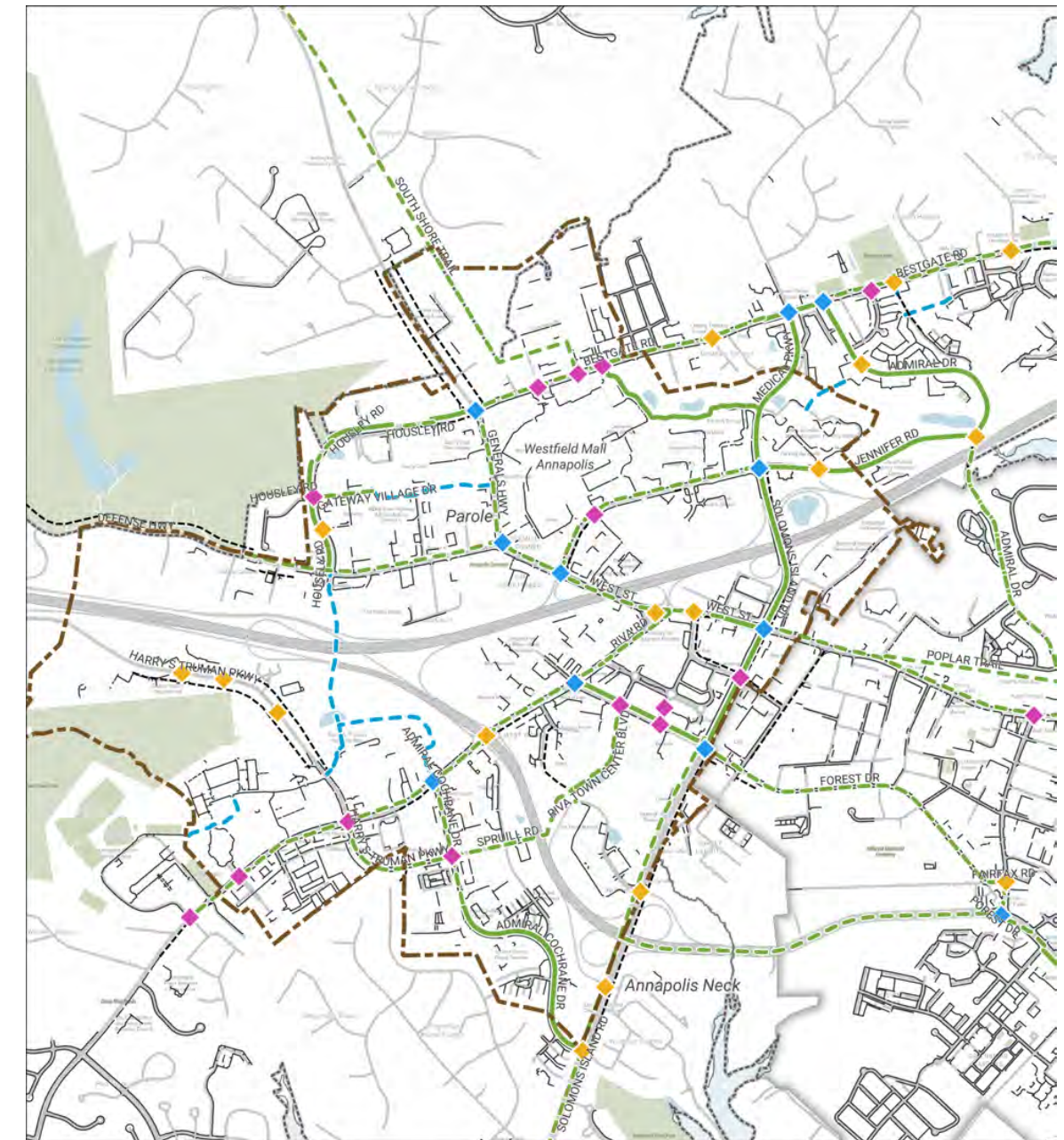
Walk & Roll Anne Arundel's bicycling and walking recommendations align with the recommendations of the 2012 CGM Plan.

Per the recommendation in the CGM's Table 1-2 for "high quality transit service" along US-50, with stops in Annapolis, Parole, Davidsonville, Bowie, and onto Washington, D.C.: it is recommended that low-stress walking and bicycling infrastructure be provided to a future stop in Parole and Davidsonville.

If recommendations from the CGM Plan are implemented to begin all-day weekday high quality transit service along I-97, low-stress walking and bicycling infrastructure should be constructed to stops in Parole Town Center and Benfield Blvd (as well as stops at Glen Burnie Town Center, Glen Burnie Light Rail Station, BWI Airport, and Arundel Mills Mall, in the Odenton-Relay and Severna Park-Brooklyn PIAs).

Parole

Parole Town Center Pedestrian Network

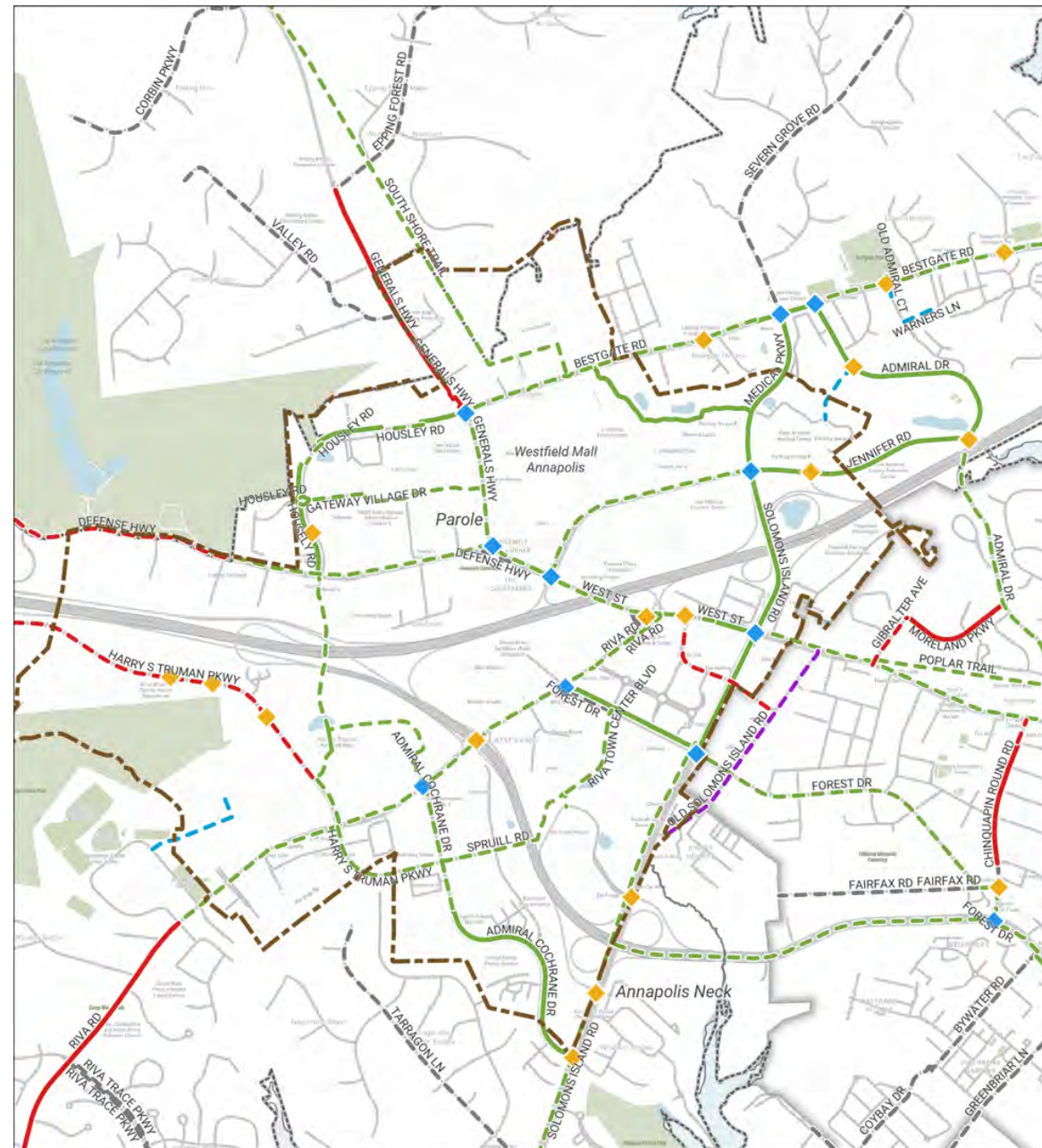


Parole Town Center Proposed Pedestrian Network

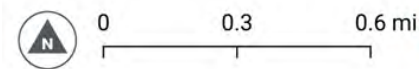
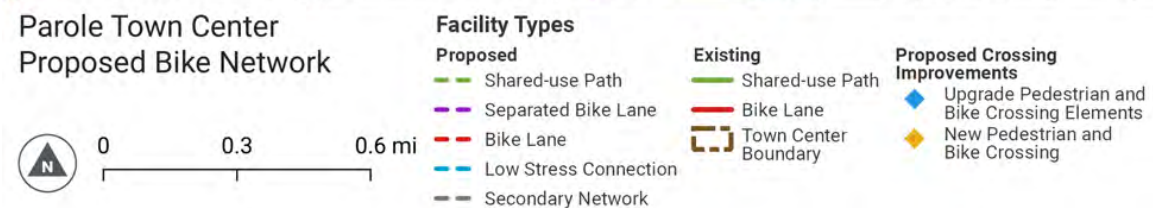
Proposed	Existing	Proposed Crossing Improvements
<ul style="list-style-type: none"> — Shared-use Path — Potential Multimodal Connection — New Sidewalk 	<ul style="list-style-type: none"> — Shared-use Path — Sidewalk — Town Center Boundary 	<ul style="list-style-type: none"> ◆ Upgrade Pedestrian Crossing Elements ◆ Upgrade Pedestrian and Bike Crossing Elements ◆ New Pedestrian and Bike Crossing

0 0.3 0.6 mi

Parole Town Center Bicycle Network



Parole Town Center
Proposed Bike Network



Description and Destinations:

The Parole Town Center has a number of designated subareas, including Westfield Annapolis, Annapolis Town Centre at Parole, Government Park, and residential subdivisions. Schools in the area include Annapolis High School on Riva Road west of the Town Center (the site of which is also home to a swim center attracting many youth), Walter S. Mills-Parole Elementary School, Annapolis Area Christian Middle School, and Monarch Academy Annapolis. Transit is provided by Annapolis Transit, Anne Arundel County, MD Travel Link, County Ride, and MTA, resulting in fairly comprehensive coverage between subareas and multiple overlapping routes on many of the arterials in the Town Center. Connecting people to transit is especially important in Parole because the roads connecting subareas are challenging for people to cross or traverse on foot or bicycle. Connections to proposed transit hubs require special attention, especially connections to the Harry S. Truman Park and Ride on Riva Road and to Annapolis Mall.

Crossings across major and secondary barriers are infrequent and are not safe for walking or bicycling. Some segments of the shared-use path or trail network have been constructed; these need to be connected to each other and the regional system. The Parole Mobility Study (2021) notes that some of the existing shared-use paths, such as along Admiral Drive, MD 2, and MD 450, are low comfort due to insufficient separation between the sidepath and high-speed roadways. Comprehensive sidewalk connections are missing within commercial superblocks with large parking lots and in residential subdivisions.

Proposed improvements:

Needed walking and rolling improvements include:

- Shared-use paths on:
 - » Bestgate Rd from Generals Hwy to N Bestgate Rd
 - » Riva Rd from Aris T Allen Blvd to Speicher Dr
 - » Housley Rd from Annapolis Ridge Rd and Generals Hwy
 - » A Housley Rd extension from Defense Hwy Harry S Truman Pkwy
 - » Defense Hwy from West St to N River Rd
 - » Gateway Village Dr from Housley Rd to Generals Hwy
 - » Generals Hwy from the Parole Town Center Line to West St
- » West St from Generals Hwy to Holly Ave
- » Jennifer Rd from West St to Medical Pkwy
- » Aris T Allen Blvd from Solomons Island Rd to Forest Dr
- » Riva Rd from MD 665 to West St
- » Rowe Blvd from N Bestgate Rd to Kirkley Rd
- » The Annapolis Harbour Center Shopping Center property from the proposed bicycle/pedestrian bridge over MD 665 to Forest Dr
- » An extension of Admiral Cochrane Dr from Riva Rd to Harry S Truman Pkwy
- » The alignment of the proposed South Shore Trail
- A bridge for pedestrians and bicyclists over MD 665 to connect Spruill Rd & Womack Dr to the Annapolis Harbour Center Shopping Mall
- Separated bicycle lanes on:
 - » Spruill Rd and Harry S Truman Pkwy from Harry S Truman Pkwy & Riva Rd to Spruill Rd & Womack Dr
 - » Admiral Cochrane Dr from Riva Rd to Solomons Island Rd
- Bicycle lanes on:
 - » Somerville Rd from West St to Solomons Island Rd
 - » Harry S Truman Pkwy from Coover Rd to Riva Rd
- Sidewalks on:
 - » Solomons Island Rd from John Hanson Hwy to Lake Shore Dr
 - » Harry S Truman Pkwy from the Parole Town Center Boundary to the Anne Arundel County Recreation and Parks Building
 - » Riva Rd from east of Admiral Cochrane Dr to Admiral Cochrane Dr
 - » Riva Rd from MD 665 to West of Hearne Rd
 - » Annapolis Harbour Center from Hearne Ct to Annapolis Harbour Center Dr & Solomons Island Rd
 - » Solomons Island Rd from Jennifer Rd to Admiral Cochrane Dr
 - » Old Solomons Island Rd from West St to Forest Dr
 - » Generals Hwy from Knollwood Dr to Bestgate Rd
 - » Old Admiral Ct from Admiral Dr to the end of the road
 - » Housley Rd from Generals Hwy to west of Generals Hwy

- » West St from Jennifer Rd to Riva Rd
- » Somerville Rd from West St to Old Solomons Island Rd
- Upgraded and more frequent pedestrian crossings throughout Parole

Previous Study Recommendations:

Riva Road Trail Feasibility Study:

Completed in 2021, this study analyzed the possibility of implementing a shared-use path on one side of Riva Road. The study recommends a trail along the west side of Riva Road from Speicher Drive to Harry S. Truman Parkway and along the east side of Riva Road from Harry S. Truman Parkway to Forest Drive. This alignment serves key pedestrian and bicycle destinations, including Annapolis High School and the Farmers Market. In addition, the alignment also reduces right-of-way costs given that the west side of Riva Road south of Harry S. Truman Parkway is County property.

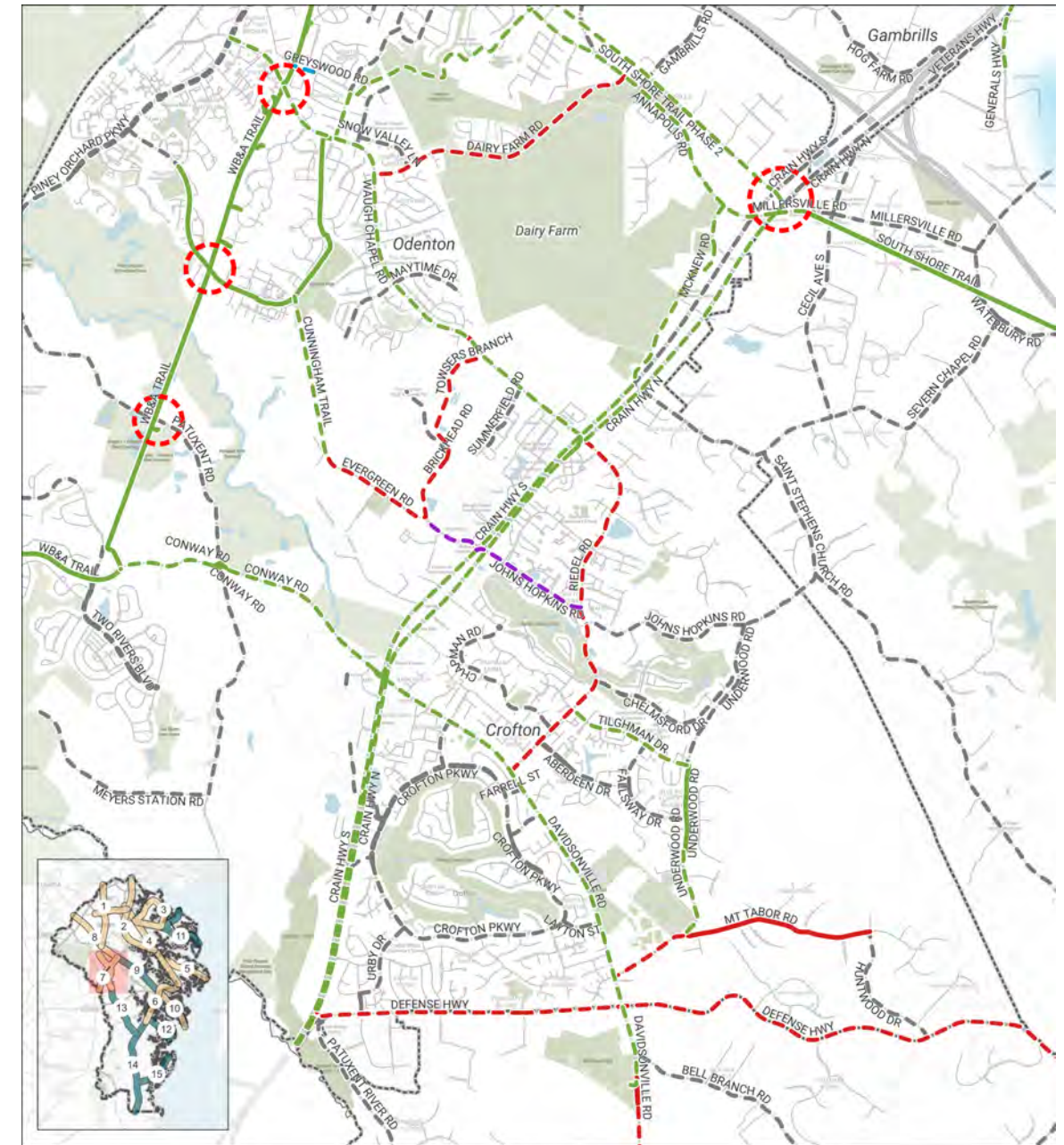
A crossing is recommended at the signalized intersection with Harry S. Truman Parkway.

Crofton Connections

Description and Destinations:

Crofton is a growing suburban area on the far west side of the County near the Prince George’s County line. There are several large subdivisions under construction and a major new shopping center called Waugh Chapel Towne Center. Route 3 (Crain Highway) roughly divides Crofton in half and is a major highway with local, commuter, and freight traffic given its role as a connector between I-97

and US-301. A wide and difficult corridor to cross, Route 3 is a dual carriageway with complex turning movements at major intersections. It is very congested at rush hour with miles-long backups, and sidewalks and bike lanes do not exist along the length of the corridor. Only two crosswalks exist on Route 3 within the Crofton area about a mile apart from each other: one at the intersection of Waugh Chapel/Riedel Roads, and the second at Evergreen/Johns Hopkins Roads. During fieldwork, bicyclists were observed crossing Route 3.



7 - Crofton Connections Proposed Bike Network

<p>Facility Types</p> <p>Proposed</p> <ul style="list-style-type: none"> — Shared-use Path — Separated Bike Lane — Bike Lane — Low Stress Connection — Secondary Network 		<p>Areas for Future Study</p> <p>Key Trail Intersections</p> <p>Existing</p> <ul style="list-style-type: none"> — Shared-use Path — Bike Lane
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0 0.6 1.2 mi

There are very few alternative routes to travel through this area. Officials have proposed converting Route 3 to an expressway, as well as building a parallel trail, but that plan remains unfunded. Pedestrian and bicycle crossings are few and far between as well, even though established subdivisions are primarily to the east of Route 3 while new shopping centers, subdivisions, and the WB&A Trail are to the west. Schools are located on both sides of Route 3. Crofton Elementary, Middle, and High Schools as well as Crofton Meadows, Crofton Woods, and Nantucket Elementary Schools are located to the east of Route 3. Four Seasons, Piney Orchard, and Waugh Chapel Elementary Schools are to the west of Route 3.

A shared-use path on Davidsonville Road and Waugh Chapel Road would in particular provide a continuous bicycling connection through the residential areas of Crofton and connect to the WB&A Trail in Odenton.

Proposed Improvements:

Proposed improvements for *Walk & Roll Anne Arundel!* include:

- Providing two bidirectional shared-use paths on Route 3, given the expansiveness of the highway, which functions as two parallel roads. Particularly on the southbound side, the ROW exists for this proposed facility.
- Bicycle lanes on Riedel Rd, which intersects Route 3, in Crofton.
- In Crofton and Odenton, shared-use paths on:
 - » Davidsonville Rd
 - » Conway Rd
 - » Tilghman Dr
 - » Annapolis Rd
 - » Waugh Chapel Rd (which are in design).
- Bicycle lanes on Evergreen Road and Johns Hopkins Rd in Crofton.
- A Cunningham Trail connection between Evergreen Rd and Strawberry Lake Way in Odenton.
- Several grade-separated bridges for bicyclists and pedestrians to safely cross Route 3.

Previous Study Recommendations:

Waugh Chapel Road Transportation Facility Planning Study:

A 2019 study evaluated Waugh Chapel Road between Maytime Drive and New Market Lane in order to

recommend improvements for all modes of transportation between the Waugh Chapel Shopping Center to the existing and planned neighborhoods to the west.

Pedestrian and bicycling recommendations resulting from this study include:

1. Providing continuous sidewalks along Waugh Chapel Road
2. Providing either new bicycle lanes or shared-use paths along Waugh Chapel Road
3. Constructing a signalized roundabout and installing leading pedestrian intervals at the intersection with Maytime Drive
4. Installing leading pedestrian intervals at the intersection with New Market Lane/Wigeon Way

The study estimates the SUP concept would cost approximately \$8 million while a concept with sidewalks and 4-6' bicycle lanes on both sides of Waugh Chapel would cost approximately \$5.9 million.

Walk & Roll Anne Arundel! recommendations for a shared-use path are therefore consistent with the 2019 study. The project team recommends locating shared-use paths away from any constructed roundabouts in order to achieve perpendicular crossings between vehicles, bicyclists, and pedestrians.

Path between Evergreen Road and Strawberry Lake Way:

While the “Waugh Chapel Road Transportation Facility Planning Study” did not include concept-level planning estimates for additional connection ideas raised during public engagement, it does note public comments on multimodal connections, such as:

1. Support for a non-motorized path between Evergreen Road and Strawberry Lake Way (parallel to Waugh Chapel Road)

Corridor Growth Management Plan:

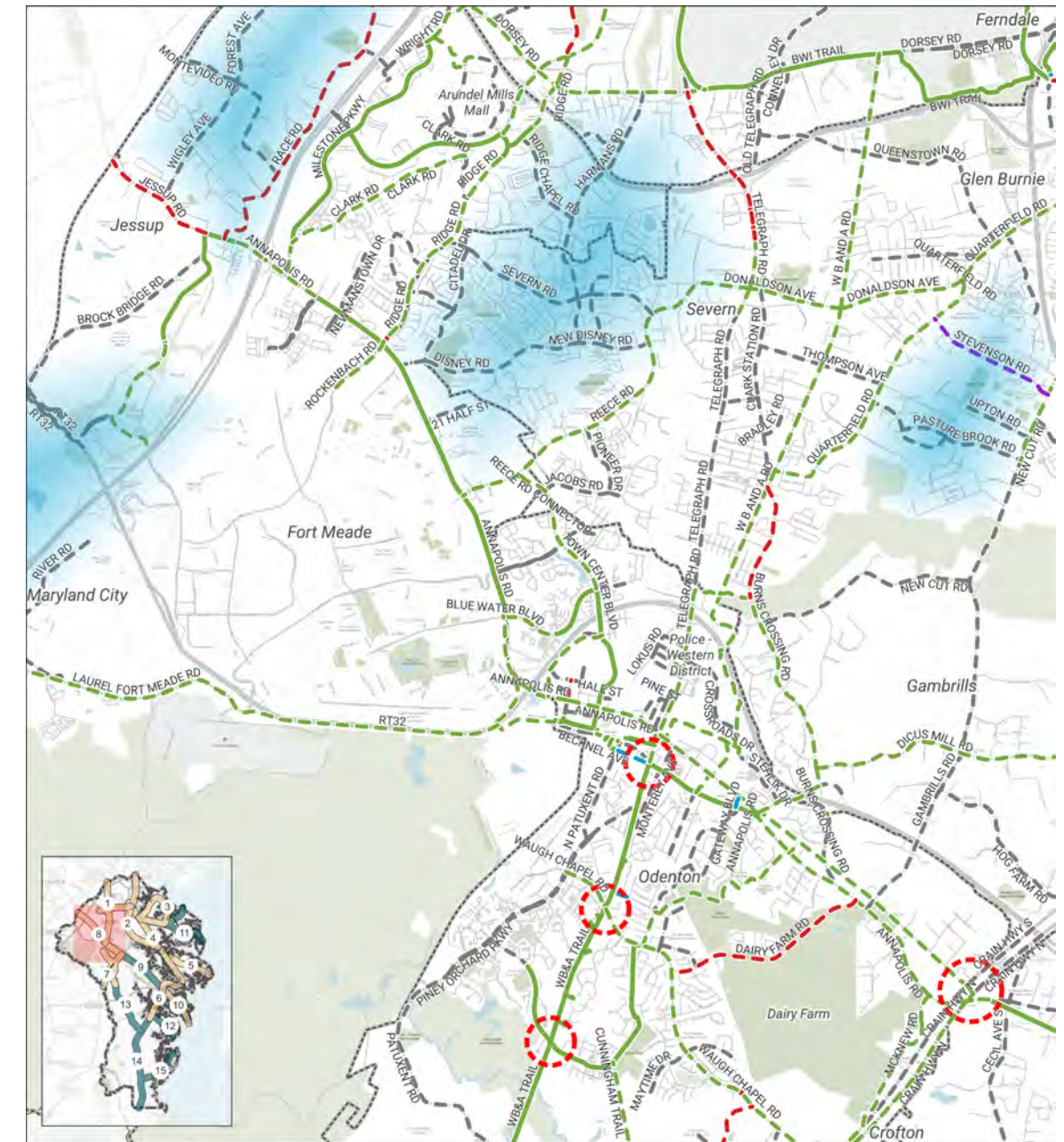
The CGM Plan recommends low-stress walking and bicycling infrastructure, like shared-use paths, be constructed to connect to future transit stops, in addition to existing stops, at Crofton, Waugh Chapel, Odenton, Benfield Blvd, Glen Burnie Light Rail, BWI Airport, and Arundel Mills.

Jessup-Gambrills-Odenton

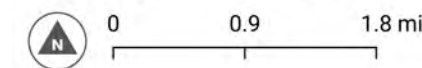
Description and Destinations:

State Route 175 provides a connection between Jessup, Odenton, and Gambrills. In Jessup, there are residential areas, businesses, the Jessup Correctional Institution, industrial centers such as produce and seafood distribution, schools, and the Jessup MARC station – all of which are accessible using Route 175. Moving along the

route towards the southeast, travelers cross into Hanover and Severn, a mostly residential area of subdivision design with some industry, commerce, and office parks, and Fort Meade, a U.S. Army Garrison. The Odenton Town Center, with its historic town center, mix of land uses, MARC station and segment of WB&A Trail, begins after crossing Patuxent Freeway. Continuing further southeast, Gambrills and Millersville are mostly rural and separated by Route 3.



8 - Jessup-Gambrills-Odenton
Proposed Bike Network



- | | |
|-----------------------|-------------------------|
| Facility Types | Areas for Future Study |
| Proposed | Key Trail Intersections |
| Shared-use Path | Existing |
| Separated Bike Lane | Shared-use Path |
| Bike Lane | Bike Lane |
| Low Stress Connection | |
| Secondary Network | |



Proposed Improvements:

The proposed improvements include:

- Existing and planned shared-use paths run along parts of Annapolis Rd/MD 175 and in adjacent residential areas, such as on Blue Water Boulevard, Reece Road/Town Center Boulevard, Milestone Parkway, and the National Business Parkway. Paths here should be connected to each other to create a continuous walking and rolling pathway.
- Safer and more direct crossings should be built for people traveling on foot, bicycle, and wheelchair on Route 175. Access to the two MARC stations, in Jessup and Odenton, should also be improved.
- A shared-use path on Laurel Fort Meade Rd (MD 198).
- Connections across Laurel Fort Meade Rd/MD 198 in the Maryland City/Russett/Laurel area
- A shared-use path on Technology Dr and National Business Pkwy from Technology Dr until the existing SUP in Jessup.
- A bicycle lane on Jessup Road and Race Rd from Jessup Road until Parkway Dr S in Jessup.
- A realignment of Race Road with a bicycle lane, from Jessup Rd & Chestnut Ave, extending north as a new road until joining the existing Race Rd, following existing County plans.
- A shared-use path on Clark Rd, between Milestone Pkwy and Watts Ave and Ridge Rd between Arundel Mills Blvd in Hanover to Fort Meade, crossing MD 175/Annapolis Rd.
- Bicycle and pedestrian crossings should be upgraded at signalized crossings.
- Additional improvements within the Odenton Town Center are described in the Town Center section.

Previous Study Recommendations:

Race Road/Jessup Village Previous Planning Study:

A 2015 study analyzed recommendations of the 2004 Jessup/Maryland City Small Area Plan to reduce congestion with modifications to Race Road and reduce vehicular turns along MD-175 by using “grid of platted ‘paper roads’ within the Jessup Village.” The modifications to Race Road include: widening, signal coordination, and addition of pedestrian and bicycling infrastructure. The project is in close proximity to Jessup Elementary School. The report is entitled, “Race Road/Jessup Village Planning Study: Findings Summary.”

The study recommends relocating Race Road with 11’ travel lanes and adding 5’ bicycle lanes, a sidewalk with a minimum 5’ width, and a 5’ buffer between the sidewalks and bicycle lanes on both sides of the street.

Clark Road from Jane Court/Gleneagle Drive to Watts Avenue:

This report recommended either:

- Constructing a sidewalk and buffer along the eastbound roadway and providing a 7’ buffer or
- Constructing a 10’ SUP along the eastbound roadway and 4’ shoulders on both sides of the road.

Walk & Roll Anne Arundel! recommends constructing a 10’ SUP along both sides of the road.

Corridor Growth Management Plan:

If recommendations from the CGM Plan are implemented to begin improved public transportation service, low-stress walking and bicycling infrastructure should be constructed to each stop. Carpool lots should also be connected between residential, commercial, and mixed-use areas via low-stress walking and bicycling infrastructure.

MD 175 Phase II Feasibility Study:

Completed in 2010 to develop a preliminary Draft Purpose and Need Statement, the MD 175 Phase II Feasibility Study looked to improve travel safety and connectivity between Odenton and Fort George G. Meade (FGGM) for motorists, pedestrians, and bicyclists. It also looked to improve access management along MD 175 within project limits, contribute to the revitalization of North Odenton’s commercial district, enhance the study area’s aesthetic, and accommodate future traffic volumes.

The study led the development of three potential concepts for the corridor. Concept 1 was developed by applying a four-lane typical section to the existing centerline along MD 175 throughout the full corridor. Concept 2 was developed by applying a five-lane typical section to the existing centerline along MD 175 throughout the full corridor. Concept 3 was developed in order to identify access management issues and measurements along the corridor that consolidated and closed some unneeded unsignalized access points along MD 175.

The estimated total costs, including construction and ROW acquisition, for the proposed alternative came out to approximately \$69,100,000.

MD 175 Sidewalk Feasibility Study:

Completed in 2017, the MD 175 Sidewalk Feasibility Study looked at the feasibility of constructing a new sidewalk along MD 175 from the Sappington Station Road roundabout to MD 170. The existing conditions forced pedestrians to walk on grass or through parking lots. The study area is relatively dense with apartments and businesses and has four bus stops within the study area. Of the four bus stops, only two are connected to an existing sidewalk. The addition of this sidewalk would provide for a safer pedestrian experience and better connect residents to surrounding destinations such as schools, businesses, and healthcare facilities.

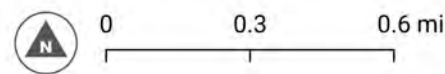
The proposed facility was a five-foot wide sidewalk with a three-foot grass buffer along the entire corridor, on both the eastbound and westbound side of the roadway. The study found a few pinch points along the corridor due to the existing utility poles, so the grass buffer might not be provided along the entire corridor. However, the study did come to the conclusion that the proposed sidewalk was reasonable and feasible due to the increased pedestrian network and most of the existing curb and gutter would be able to remain as-is.

Odenton Town

Odenton Town Center Pedestrian Network



Odenton Town Center
Proposed Pedestrian Network



Facility Types

Proposed

- Shared-use Path
- Potential Multimodal Connection
- New Sidewalk

Existing

- Shared-use Path
- Sidewalk
- Town Center Boundary

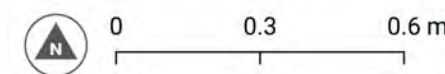
Proposed Crossing Improvements

- Upgrade Pedestrian Crossing Elements
- Upgrade Pedestrian and Bike Crossing Elements
- New Pedestrian and Bike Crossing

Odenton Town Center Bicycle Network



Odenton Town Center
Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Bike Lane
- Town Center Boundary

Proposed Crossing Improvements

- Upgrade Pedestrian and Bike Crossing Elements
- New Pedestrian and Bike Crossing

Description and Destinations:

Odenton Town Center is a designated zoning district and is bounded by Patuxent Parkway (Rt 32) and generally Odenton Road to the south. The area is rapidly redeveloping with plans for major and minor subdivisions on much of the land north of Annapolis Rd (Rt 175), which has been predominantly a mix of large-lot commercial and industrial uses. South of Annapolis Road, the land use is mostly medium-low density with single family detached homes. Fort Meade is located to the northwest of the Town Center across Patuxent Way (Rt 32). Town Center across Patuxent Way (Rt 32). Odenton Elementary School is within Odenton Town Center.

The Penn Line Odenton MARC Station is located on Odenton Road. Amtrak service also uses the rail corridor but does not stop at this station. The rail corridor creates a barrier for the continuation of Odenton Road, which is split into eastern and western segments. There is limited sidewalk infrastructure when accessing the station from either direction. There is a mezzanine to connect passengers from the inbound and outbound platforms.

The Washington, Baltimore and Annapolis Trail (WB&A Trail) begins at the intersection of Odenton Road and Piney Orchard Parkway and runs south to the Patuxent River and the Prince Georges County border. A shared-use path runs along Odenton Road from the WB&A Trail/Piney Orchard Parkway east to the roundabout intersection of Odenton Road, Annapolis Road, Sappington Station Road, and Higgins Drive. Another shared-use path has been constructed as part of a new residential development on Town Center Boulevard from Odenton Road to Charter Oaks Boulevard, providing access across Patuxent Parkway; it then continues part of the way to Annapolis



A worker walking towards a bus stop in Odenton. Taken by Toole Design Staff.

Road on Charter Oaks Boulevard. The Odenton pedestrian network has significant gaps, especially connecting individual residential subdivisions, along Annapolis Road/ MD 175 and Telegraph Road, and within the northern portions of the Town Center.

Proposed improvements:

Needed walking and rolling improvements include:

- Continuous connections:
 - » On Route 175 (Annapolis Road) within the Town Center zoning district and continuing into the Jessup-Gambrills-Odenton PIA
 - » On Route 170 (Telegraph Road/Piney Orchard Pkwy) to connect to the WB&A Trail and as part of the Northwest County PIA
- Sidewalk gaps filled in neighborhoods and connecting to schools, especially on Route 175, N Patuxent Rd, Waugh Chapel Rd, Monterey Avenue, Rita Drive, King Malcolm Avenue, Bruce Avenue, and Hammond Lane
- Community connections across major barriers such as Patuxent Pkwy (MD 32) to connect to Fort Meade, the National Security Agency, and residential developments
- Shared-use path connections to Fort Meade at designated gates
- Upgraded and more frequent pedestrian crossings, especially across Route 175, Route 170, and Odenton Road.



A cycle track. Taken by Toole Design staff.

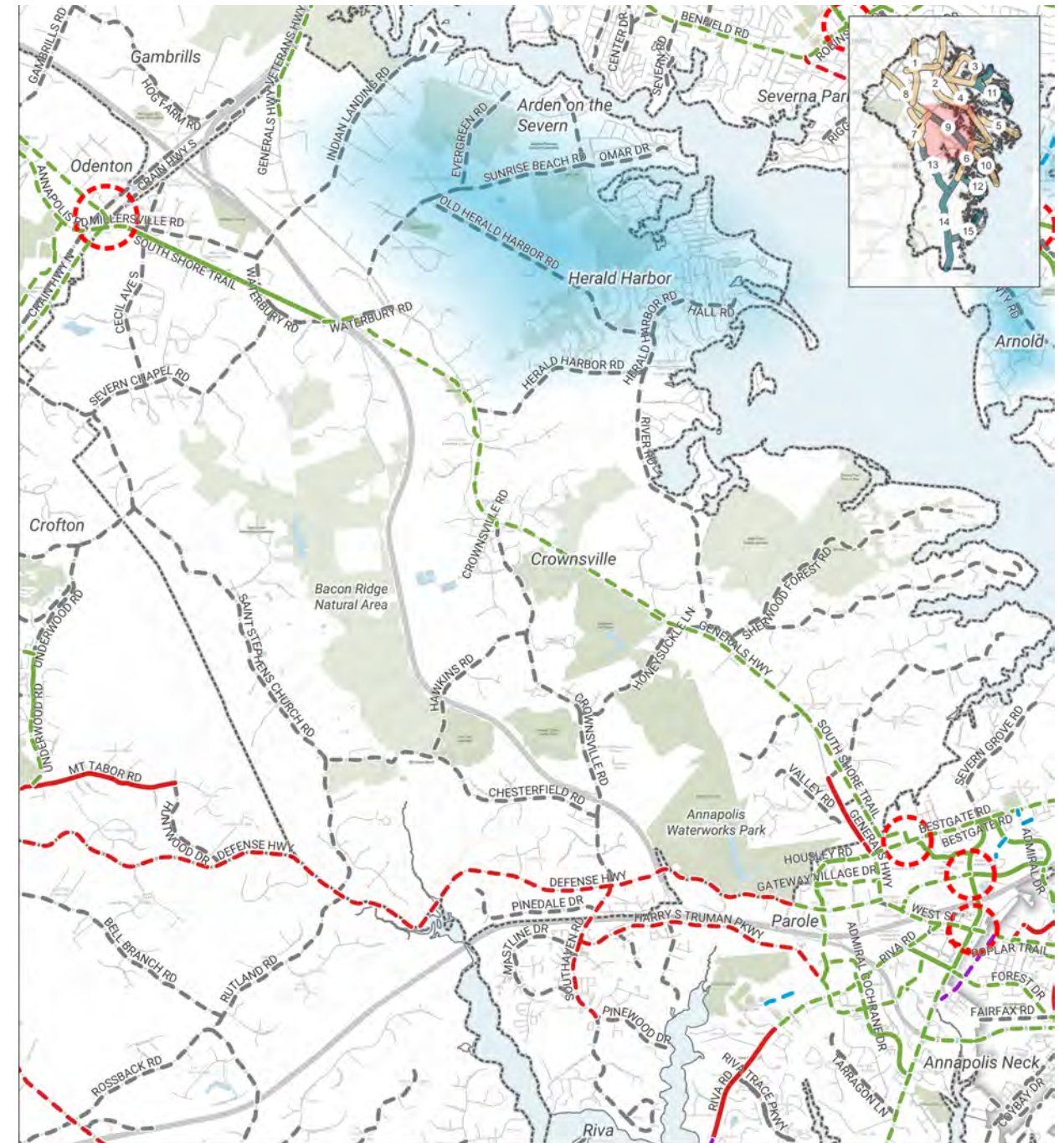
South Shore Trail

Description and Destinations:

The South Shore Trail follows the abandoned roadbed of the WB&A Railroad between Annapolis and Odenton. Once complete, it will provide about 14 miles of paved pathway for people on foot, bicycle, and wheelchair. It will

connect with the WB&A Trail in Odenton on the western end, the Poplar Trail in Annapolis on the eastern end, and the B&A Trail.

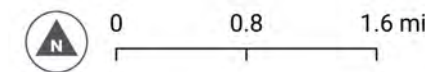
The constructed portion of the South Shore Trail begins along Route 175/Millersville Road in Millersville, close to Crain Highway, and ends at Waterbury Road. Another



9 - South Shore Trail Proposed Bike Network

Facility Types

Proposed	Areas for Future Study
Shared-use Path	Key Trail Intersections
Separated Bike Lane	Existing
Bike Lane	Shared-use Path
Low Stress Connection	Bike Lane
Secondary Network	



completed portion runs between Anne Arundel Medical Center and Annapolis Plaza in Annapolis. The trail passes through mainly rural/estate areas, but the connections to Odenton and Parole Town Centers create potential for a future key commuter route. In addition to the Town Centers, Millersville Elementary School and Rolling Knolls Elementary School are located within this PIA, providing the opportunity to create safe connections to the schools. The main concern is confirming the route of the trail and identifying opportunities to improve wayfinding and crossings.

Proposed improvements:

The proposed improvements include:

- Ensuring that the route is safe and comfortable, such as by designing a shared-use path;
- Providing safe crossings for people on foot, bicycle, and wheelchair;
- Providing connections between the trail and essential destinations near the route; Millersville Elementary School, for example, is along the completed part of the trail, but not accessible to it; and
- Wayfinding.

The portions in Odenton and Parole would be useful to commuters on both foot and bicycle.

Previous study recommendations:

The County previously studied and designed the South Shore Trail.

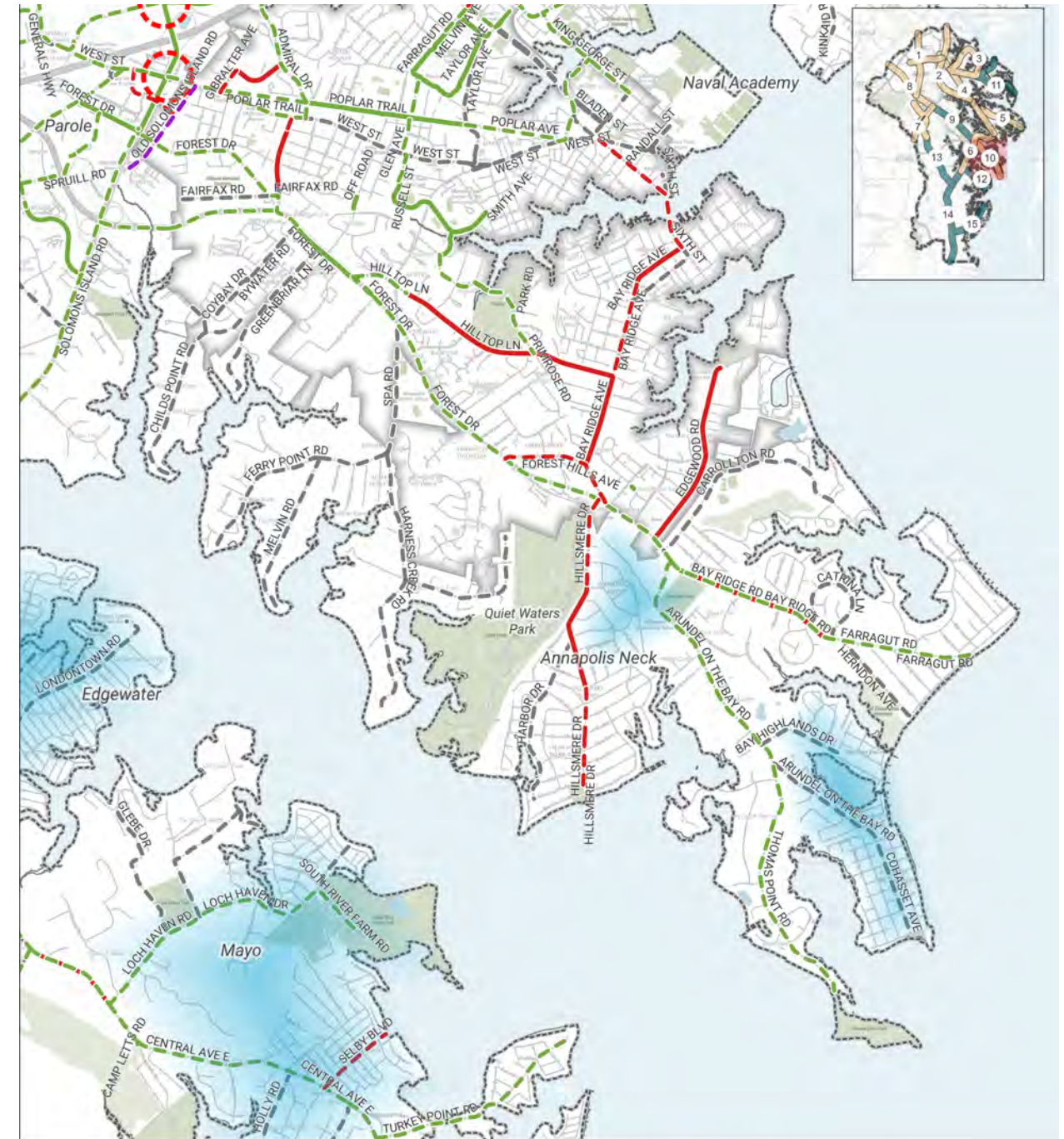
Annapolis Neck

Description and Destinations:

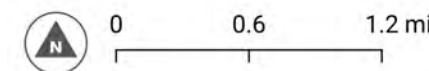
This PIA is adjacent to the City of Annapolis. The networks of this PIA must be coordinated with the City; however, *Walk & Roll Anne Arundel!* does not make recommendations within City boundaries. The City of

Annapolis has its own adopted plans, which should be consulted for improvements that lead to destinations within the city.

Continuous walking and rolling facilities will connect Parole to residential areas and businesses within the City of Annapolis and communities at the end of the Annapolis



10 - Annapolis Neck Proposed Bike Network



Facility Types

- Proposed**
- Shared-use Path (green dashed line)
 - Separated Bike Lane (purple dashed line)
 - Bike Lane (red dashed line)
 - Low Stress Connection (blue dashed line)
 - Secondary Network (grey dashed line)
- Existing**
- Shared-use Path (green solid line)
 - Bike Lane (red solid line)
- Areas for Future Study (light blue shaded area)
- Key Trail Intersections (red dashed circle)



peninsula, including Hillsmere, Highland Beach, and Arundel on the Bay. Regionally significant routes, such as Forest Drive, are addressed in this plan given their role in connecting Anne Arundel County's communities to each other and to the County's four elementary schools (Georgetown East, Hillsmere, Mills-Parole, and Tyler Heights), Annapolis Middle School, and Quiet Waters and Thomas Point Parks.

Improvements to Bay Ridge Road, Farragut Road, and Forest Drive (a County-owned residential collector between Parole and Thomas Point), would improve access to shopping centers, Quiet Waters Park, several schools, such as Annapolis Middle School, Hillsmere Elementary School, Tyler Heights Elementary School, and private schools, and other destinations. While recommendations within the City of Annapolis are not part of this Plan, coordination with the City is needed to ensure an interconnected regional network.

Proposed Improvements:

Proposed walking and bicycling connections include:

- A shared-use path on the full extent of Forest Dr from Annapolis eastward down the peninsula, with the SUP continuing on Bay Ridge Rd and Farragut Rd until the end of the peninsula.
- It is important that routes in neighborhoods along Forest Dr and Bay Ridge Rd have connecting walking and bicycling infrastructure. At the eastern end there are multiple roads that branch off Forest Dr and Bay Ridge Rd, including Forest Hills Ave, Hillsmere Dr, Arundel on the Bay Rd, Thomas Point Rd, and Bay Ridge Ave. Bicycle lanes are proposed on these roads.
- Other proposed improvements shown on the map are within the City of Annapolis, which are shown but not studied as part of this Plan.

Previous Study Recommendations:

Major Intersections/Important Facilities:

Walk & Roll Anne Arundel! recommendations align with the 2016 MIFF study, which studied Forest Drive from Old Solomons Road to east of Hilltop Lane. MIFF proposed adding bicycle lanes on Forest Drive, including on the segment from MD-2 to Chinquapin Round Rd, and constructing missing sidewalks. MIFF also recommended increased density and transit-oriented development in the Annapolis Towne Center area, New Urbanism development, and Complete Streets.

MTA Commuter Bus:

If MTA commuter bus service is extended from the Riva Road Park & Ride lot to Bay Ridge Avenue, low-stress walking and bicycling infrastructure should be built to additional transit stops.

Poplar Trail Extension - Feasibility Study:

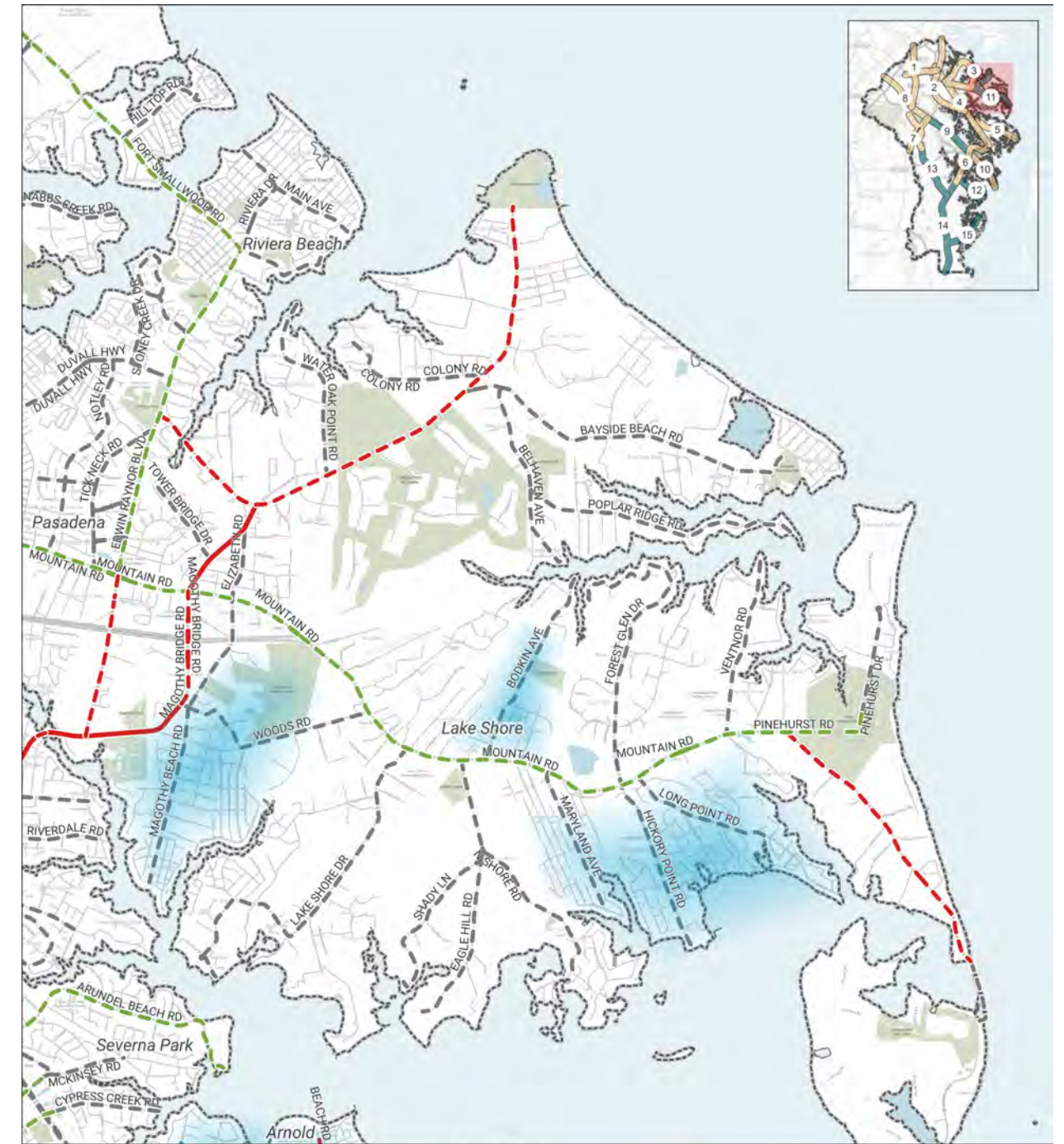
Anne Arundel County considered several alternatives to connect the existing Poplar Trail and the existing South Shore Trail for people walking and rolling. As the preferred alternative, the study team identified Alternative 1: a path along MD 450 from MD 2 to MD 393. This route is the least expensive and most direct east-west connection. The link will also connect to the B&A Trail and the South Shore Trail, as well as two multi-state trails: The East Coast Greenway and the American Discovery Trail.

The City of Annapolis is conducting a additional study to design the extension to 30% design.

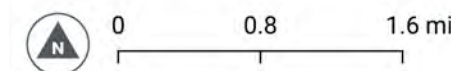
Pasadena

Description and Destinations:

Pasadena consists of a set of peninsulas with limited roads in and out. The primary routes consist of Fort Smallwood Road, Hog Neck Road, and Mountain Road.



11 - Pasadena
Proposed Bike Network



Facility Types

- | | | |
|-----------------------|-----------------|-------------------------|
| Proposed | Shared-use Path | Areas for Future Study |
| Separated Bike Lane | Bike Lane | Key Trail Intersections |
| Low Stress Connection | Bike Lane | Existing |
| Secondary Network | Shared-use Path | |



The peninsula is home to low-density suburban and estate areas with essential destinations consisting of:

- Schools: Chesapeake High School, Chesapeake Bay Middle School, and Bodkin, Lake Shore, and Fort Smallwood Elementary Schools
- Lake Shore Athletic Complex
- Downs Park
- A private golf course
- Compass Point Golf Course
- Small public beaches.

The roadways on this peninsula are narrow.

Proposed improvements:

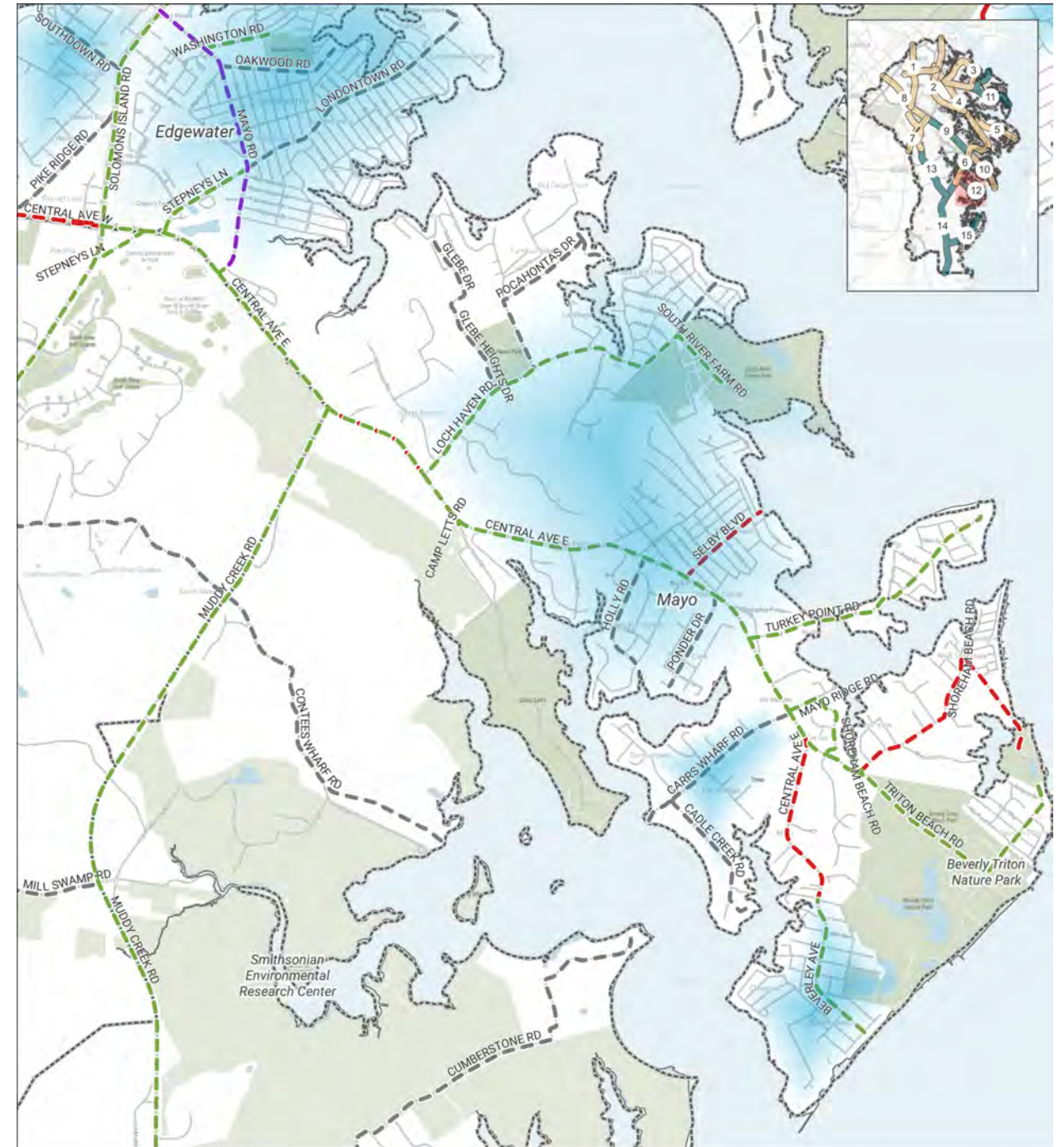
- A shared-use path on Mountain Road to Downs Park
- Bike lanes on:
 - » Mountain Rd extending past Downs Park towards Gibson Island
 - » Fort Smallwood Rd toward Fort Smallwood Park from Edwin Raynor Blvd to Fort Smallwood Park
 - » Magothy Bridge Rd from Fort Smallwood Rd to Woods Rd.

Mayo Peninsula

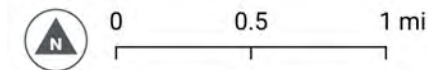
Description and Destinations:

The main corridor along the Mayo Peninsula is Route 214/Mayo Road/Central Avenue/Beverly Avenue. This corridor stretches from Edgewater to several small beach

communities, including Mayo and Beverly Beach, as well as Turkey Point Island via a connecting roadway. Beverly Triton Beach Park and Mayo Beach Park were once home to some of the East Coast’s largest beach resorts. Today, the beaches are identified as historical sites. Mostly residential areas are located on the peninsula, with



12 - Mayo
Proposed Bike Network



- Facility Types**
- Proposed**
 - Shared-use Path (green dashed line)
 - Separated Bike Lane (purple dashed line)
 - Bike Lane (red dashed line)
 - Low Stress Connection (blue dashed line)
 - Secondary Network (grey dashed line)
 - Existing**
 - Shared-use Path (green solid line)
 - Bike Lane (red solid line)
 - Areas for Future Study (light blue shaded area)
 - Key Trail Intersections (red circle with white center)



schools, additional parks, and commerce. Mayo Road/Central Avenue/Beverly Avenue (Route 214), Shoreham Beach Road, and Turkey Point Road are the main corridors through the eastern part of the peninsula. The character of the peninsula and these roadways is rural but they have many businesses, health care centers, schools, daycare centers, and recreation areas. Improved walking and rolling infrastructure would allow residents to safely and easily travel to these destinations.

Mayo, Beverly Beach, and Turkey Point Island each have internal networks of small, low-speed streets. With traffic calming, a wider shoulder along Mayo Road/Central Avenue would provide sufficient bicycling conditions. Central Avenue currently has wide, paved shoulders for a few miles east of the growing area of Edgewater, but where Shoreham Beach Road branches off it becomes narrower. The County has proposed a bicycle lane following Central Avenue, Shoreham Beach Road, and Honeysuckle Road to Mayo Beach Park. Community members have expressed a preference for a shared-use path on Mayo Road/Central Avenue all the way to Beverly Beach.

Several schools are located on this corridor: Central Elementary School, South River High School, Central Middle School, Mayo Elementary School, daycare centers, and The Summit School, which is a private school serving students with learning differences.

Proposed Improvements:

The proposed improvements include:

- The prioritization of safe crossings at intersections along the corridor.
- Construction of a shared-use path along:
 - » Central Ave East (until the intersection with Shoreham Beach Road)
 - » Shoreham Beach Road between Central Ave E and Triton Beach Road
 - » Triton Beach Road
 - » Turkey Point Road
 - » Loch Haven Road
 - » Muddy Creek Road
 - » Solomans Island Road.
- Bicycle lanes on:
 - » Central Avenue East (southward from the intersection with Shoreham Beach Road)

- » Shoreham Beach Road (northward from the intersection with Triton Beach Road)
- » Honeysuckle Road
- » Selby Blvd.

Previous Study Recommendations:

Major Intersections/Important Facilities:

For Central Ave/MD-214, the MIFF recommends:

- “8’ bicycle lanes on shoulders from MD-424 to west of MD-214/Pike Ridge Rd and 5’ bicycle lanes from MD-468 to Shoreham Beach Road; 5’ bicycle lanes on proposed curb and gutter cross section from west of Pike Ridge Road to MD-468; sidewalks on proposed cross section from west of Pike Ridge Road to MD-468.”

MD 214 Transportation Facility Planning Study:

A 2019 study was conducted to evaluate vehicular, pedestrian, and bicyclist conditions and recommendations. Related to bicycle and pedestrian infrastructure, the study recommends on Central Avenue (MD 214):

- A 10’ sidepath for pedestrians and less experienced/confident bicyclists on one side of the road, a 5’ grass buffer, and a 5’ bicycle lane on both sides of the road from Muddy Creek Rd (MD 468) to Weirs Rd
- A 10’ sidepath for pedestrians and less experienced/confident bicyclists on one side of the road, a 5’ grass buffer, and a 4’ shoulder for bicylists’ use on both sides of the road from Weirs Rd to Shoreham Beach Road
- A 4’ shoulder for bicyclists’ use on both sides of the road from Shoreham Beach Road to Mayo Avenue.

The study recommends on Shoreham Beach Road:

- A 10’ sidepath for pedestrians and less experienced/confident bicyclists on one side of the road, a 5’- 190’ grass buffer, and a 4’ shoulder for bicylists’ use on both sides of the road from Central Avenue to Triton Beach Road.

Walk & Roll Anne Arundel! recommends 10’ shoulder for bicyclists use on both sides of the road where the previous study recommends a 4’ shoulder. Between Muddy Creek Road to Weirs Rd, *Walk & Roll Anne Arundel!* also recommends a 3’ buffer between the 5’ bicycle lane and the travel lane.

To be consistent with the layout of the proposed bicycle/SUP network, this plan recommends:

- A shared-use path on Central Avenue (MD 214) between Muddy Creek Road (MD 468) and Shoreham Beach Road
- A bicycle lane on Central Avenue (MD 214) from Shoreham Beach Road to Mayo Avenue
- A 5’ bicycle lane with 3’ buffer on Shoreham Beach Road, continuing on Ramsey Drive until Ramsey Drive intersects with Honeysuckle Dr.

Crofton-Davidsonville

Description and Destinations:

The three roads identified for bicycle lanes are two-lane state routes in rural areas: Davidsonville Road, Birdsville Road, and Solomons Island Road. There is a park-and-ride

near Davidsonville Road and Route 50 with commuter bus service to Washington, D.C. Except for a dual carriageway portion at the Route 50 interchange, Davidsonville Road is a two-lane, high speed (45 mph) open section road with no shoulders for much of its length.

The County has previously recommended a signed route along this corridor between Route 450/Defense Highway and Route 214/Central Ave, which community members have expressed a desire to extend further south to Route 2. Another proposed signed route along 214 between the County line and Edgewater intersects with this corridor. Community members have also identified concerns with a parallel route following Rossback Road and Patuxent River Road, citing aggressive drivers.

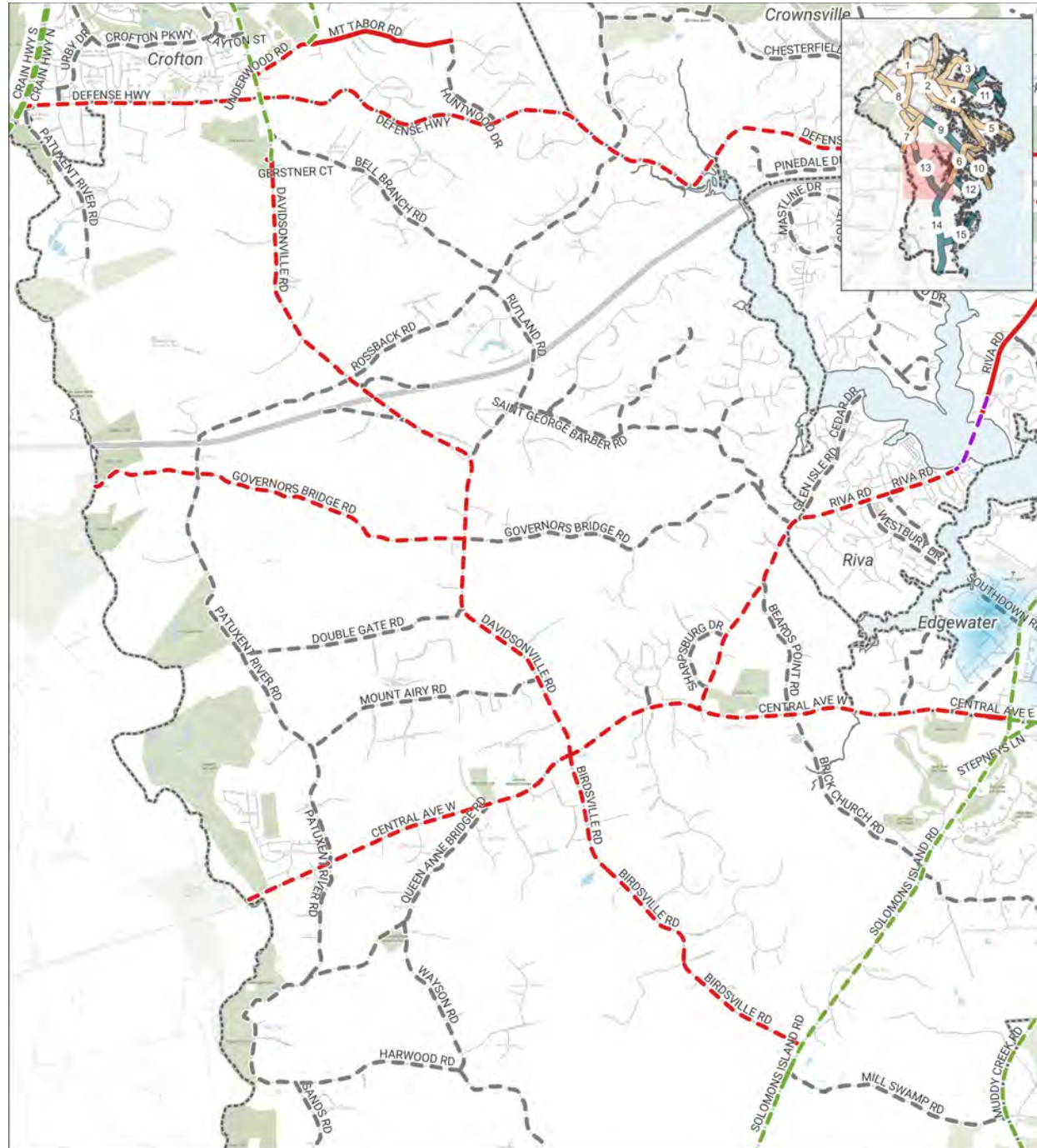
Proposed improvements:

The proposed improvements include:

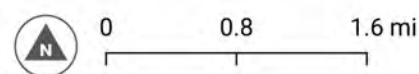
- Protected bicycle lanes on:
 - » Riva Road from north of the bridge over the South River to MD-214
- Buffered bicycle lanes on:
 - » Davidsonville Rd from south of Bell Branch Road to where road transitions to Birdsville Road
 - » Special attention should be paid to intersections – like that at MD 424 and MD 214 in Davidsonville – where a slip lane currently allows high-speed right turns
 - » Birdsville Road from Davidsonville Road to Solomons Island Road
 - » Governors Bridge Road from the western County line to Davidsonville Road.
 - » MD 214

Per the FHWA Bikeway Selection Guide, the bicycle lanes along these routes should be 10' wide with a 3' buffer and include rumble strips at the edge of the travel lane.

- Shared-use path on Davidsonville Road from Defense Highway to south of Bell Branch Road.



13 - Crofton–Davidsonville
Proposed Bike Network



Facility Types

Proposed

- Shared-use Path
- Separated Bike Lane
- Bike Lane
- Low Stress Connection
- Secondary Network

Existing

- Shared-use Path
- Bike Lane

Key Trail Intersections

- Key Trail Intersections

Areas for Future Study

- Areas for Future Study



South County

Description and Destinations:

The main roads of South County are primarily two-lane state highways that connect travelers through scenic rural areas. There are small job centers along each of the

corridors identified for improved bicycling infrastructure, such as Solomons Island Road, Owensville Road, Muddy Creek Road, Birdsville Road, Mount Zion Marlboro Road, Bayard Road, Sands Road, and more.

There is a larger job center near Maryland Route 4/ Stephanie Roper Highway and Sands Road. There are also transit stops along Route 4 in Lothian, Bristol, and Dunkirk.

South County is home to several neighborhoods with higher averages of people with low incomes and people who do not own a vehicle. Mobile home parks, for example, exist at the southwestern portion of the County, with several along Mount Zion Marlboro Road and Sands Road. Providing safe bicycling infrastructure would help people inexpensively get to South County jobs, or transit stops to connect them to the larger region. The County overall, and individual County residents and families, would economically benefit from this improved access.

There are transit stops along Southern Maryland Boulevard, as well as a significant jobs center near the intersection of Mt. Zion Marlboro Road and Southern Maryland Boulevard, which should be connected via low-stress walking and bicycling infrastructure.

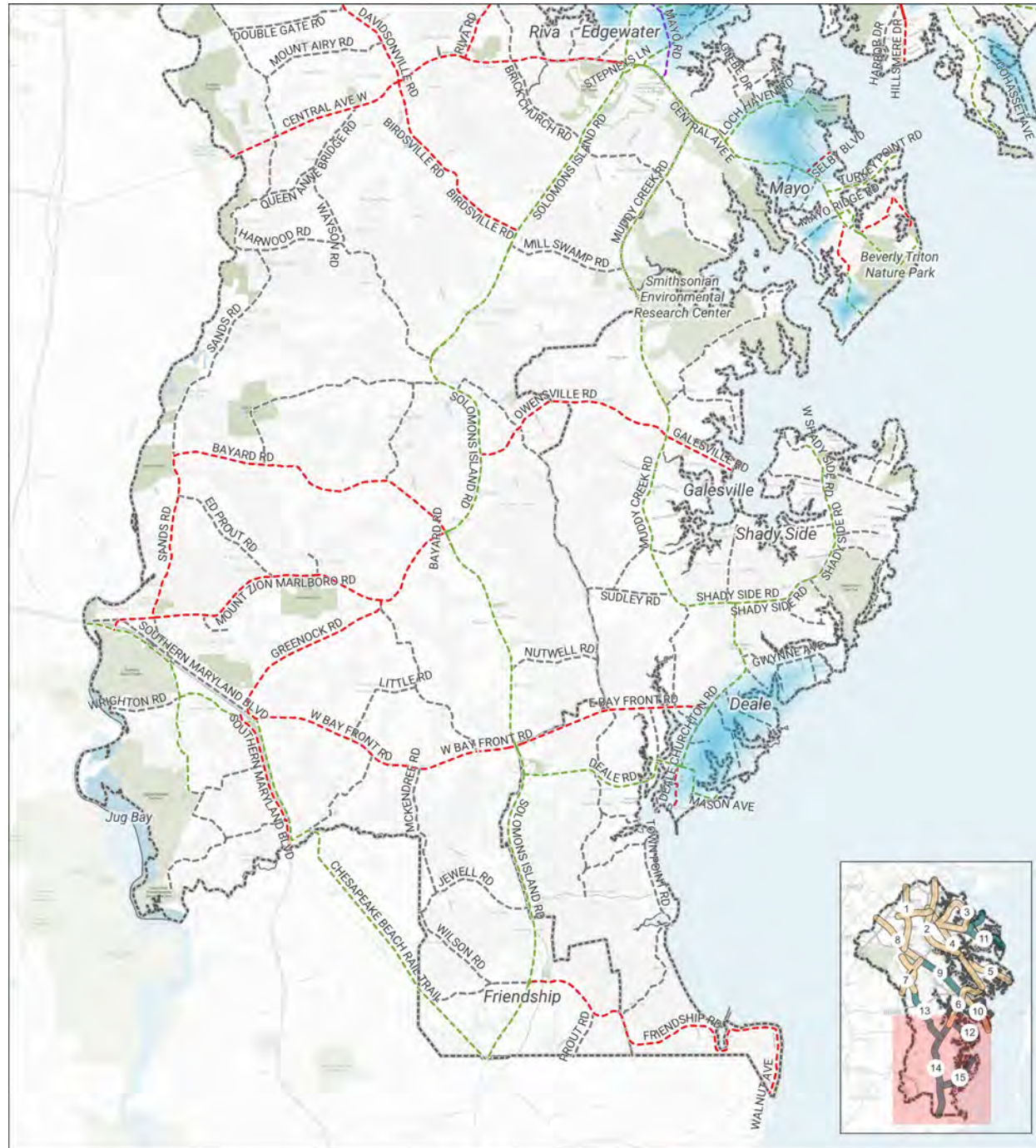
Proposed Improvements:

Completion of the bike lanes on Route 2/Solomons Island Rd from Edgewater to the Anna Arundel/Calvert County Line is crucial to the success of the bike network in South County given its role as the central spine.

Given the low traffic volumes on most roads within South County, buffered bicycle lanes are proposed for the following roads to improve connections between South County communities:

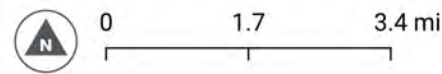
- Owensville Rd from Route 2 to Main St in Galesville
- Birdsville Rd (see Davidsonville PIA, above, for further description)
- Central Ave from Route 2 to County Line
- Bayard Rd and Sands Rd from Lothian to Waysons Corner
- Mount Zion Marlboro Rd from Lothian to Waysons Corner
- Greenock Rd from Greenock Rd to Bristol
- Wrighton Rd from Southern Maryland Blvd to Pindell Rd
- Plummer Ln from Wrighton Rd traveling northward, continuing onto Maryland 4 Service Rd, then continuing onto Mt. Zion Marlboro Rd. This area has a Park and Ride and a significant jobs center
- Pindell Rd from Wrighton Rd traveling southward to Putuxent Fishery Rd

- Southern Maryland Boulevard from the southern County line to Wrighton Road near the Lothian Park and Ride.
- MD 261/Walnut Ave from the County Line (near the Beach Rd), continuing north to transition to Lake Shore Drive (still in North Beach, MD), continuing west as the road transitions to Friendship Road in Friendship, MD, ending at Solomons Island Road/Route 2
- Fairhaven Rd to Solomons Island Rd to Friendship Rd.
- A shared-use path on:
 - » Solomons Island Rd from Central Ave to the southern County Line;. From north to south this path starts in a southwestern direction, then changes to a southeastern direction at the intersection of Old Solomons Island Rd and MD 22 in Lothian.
 - » Muddy Creek Rd from Central Ave to Deale Churchton Rd/Shady Side Rd as a parallel alternative to Route 2.
 - » MD 256 (Deale Churchton Rd) from MD 468 (Shady Side Rd) to Deale Rd/Masons Beach Rd.



14 - South County Proposed Bike Network

Facility Types	
Proposed	Areas for Future Study
Shared-use Path	Key Trail Intersections
Separated Bike Lane	Existing
Bike Lane	Shared-use Path
Low Stress Connection	Bike Lane
Secondary Network	



Shady Side

Description and Destinations:

This area is in the southeastern part of the County with two essential corridors needing multimodal improvements:

1. The u-shaped section of Route 468 that extends southward down Muddy Creek Rd, passing through Harwood, Galesville, and West River, until the road bends east into Churchton, then northbound to Shady

Side and Avalon Shores. With about 6,000 residents, Shady Side is the largest town in South County. Several businesses, restaurants, grocery stores, and community centers are on this route, with residential areas, museums, Shady Side Elementary School, marinas, parks, and beaches extending from the highway.

2. Route 256 from where the road begins at Route 2 in Tracy's Landing, extending eastward into Deale. It serves several schools, community facilities (libraries, post offices), marinas, tourist attractions, and

residential areas. Deale is a fishing town with four creeks and several marinas on the Chesapeake Bay. The population of Deale is about 4,500 people and it supports over 3,700 jobs in sectors ranging from public administration to construction to tourism. The roundabout at Route 256 and Bay Front Road is an example of the type of traffic calming that could occur in this area.

Both Deale and Shady Side are waterfront communities that are more populated than other areas of the predominantly rural South County.

Proposed Improvements:

The proposed improvements include:

- Shared-use paths on:
 - » Shady Side Rd from Muddy Creek Rd/Deale Churchton Rd to the end of the road
 - » MD 256 (Deale Churchton Rd) from MD 468 (Shady Side Rd) to Deale Rd/Masons Beach Rd
 - » Deale Rd from Deale Churchton Rd to Solomons Island Rd
 - » Masons Beach Rd from Deale Churchton Rd to Mason Ave
 - » Mason Ave from Masons Beach Rd to the end of the road.
 - » Muddy Creek Road from Central Ave E to Deale Churchton Road/Shady Side Road
- Buffered bicycle lanes on:
 - » Galesville Road and Main Street extending east from Muddy Creek Road
 - » East Bay Front Road from Solomons Island Road/Route 2 to Deale Churchton Road; this road has a wide shoulder that could easily be converted to a bicycle lane, although ridership will likely be low due to the rural setting.
- In the commercial centers, it is important to ensure safe travel for pedestrians due to high volumes of tourists.

Previous Study Recommendations:

MIFF:

For this area, MIFF recommends the following improvements related to walking and bicycling:

- Roundabout at MD-2 and MD-256
- 6' Bicycle lanes on shoulders along Deale Road/MD-256 and Shady Side Road/MD-468
- Sidewalks throughout central Deale.



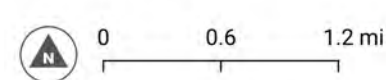
People walking on Drum Point Road in Deale

Facility	Miles of Existing Facility	Miles of Proposed Facility	Total Miles with Full Implementation
Shared-use path	63.8	231.9	295.7
Separated bicycle lanes	0.1	23.3	11.7
Bicycle lanes	36.9	123.3	160.2
Sidewalks	2046.6	28.2	2074.8

Table 1: Proposed and existing facility mileage in centerline miles



15 - Shady Side Proposed Bike Network



Facility Types

Proposed

- Shared-use Path (dashed green line)
- Separated Bike Lane (dashed purple line)
- Bike Lane (dashed red line)
- Low Stress Connection (dashed blue line)
- Secondary Network (dashed grey line)

Existing

- Shared-use Path (solid green line)
- Bike Lane (solid red line)

Other Symbols

- Areas for Future Study (light blue shaded area)
- Key Trail Intersections (red circle with a dot)



Appendix E.2: Priority Projects

Rank	Regional Plan Area	Street Name	Facility Type	Start	End	Estimated Cost
1	3	Aquahart Rd	Separated Bike Lane	Crain Hwy S, Thelma Ave	Fitzallen Rd, Ritchie Hwy	\$911,625
2	7	West St	Shared Use Path	Generals Hwy	Holly Ave	1,203,345
3	4	Forest Dr	Shared Use Path	Fairfax Rd	Null	12,234,008
4	1	Ritchie Hwy	Shared Use Path	Northern County Line	Crain Highway	18,980,762
5	3	Crain Hwy N and Crain Hwy S	Shared Use Path	Crain Hwy N & Ritchie Highway	Crain Hwy S & Veterans Hwy	14808072
5	5	Crain Highway S	Shared Use Path	Western County Line	Annapolis Rd	
6	3	5Th Ave Se	Shared Use Path	5Th Ave Sw, Crain Hwy S	Ritchie Hwy	898,066
7	3	Fitzallen Rd	Separated Bike Lane	B&A Trail	Upton Rd	\$439,745
8	7	Bestgate Rd	Shared Use Path	Generals Highway	Bestgate Rd west of Medical Parkway	1,203,345
8	7	Bestgate Rd	Shared Use Path	Admiral Dr	N Bestgate Rd	\$1,805,018
9	4	Jumpers Hole Rd	Shared Use Path	Baltimore Annapolis Blvd, Mountain Rd	Benfield Rd	4,612,823
10	1 and 3	Baltimore Annapolis Blvd	Separated Bike Lane	Nursery Rd	Mountain Rd	3619476
11	3	Quarterfield Rd	Shared Use Path	Crain Hwy S	Old Stage Rd	\$1,203,345
12	1 and 3	8Th Ave Nw	Separated Bike Lane	8Th Ave Ne, Crain Hwy N	Baltimore Annapolis Blvd/MD 648	557,025
13	3	Griffith Ln Se	Separated Bike Lane from 4th Ave SE to 3rd Ave SE; Bike Lane from 3rd Ave SE to Greenway SE	4Th Ave SE	Greenway SE	\$2,852,792
14	4	Riggs Ave/McKinsey Rd	Shared Use Path	Riggs Ave & Lennox Ave	McKinsey Rd & Leelyn Dr	1,203,345
15	7	Solomons Island Rd	Shared Use Path	Central Ave/MD 214	MD 260	37,148,719
15 (project split in 2 on this sheet due to length)	8 and 9	Solomons Island Rd	Shared Use Path	Admiral Cochrane Dr/Lake Shore Rd	Central Ave/MD 214	34,247,928
16	4	Mountain Rd	Shared Use Path	Edwin Raynor Blvd	Ritchie Highway	7,612,069
17	3	Marley Neck Blvd	Shared Use Path	Baltimore Annapolis Blvd, Marley Station Rd	Fort Smallwood	9,025,088
18	3	Reece Rd	Shared Use Path	Donaldson Ave, Telegraph Rd	Annapolis Rd	6454305
19		E Furnace Branch Rd	Shared Use Path	Crain Hwy N	Baltimore Annapolis Blvd	\$20,958,988
20	1	Belle Grove Road (Baybrook Connector)	Separated Bicycle Lane	10th Avenue	Potee/Hanover/Jack Intersection	\$963,226
21		Riva Rd	Shared Use Path	West St	Towne Centre Blvd	\$401,115
22	1	Ridge Road (MD 713)/New Ridge Road	Shared Use Path	Annapolis Road & Ridge Road	Stoney Run Road & Ridge Road	\$12,400,000
23	7	Admiral Cochrane Dr	Separated Bike Lane	Riva Rd	Lake Shore Dr, Solomons Island Rd	797478
24	3	Oakwood Rd	Separated Bike Lane	Oakwood Station Rd, Whitman Dr	Amber Beacon Way, Old Mill Blvd	\$1,629,699
25	4	Evergreen Rd & Leelyn Dr	Shared Use Path	Evergreen Rd & Benfield Rd	Leelyn Dr & Arundel Beach Rd	1403903

Rank	Regional Plan Area	Street Name	Facility Type	Start	End	Estimated Cost
26	3	Americana Cir	Shared Use Path	Ritchie Hwy	B&A Trail	
27	1	Church St	Bike Lane	Ritchie Hwy	County Line	\$439,499
28	5	Annapolis Rd	Shared Use Path	Blue Water Blvd	Odenton Rd	\$6,381,375
28	5	Annapolis Rd	Shared Use Path	Telegraph Rd	Crain Hwy S	
28	2	Annapolis Rd	Shared Use Path	Baltimore Washington Pkwy	South of Rae Rd	\$911,625
29	4	Robinson Rd	Shared Use Path	Leelyn Dr, Ritchie Hwy	Benfield Rd, Evergreen Rd	\$1,203,345
30	5	Telegraph Rd	Shared Use Path	Old Mill Rd	Annapolis Rd/MD 175	8159044
31	3	Marley Station Mall Trail	Shared Use Path	Marley Station Rd, Ritchie Hwy	B&A Trail	
32	3	E Ordnance Rd	Bike Lane	Ritchie Hwy, W Ordnance Rd	City Line	\$571,219
33	3	Crain Hwy S	Separated Bike Lane	5Th Ave Se, 5Th Ave Sw	Quarterfield Rd	\$283,804
34	5	Waugh Chapel Rd	Shared Use Path	Crain Highway S	Maytime Dr	2807805
35	3	Stewart Ave	Bike Lane	4Th Ave Sw	Old Stage Rd	\$223,282
36	3	Thelma Ave	Separated Bike Lane	Elizabeth Ln	Crain Hwy S/Aquahart Ln	\$898,066
37	7	Old Solomons Island Rd	Separated Bike Lane	Solomons Island Road	West St	\$566,899
38	7	Harry S Truman Pkwy	Bike Lane	Southaven Rd	Riva Rd	\$666,051
39	7	Housley Rd	Shared Use Path	Annapolis Ridge Rd including the traffic circle	Generals Hwy with gaps in between	\$1,002,788
40	7	Bay Ridge Rd	Shared Use Path	Bay Ridge Ave, Forest Dr, Hillsmere Dr	Catrina Ln, Farragut Rd	\$3,418,594
41	9	Mayo Rd	Separated Bike Lane	Solomons Island Road	Central Ave E	\$534,856
42	4	Earleigh Heights Rd W	Shared Use Path	Magothy Bridge Rd, Ritchie Hwy	Earleigh Heights Connector	\$911,625
43	1 and 3	W B And A Rd	Shared Use Path	BWI Trail	George Ave	\$8,423,415
44	4	Pasadena Rd	Shared Use Path	B&A Trail on Dorsey Rd	Baltimore Annapolis Blvd	\$2,342,876
45	6	Generals Hwy	Shared Use Path	Belvoir Farms Rd	Carriage Hill Pkwy	\$2,607,248
46	5 and 8	Davidsonville Rd	Shared Use Path	Conway Rd, Crain Hwy N, Crain Hwy S	Barbara Swann Way	\$5,734,121
47	5, 6, and 8	Defense Hwy	Bike Lane	Crain Hwy	N River Road	\$2,057,473
48	4	College Pkwy	Shared Use Path	Ritchie Hwy	Anne Arundel Community College Rd	\$920,741
49	3	3Rd Ave Sw	Separated Bike Lane	3Rd Ave Se, Crain Hwy S	Newfield Rd, Willow Ln Sw	\$754,806
50	4	Benfield Rd	Shared Use Path	Evergreen Rd, Robinson Rd	Benfield Blvd, Laurel Dr	\$5,615,610

Appendix F: Cost Estimates

COST ESTIMATES

To provide a per-mile cost estimate for each project evaluated for the Plan, the project team used MDOT's "Planning Level Cost Estimating Tool for Bicycle Infrastructure Projects."1 The "Planning Level Cost Estimating Tool" is based on FY19 construction costs. Construction costs should be escalated based on more

recent construction bids. At this level of project planning, the tool requires inputs of project length, number of intersections, average width of intersections, and for shared-use paths, the type of terrain. Cost estimates were generated for each project using this tool. Cost estimates do not include projected expenses to move utilities or acquire right-of-way.

Table 2: Cost Estimates (based on FY2019 construction costs).

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
NORTHWEST COUNTY							
Benfield Rd	Veterans Hwy	Robinson Rd	SUP	\$4,612,823	Sufficient space for construction; connects to existing facilities (sidewalks on Jumpers Hole Road)	4	0
Milestone Pkwy	Annapolis Rd	Clark Rd	SUP	Funded	Programmed for funding	2	3
Jumpers Hole Rd	Benfield Rd	MD 2 (Ritchie Hwy)	SUP	\$6,818,955	Part of this route is funded in the CIP; connects to existing facility (B&A Trail, bicycle lane on East West Blvd); connects to job centers; sufficient space although there may be ROW and utility constraints	4,3	3
Quarterfield Rd	WB&A Rd	Donaldson Ave	SUP	\$4,968,356	Sufficient space; connects to a school and job centers	3	3
Donaldson Ave	Telegraph Rd	Quarterfield Rd	SUP	\$3,409,478	Sufficient space; connects to existing SUPs on Annapolis Rd	3	2
Clark Rd	Milestone Pkwy	Watts Ave	SUP	\$1,800,000	Sufficient space and connects to job centers; however, will have ROW, utility and environmental issues; incorporate into CIP-funded project	2	3
Old Mill Road, Burns Crossing Road, and Sappington	Old Mill Rd & WB&A Rd	Sappington Station Rd & Annapolis Rd	SUP	\$5,925,563	Sufficient space and connects to job centers; however, there will be ROW, utility and environmental issues	3	0
Najoles Rd/Dicus Mill Rd	Benfield Blvd & Najoles Rd	Dicus Mill Rd & Burns Crossing Rd	SUP	\$9,116,250	Connects to park and jobs	3	2
WB&A Rd	George Ave	MD 176 (Dorsey Rd)	SUP	\$9,936,713	Sufficient space, connects to job centers and preliminary study complete; however, will have ROW, utility and environmental issues	1,3	2
MD 713 (Ridge Rd) and New Ridge Rd	Annapolis Rd & Ridge Rd	Stoney Run Rd & Ridge Rd	SUP	\$12,400,000	Sufficient space and connects to job centers; however, will have ROW, utility and environmental issues	1,2	4,3
Benfield Blvd/Robinson Rd	Jumpers Hole Rd	B&A Trail	SUP	\$3,810,593	Existing sidewalks; ROW constraints; connects to a high school	4	0
Veterans Hwy	Crain Hwy	MD 178	SUP	\$20,126,128	Connects to jobs, services; underpass constraints; likely ROW acquisition and utility relocation needs	3	3
Hanover Road Connection	Stoney Run Rd	Hanover Rd (end)	SUP	\$13,003,054	Project design funded from Ridge Rd to Stoney Run	1	2
BWI Trail to Arundel Mills Shopping Center	From Dorsey Rd to Ridge Rd over MD 100		SUP	\$30,901,000	In existing County plans	1,2	2,3,4
BWI Trail to Arundel Mills Shopping Center	From Dorsey Rd to Wright Rd over MD 100		SUP	30,676,000	In existing County plans	1	2
NORTH COUNTY (excluding Glen Burnie Town Center)							
Belle Grove Rd (Baybrook Connector)	Gibbons Ave	Potee St /S Hanover St / Jack St	SUP	\$963,226	Already studied, moving to design	1	4,3

1 https://mdot.maryland.gov/OPCP/MDOT_BPAG_Bikeways_Project_Cost_Estimator.xlsx

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Nursery Rd	N Hammonds Ferry Rd	Baltimore Annapolis Rd	Bike Lane	\$478,160	CIP has funded sidewalks on Nursery Rd from Hammonds Ferry Road to Nursery Rd Light Rail Station	1	2,4
E Ordnance Rd	MD 2 (Ritchie Hwy)	City Line	Bike Lane	\$571,219	Sufficient space and connects to job centers; however, will have ROW, utility and environmental issues	1,3	2,3
Quarterfield Rd	Donaldson Ave	Crain Highway S	SUP	\$3,610,035	Connects to jobs, transit	3	2,3
New Cut Rd	Veterans Hwy	Gambrills Rd	SUP	\$4,202,591	CIP has funded a sidewalk on New Cut Rd from Stevenson Rd to Green Branch Lane	3	2,3,5
Old Mill Rd/Blvd	Veterans Hwy	Oakwood Rd	Cycle Track	\$670,001	Sufficient space; connects to jobs	3	3
Oakwood Rd	Oakwood Station Rd	Old Mill Blvd	Cycle Track	\$1,253,029	Sufficient space with lane reductions/median adjustments; connects to jobs and schools	3	3,5
East West Blvd	Veterans Hwy	B&A Trail	Cycle Track	\$2,136,446	Sufficient space; there will be environmental/slope challenges on sections; connects to B&A Trail	4	0
Benfield Rd/Robinson Rd	Veterans Hwy	MD 2 (Ritchie Hwy)	SUP	\$8,824,530	CIP has funded Benfield Rd corridor improvements from I-97 to MD 2	4	0
W Benfield Rd	W Benfield Rd & Veterans Hwy	W Benfield Rd & Najoles Rd	SUP	\$16,373,514	Connects to jobs and park; costly bridge work	3	0
Earleigh Heights Connector	Jumpers Hole Rd	WB&A Trail	SUP	\$1,203,345	Connects to B&A Trail and Magothy Bridge Rd bike lanes; sufficient space	4	0
Baltimore Annapolis Blvd	Mountain Rd	Crain Hwy N	Separated Bike Lane	\$2,860,906	Connects to existing facility (B&A Trail); relatively sufficient space with some challenging areas due to need to remove turn lanes, move utilities, etc.)	3	2,6,3,5
Baltimore Annapolis Blvd	Broadview Blvd	County Line	Bike Lane	\$758,570	Sufficient space; connects to a school	1	2
Stevenson Rd	Quarterfield Rd	New Cut Rd	Separated Bike Lane	\$913,740	Connects to jobs; proximate to schools; may need ROW acquisition		
SUP along Cabin Branch	BWI Trail	MD 2 (Ritchie Hwy)	SUP	\$9,572,063	Connects to jobs, parks, trail, school	1,3	2,5
MD 2 (Ritchie Hwy)	Northern County Line	Crain Hwy & Holsom Way	SUP	\$18,980,762	Existing sidewalks in segments; connects to jobs and transit; sufficient space in segments; ROW, utility and environmental issues; bridge at I-695 and underpass at I-695 ramps will add significant cost and should be phased appropriately	1,3	3,4,5
Church St	MD 2 (Ritchie Hwy)	City Line	Bike Lane	\$439,499	On-street parking would need to be removed with few parking alternatives nearby	1	3,5
MD 3 (Crain Hwy)	Holsom Way/MD 2 (Ritchie Hwy)	Veterans Hwy	SUP	\$14,808,072	Sufficient space in many segments with ROW and utility relocation issues; some segments will have very constrained ROW issues and will need to be designed for rerouting and appropriately phased; some segments have sidewalks; underpass will cause constraints	3	2,4,5
N Broadview Blvd/Baltimore Annapolis Blvd	Andover Rd	Crain Hwy	Bike Lane	\$1,152,249	Constrained due to railroad tracks, underpasses, utilities, ROW; lane reduction could expedite design and construction; connects to jobs and transit	1,3	2,4,5
Marley Station Rd	Baltimore Annapolis Blvd	B&A Trail	SUP	\$7,396,561	Sidewalks exist; bridge will significantly add to cost; a safer pedestrian and bicyclist crossing at Marley Station Rd & MD 2 (Ritchie Hwy) should be prioritized	3	2,5,6
SUP between House of Tropicals and Cromwell Park Dr	Cromwell Park Dr	House of Tropicals	SUP	\$1,367,438	Connects to jobs, transit	1	2

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
MARLEY NECK LOOP							
Marley Station Rd/ Marley Neck Blvd	MD 2 (Ritchie Hwy)	Fort Smallwood Rd	SUP	\$9,025,088	Some SUPs and sidewalks already exist however there is sufficient space for much of the corridor to provide needed upgrades	3	2,3,5,6
Fort Smallwood Rd	Marley Neck Blvd	Bar Harbor Rd	SUP	\$27,580,667	Sufficient space, however will have ROW, environmental and potentially utility issues; connects to relatively few destinations	1,4	2
Fort Smallwood Rd/ Edwin Raynor Blvd	Appian Way	Maro Rd	SUP	\$5,214,495	Study complete	4	0
Mountain Rd	MD 2 (Ritchie Hwy)	Pinehurst Rd	SUP	\$7,612,069	Bicycle lanes already studied; study would need to be revised for SUP; ROW, utility constraints; underpass constraints warrant appropriate phasing	3,4	2,5,6
Baltimore Annapolis Rd	Glen Rd	Mountain Rd	Separated Bike Lane	\$2,285,399	Sufficient space, however will have ROW, environmental and potential utility issues; connects to jobs and schools	3	2,3,6
Solley Rd	Willow Tree Rd	Mountain Rd	SUP	\$2,807,805	Study complete	3,4	2
Fort Smallwood Rd	County Line	Marley Neck Blvd	Bike Lane	\$144,664	Sufficient space, however will require ROW acquisition and utility relocation unless there are lane reconfigurations; connects to some jobs	1	0
Jumpers Hole Rd	Mountain Rd	W Pasadena Rd	SUP	\$3,810,593	Sidewalks exist for much of the corridor, although they need to be upgraded; upper portion of corridor connects to jobs; parts of the corridor directly connect to few destinations; sufficient space in parts of the corridor	3,4	0
Waterford Rd	Mountain Rd	Catherine Ave	Bike Lane	\$486,768	Significant ROW constraints	4	0
Marley Neck Rd	Marley Neck Blvd	Baltimore Annapolis Rd	Bike Lane	\$460,465	Parallel facility proposed on Marley Neck Blvd; ROW, utility and environmental issues	3	2,6
B&A CONNECTIONS							
Baltimore Annapolis Blvd & MD 176 (Dorsey Rd)	B&A Trail on MD 176 (Dorsey Rd)	Glen Burnie Cromwell Light Rail	SUP	\$911,625	Connects to job centers, transit stops, and B&A Trail; sufficient space, however there will be ROW, utility and environmental issues	1	2,4
Jumpers Hole Rd	Obrecht Rd	Mountain Rd	SUP	\$8,308,186	Connects to B&A Trail, transit stops, and jobs; sufficient space, with some (relatively few) utility issues, some environmental issues, likely ROW issues	3,4	0
Pasadena Rd	B&A Trail on MD 176 (Dorsey Rd)	Baltimore Annapolis Blvd	SUP	\$2,342,876	Connects to B&A Trail, transit stops, and jobs; sufficient space, with some (relatively few) utility issues, some environmental issues, likely ROW issues	4	0
Whites Rd	Glenns Rd	Baltimore Annapolis Blvd	Bike Lane	\$476,432	Connects to B&A Trail; sufficient space, however there will be ROW and environmental issues, and structural challenges at the bridge	4	0
Cypress Creek Rd	MD 648	Creek Rd	Bike Lane	\$990,230	Connects to B&A Trail, transit, jobs; sufficient space with likely ROW acquisition	4	0
College Pkwy	B&A Trail	Peninsula Farm Rd	SUP	\$2,279,063	Sufficient space, however there will be ROW, environmental and utility challenges	4	0
Arnold Rd & Severn Way	B&A Trail & Severn Way	Arnold Rd & Baltimore Annapolis Blvd	SUP	\$401,115	Sufficient space, however there will be ROW, perhaps utility and some environmental issues	4	0
Earleigh Heights Rd	Kinder Rd & Wembly Way	W Earleigh Heights Rd & MD 2 (Ritchie Hwy)	SUP	\$2,206,133	Connects to B&A Trail, jobs	4	0
Americana Cir	Ritchie Hwy	B&A Trail	SUP	\$601,673	Connects to B&A Trail, transit, jobs	3	5

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Marley Station Mall Trail	shared-use Path	Marley Station Rd, Ritchie Hwy	SUP	\$802,230	Connects to B&A Trail, transit, jobs	3	5
MD 648 (Evergreen Rd)	Evergreen Rd & Benfield Rd	MD 2 & Cyprus Creek	SUP	\$1,604,460	There is a sidewalk on one side of the road (both sides of the road in some locations); there will be ROW and utility challenges; connects to a school and B&A Trail	4	0
Riggs Ave & McKinsey Rd	Riggs Ave & Lennox Ave	McKinsey Rd & Leelyn Dr	SUP	\$1,203,345	Sufficient space, however will be ROW and utility issues	4	0
Magothy Bridge Rd	B&A Trail	Woods Rd	Bike Lane	\$1,009,807	Whites Rd is already a low stress connection; will be ROW and utility issues	4	0
MD 2 (Ritchie Hwy)	Near Boulders Way	Naval Academy Bridge	SUP or Separated Bike Lane	\$5,469,750	Connects jobs, park, trail; ROW, utility and environmental constraints	4	0
ARNOLD-BROADNECK PENINSULA							
Shore Acres Rd	College Pkwy	Bay Green Dr	Cycle Track	\$517,045	Sufficient space, however there will be ROW, utility, underpass and environmental issues	4	0
Mago Vista Rd	Jones Station Rd	River Rd	Bike Lane	\$337,785	Sufficient space, however there will be ROW and potentially utility issues; connects to a school	4	0
Broadneck Rd	Bay Dale Dr		SUP	\$6,837,188	Sufficient space; there will be environmental issues	4	0
Cape St. Claire Rd	College Pkwy	River Bay Rd	SUP	\$3,008,363	Sufficient space; there will be environmental issues, potential utility issues; a portion of the route has sidewalks	4	0
Whitehall Rd (named St. Margarets Rd at the intersection with MD 179)/Interstate 50/Skidmore Dr/Old Ferry Slip Road/Oceanic Dr	MD 179	Oceanic Dr near Shoreline	SUP	\$15,041,813	Sufficient space; there will be environmental issues, potential utility issues; h portion of the route has sidewalks	4	0
S Beach Rd	E College Pkwy	Sandy Point Playground	SUP	\$1,604,460	Sufficient space	4	0
Shore Acres Rd	Magothy Ave	College Pkwy	Bike Lane	\$402,303	Sufficient space	4	0
Bay Green Dr	Shore Acres Rd	College Pkwy	SUP	\$3,208,920	Sufficient space; there will be environmental issues	4	0
Bellerive Dr	College Pkwy	College Pkwy	Cycle Track	\$517,037	Sufficient space; connects to jobs and an existing facility	4	0
Jones Station Rd	College Pkwy	College Pkwy	Cycle Track	\$414,845	Sufficient space; connects to jobs and an existing facility	4	0
Mago Vista Rd	Beach Rd	Jones Station Rd	Bike Lane	\$441,041	Sufficient space; connects to a school	4	0
Peninsula Farm Rd/ Baybourne Dr	College Pkwy	Mill Harbor Dr	SUP (improved)	\$1,604,460	Sufficient space; connects to a school	4	0
Broadneck Trail					Phases 1A, 2, 4 complete; phase 3 just started construction; phase 1B/5 to be bid in winter 2022/2023	4	0
Pleasant Plains Rd	St Margarets Rd	Whitehall Beach Rd	Bike Lane	\$538,451	Sufficient space; lack of connection to essential destinations	4	0
Church Rd	MD 2 (Ritchie Hwy)	Jones Station Rd	Bike Lane	\$364,536	Already a sidewalk, although it needs upgrades; connects to a school	4	0
Jones Station/ Kimwood Rd	Jones Station Rd & Church Rd	End of Kimwood Rd	SUP	\$1,805,018	Segments have sufficient space, however there will be ROW and environmental issues; lack of connection to essential destinations	4	0
Arundel Beach Rd	End of Arundel Beach Rd	Sunset Dr	SUP	\$3,008,363	Significant ROW constraints; connects to a school, sidewalks and existing bicycle lanes on Arundel Beach Rd; incorporate into CIP-funded project	4	0

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Ferguson Rd/Old Mill Bottom Rd S	Int. of Old Mill Bottom Rd & Bay Dale Dr	Int. of Old Mill Bottom Rd and St. Margarets Rd	SUP	\$4,102,313	Connects to jobs and existing facilities; challenging ROW	4	0
Green Holly Dr	Rolling View Dr	College Pkwy	Bike Lane	\$352,472	Significant ROW constraints; sidewalk exists	4	0
Rolling View Dr	Mariners Circle	Green Holly Dr	Bike Lane	\$103,411	Significant ROW constraints	4	0
Chestnut Tree Dr	Green Holly Dr	Hilltop Dr	Bike Lane	\$345,021	Significant ROW constraints; sidewalk exists	4	0
Blue Ridge Dr	Chestnut Tree Dr	Hilltop Dr	Bike Lane	\$247,041	Significant ROW constraints; sidewalk exists	4	0
Summit Dr	St. Margarets Dr	Blue Ridge Dr	Bike Lane	\$227,941	Significant ROW constraints	4	0
Hampton Rd	River Bay Rd	Blue Ridge Dr	Bike Lane	\$455,420	Significant ROW constraints; sidewalk exists	4	0
Hilltop Dr	Mountain Top Dr	Cape St Claire Rd	Bike Lane	\$247,041	Significant ROW constraints; sidewalk exists between Cape St Claire Rd & Blue Ridge Dr	4	0
Mountain Top Dr	Hampton Rd	Cape St Claire Rd	Bike Lane	\$187,259	Significant ROW constraints	4	0
Lake Claire Dr	River Bay Rd	Latrobe Dr	Bike Lane	\$328,405	Significant ROW constraints	4	0
Crestview Dr	Lake Claire Dr	St. Margarets Dr	Bike Lane	\$184,775	Significant ROW constraints	4	0
St. Margarets Dr	Broadview Dr	Southview Dr	Bike Lane	\$431,353	Significant ROW constraints	4	0
Southview Dr	Round Top Dr	Chestnut Tree Dr	Bike Lane	\$249,524	Significant ROW constraints	4	0
St. Margarets Rd & MD 179	E College Pkwy	Hwy 450	SUP	\$7,293,000	Sufficient space; there will be environmental issues	4	0
PAROLE-ANNAPOLIS-EDGEWATER - Excluding Parole Town Center							
Old Solomons Island Rd	West St	Solomons Island Rd	Separated Bike Lane	\$779,397	Connects to jobs and transit; ROW acquisition and utility relocation needed	7	2
Solomons Island Rd	Forest Dr	Central Ave	SUP	\$33,974,441	Connects to jobs, services; ROW acquisition and utility relocation needed; bridge portion will be expensive	7	2
Ridgely Ave	N Bestgate Rd	Bridge	SUP	\$1,595,344	Connects to businesses; ROW acquisition and utility relocation needed; maintain separated bike lane over bridge to reduce cost	7	2
Forest Dr	Solomons Island Rd	Bay Ridge Rd	SUP	\$3,008,363	Connects to jobs, transit, park; utility relocation, environmental issues, and ROW acquisition needed	7	3,4,5
Hillsmere Dr	Bay Ridge Rd	W Bay View Dr	Bike Lane	\$669,831	Connects to jobs, transit, park; utility relocation and ROW acquisition needed in segments	7	0
Bay Ridge Rd	Bay Ridge Ave, Forest Dr, Hillsmere Dr	Catrina Ln, Farragut Rd	SUP	\$3,418,594	Connects to jobs, park; utility relocation and ROW acquisition needed	7	3
Arundel on the Bay Rd/Thomas Point Rd	Bay Ridge Rd & Arundel on the Bay Rd	Thomas Point Park	SUP	\$6,217,283	Connects to jobs, schools, park; utility relocation and ROW acquisition needed	7	0
Mayo Rd	Solomons Island Rd	Central Ave E	Separated Bike Lane	\$1,121,950	Connects to jobs; ROW and utility constraints	8,9	2
Loch Haven Dr	Central Ave E	Scrivners Rd	SUP	\$3,190,688	Sufficient space; utility relocation needed	9	0
Srivners Rd	Loch Haven Rd	South River Farms Park	SUP	\$1,805,018	Connects to jobs, parks; significant ROW and utility constraints	9	0
Stepneys Ln	Solomons Island Rd	Mayo Rd	SUP	\$2,607,249	Connects to jobs, services, school; sufficient space with some utility relocation and likely ROW acquisition needs	7	2,3

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Elvaton Rd	Oakwood Rd	Jumpers Hole Rd	Bike Lane	\$1,344,321	Connects to jobs, health care; sufficient space	3	3
Riva Rd	Southwater Point Dr	Evans Rd	Separated Bike Lane	\$16,774,629	Connects to jobs and services; bridge work will be costly	7	2
Central Ave E	Mayo Rd	Rolling Rd	SUP	\$6,153,469	Connects to jobs; sufficient space with some ROW and utility constraints	9	2
CROFTON CONNECTIONS							
Waugh Chapel Rd	Crain Hwy S	Maytime Dr	SUP	\$2,807,805	Section from Maytime to MD 3 is in design; west of Maytime to Piney Orchard there has only been a study	5	2
Riedel Rd	Crain Hwy	Davidsonville Rd	Bike Lane	\$1,818,470	Sufficient space, will need to remove on-street parking	5	0
Evergreen Rd/Johns Hopkins Rd	Waugh Chapel Way	Riedel Rd	Separated Bike Lane	\$576,148	Road diet would provide space for bike lanes	5	0
Generals Hwy	Belvoir Farms Rd	Carriage Hill Pkwy	SUP	\$2,607,248	Connects to park and jobs; slope issues	6	2,3
Annapolis Rd	Dairy Farm Rd	Crain Hwy	SUP	\$2,607,248	Sufficient space, however there will be ROW and utility issues; connects to jobs	5	2
Connection West of Crain Hwy	South Shore Trail	Annapolis Rd	Trail	\$601,673	Connects to South Shore Trail	5,6	2
Conway Rd/Davidsonville Rd	Patuxent Rd	Barbara Swann Way	SUP	\$8,140,811	Sufficient space for most segments; will have ROW, utility and environmental issues	5,8	0
Crain Hwy	North of Millersville Rd	Western County Line	SUP	\$34,782,505	Segments in Crofton boundaries connect to essential destinations; segments outside of Crofton lack direct connection to essential destinations	5	2
Dairy Farm Rd	Waugh Chapel Rd	Annapolis Rd	Bike Lane	\$1,209,238	Lack of direct connection to destinations	5	2
S Haven Rd	Defense Hwy	Pinewood Rd	Bike Lane	\$941,509	Lack of direct connection to destinations	6,7	3
Millersville Rd	Crain Hwy	South Shore Trail	SUP	\$401,115	Connects to jobs, trail; utility and ROW constraints	6	2
Crownsville Rd	Generals Hwy	Marbury Dr	SUP	\$200,558	Connects to jobs; sufficient space	6	3
JESSUP-GAMBRILLS-ODENTON (excluding Odenton Town Center)							
MD 175 (Annapolis Rd)	Milestone Pkwy	South of Race Rd	SUP	\$911,625	Connects to jobs; sufficient space however will be utility relocation needs	2	3
Race Rd	Jessup Rd	Dorsey Rd	Bike Lane	\$2,003,477	Sufficient space, is proximate to job centers, and preliminary study complete; however, will have ROW, utility and environmental issues	2	3
Race Road Realignment	Jessup Rd & Chestnut Ave	Race Rd & Existing)	Bike Lane	\$153,581	In existing County plans	2	2,3
Jessup Rd	Western County Line	Milestone Pkwy	Bike Lane	\$1,256,878	Sufficient space, is proximate to job centers, and preliminary study complete; however, will have ROW, utility and environmental issues	2	3
Clark Rd	Milestone Pkwy	Watts Ave	SUP	\$3,008,363	Sufficient space and connects to job centers; however, will have ROW, utility and environmental issues; address with CIP-funded project	2	3
Telegraph Rd	Dorsey Rd	Paul T Pritcher Memorial Hwy	Bike Lane	\$1,899,218	CIP has funded bicycle lanes and sidewalks as part of widening project between MD 100 and MD 174; sufficient space and connects to job centers, however, will have ROW, utility and environmental issues	1,3	2
WB&A Rd	BWI Trail	Old Mill Rd	SUP	\$8,423,415	Connects to jobs, parks, trail; ROW acquisition and utility relocation needs	1,3	2

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Waugh Chapel Rd	Crain Hwy N	WB&A Trail	SUP	Cost estimated in design report	Sufficient space; connects to existing facilities; section from Maytime to MD 3 is in design; west of Maytime to Piney Orchard there has only been a study	5	2
Reece Rd	Annapolis Rd	Telegraph Rd/Donaldson Ave	SUP	\$6,454,305	Connects to relatively few destinations; there is a school but residential areas are far	2,3	2,3,5
MD 175 (Annapolis Rd)	Sellner Rd	Milestone Pkwy	SUP	\$6,193,216	Connects to jobs but likely limited ridership; bridge will significantly add to cost	2	3
Ridge Rd	Jessup Rd	Dorsey Rd	SUP	\$6,217,283	The southern segment has a lack of connection to destinations; the northern segment connects to jobs	1	2
Ridge Road	Ridge Road and New Ridge Road	Ridge Road and Corporate Center Dr	Bike Lane	\$612,719	In existing County plans	1	2
MD 198 (Laurel Fort Meade Rd)	Western County Line	Mapes Rd	SUP	\$11,167,406	Connects to jobs; some utility relocation, ROW and environmental issues	2	2,3
National Business Pkwy	Technology Dr	North of Sentinel Way, at existing SUP on National Business Pkwy	SUP	\$1,604,460	Connects to jobs, transit; sidewalks exist; utility relocation needs	2	3
SOUTH SHORE TRAIL							
South Shore Trail				\$31,465,000	CIP has funded portions of project	6,7	0
ANNAPOLIS NECK							
Hilltop Ln	Forest Dr	Spa Rd	SUP	\$401,115	Sufficient space with roadway lane configuration; access to jobs	7	3,4
Forest Hills Ave	Forest Dr	Forest Dr	Bike Lane	\$295,961	Sufficient space with removal of on-street parking	7	3,4
Hillsmere Dr	Forest Dr	Hickory Ln	Bike Lane	\$186,904	Sufficient space on one side with removal of trees	7	0
Thomas Point Rd	Arundel on the Bay Rd	Thomas Point Park	SUP	\$3,008,363	Sufficient space; access to Thomas Point Park	7	0
Arundel on the Bay Rd	Forest Dr	Cohasset Ave	SUP	\$4,011,150	Sidewalks funded in CIP near Bay Ridge Rd; sidewalks exist on many segments; limited ROW available	7	3
Bay Ridge Rd	Forest Dr	Farragut Rd	SUP	\$3,418,594	Limited connection to destinations; ROW, utility and environmental issues	7	3,4
Silopanna Rd/Pump House Rd/Primrose Rd	Silopanna Rd & Spa Dr	Primrose Rd & Hilltop Ln	SUP	\$1,805,018	Park has relatively low AADT; Primrose Rd has existing sidewalks	7	4
PASADENA							
Mountain Rd	Edwin Raynor Blvd	Downs Park	SUP	\$11,431,778	Sufficient space; connects to jobs and schools; however, will have ROW, utility and environmental issues; sidewalk exists in segments	4	0
Mountain Rd	Pinehurst Rd	Gibson Island Beach	Bike Lane	\$689,656	Sufficient space; lack of essential destination connections	4	0
Magothy Bridge Rd	Mountain Rd	Woods Road	Bike Lane	\$306,915	Sufficient space; lack of direct essential destination connections	4	0
Edwin Raynor Blvd	Mountain Rd	Magothy Bridge Rd	Bike Lane	\$421,726	Sufficient space; lack of direct essential destination connections	4	0
Fort Smallwood Rd	Edwin Raynor Blvd	Fort Smallwood Park	Bike Lane	\$1,417,263	ROW and environmental challenges; lack of essential destination connections	4	3
MAYO PENINSULA							
Loch Haven Rd	Central Ave E	End of road	SUP	\$3,409,478	Phase 1: Sufficient space if ROW is acquired; connects to jobs	9	0

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Mayo Rd	Central Ave E	Solomons Island Rd	Separated Bike Lane	\$534,856	Phase 1: Sufficient space if ROW is acquired; connects to jobs	8,9	2
Muddy Creek Rd	Central Ave E	Old Muddy Creek Rd	Bike Lane	\$845,751	Sufficient space; lack of direct essential destination connections	8,9	2
Selby Blvd	Central Ave E	1st Ave	Bike Lane	\$216,942	Sufficient space if ROW is acquired	9	0
Turkey Point Rd	Central Ave E	End of road	SUP	\$13,188,661	Sections have very constrained ROW; 1 water crossing significantly adds to cost; connects to jobs	9	0
Shoreham Beach Rd	Triton Beach Rd	Honeysuckle Dr	Bike Lane	\$291,749	Sufficient space if ROW is acquired; connects to jobs/tourist destinations	9	0
Honeysuckle Dr	Shoreham Beach Rd	Mayo Beach Park	Bike Lane	\$95,959	Sufficient space and connections to tourist destination; should be completed after Shoreham Beach Rd connection	9	0
Triton Beach Rd	Mayo Rd	Carvel Rd	SUP	\$2,005,575	Sufficient space and connection to tourist destination; however, it should be completed after Shoreham Beach Road connection	9	0
Central Ave E	Solomons Island Rd	Mayo Ave	SUP	\$38,350,241	Sufficient space; however, will have ROW, utility and environmental issues; 2 stream crossings significantly add to cost	9	0
Carvel Ln	Triton Beach Rd	End of road	SUP	\$802,230	Very constrained ROW; lack of essential destination connections	9	0
CROFTON-DAVIDSONVILLE							
Central Ave W	Solomons Island Rd	County Line	Bike Lane	\$2,257,289	Sufficient space; connects to jobs and schools; however, will be ROW, utility and environmental issues	8,9	2
Davidsonville Rd/Birdsville Rd	Solomons Island Rd	Defense Hwy	Bike Lane	\$3,153,211	Few connections to essential destinations; relatively frequent environmental/slope challenges	8	0
Defense Hwy	Crain Hwy N	N River Rd	Bike Lane	\$2,057,473	Few connections to essential destinations; relatively frequent environmental/slope challenges	5,6,7,8	3
Governor Bridge Rd	County Line	Davidsonville Rd	Bike Lane	\$888,423	Few connections to essential destinations	8	0
SOUTH COUNTY							
Plummer Ln/MD Service Rd	Pindell Rd	Mt Zion Marlboro Rd & Sands Rd	SUP	\$5,469,750	Connects to jobs; sufficient space; environmental/slope challenges	8	2
Chesapeake Beach Rail Trail			SUP		County priority	8	2
Bayard Rd	Sands Rd	Mt Zion Marlboro Rd	Bicycle Lane	\$495,848	Connects to jobs and transit	8	0
Greenock Rd	Mt. Zion Marlboro Rd	Southern Maryland Blvd	Bike Lane	\$169,217	Few connections to essential destinations; sufficient space; ROW, utility and environmental issues	8	2
Southern Maryland Blvd	Greenock Rd	Fishers Station Rd	SUP	\$5,697,656	Connects to transit and jobs; sufficient space; environmental/slope challenges	8	2
Wrighton Rd	Lothian Service Center Park & Ride	Pindell Rd	Bicycle Lane	\$504,942	Connects to transit and jobs; sufficient space; environmental/slope challenges	8	2
Fairhaven Rd	MD 2 (Solomons Island Rd)	Town Point Rd	Bicycle Lane	\$401,933	Is proximate to jobs; sufficient space; ROW, utility and environmental issues	9	2
MD 2 (Solomons Island Rd)/MD 408 (Mt Zion Marlboro Rd)	MD 2 & Bayard Rd	MD 408 & Sands Rd	Bicycle Lane	\$919,070	Lack of direct access to essential destinations	8	2

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
MD 408 (Mount Zion Marlboro Rd)	Main St	MD 2 (Solomons Island Rd)	SUP	\$12,626,006	Connects to jobs on western segment; lack of direct access to essential destinations on eastern segment	8	2
MD 261/Walnut Ave/Lake Shore Dr/ Friendship Rd	Beach Rd	MD 2 (Ritchie Hwy)/ Solomons Island Rd N	Bicycle Lane	\$1,192,615	Prioritize segments connecting essential destinations first; remaining segments where there is a lack of direct access to destinations should be phased accordingly	8,9	2
Solomons Island Rd	MD 214 (Central Ave)	MD 260 (W Chesapeake Beach Rd)	SUP	\$37,148,719	Connects to few destinations; sufficient space; ROW, utility and environmental issues; bridge crossings significantly add to cost	8,9	2
W Bay Front Rd	MD 2 (Solomons Island Rd)	Southern Maryland Blvd/Greenock Blvd	Bike Lane	\$1,320,687	Few connections to essential destinations; relatively frequent environmental/slope challenges	8	2
MD 778/Old Solomons Rd	County Line	MD 2 (Solomons Island Rd)	Bicycle Lane	\$406,484	Few connections to essential destinations; ROW and utility issues	8	2
Southern Maryland Blvd	Wrighton Road	County Line	Bicycle Lane	\$450,923	Parallel proposed SUP on northward bound Southern Maryland Blvd	8	2
Pindell Rd	Wrighton Rd	Lower Pindell Rd	SUP	\$3,646,500	Few connections to essential destinations	8	2
SHADY SIDE							
MD 256 (Deale Churchton Rd)	Shady Side Rd	Masons Beach Rd/Deale Rd	SUP	\$5,415,053	Connects to jobs and essential destinations and is proximate to a school; there will be ROW, utility and environmental issues	9	0
Shady Side Rd	MD 256 (Deale Churchton Rd)	Hayes Rd	SUP	\$8,204,625	Connects to jobs and essential destinations; sufficient space, however there will be ROW, utility and environmental issues	9	0
Muddy Creek Rd	MD 256 (Deale Churchton Rd)	Galesville Rd	SUP	\$7,748,813	Sufficient space; some ROW and environmental issues	9	2
Owings Beach Rd	Drum Point Rd	E Marshall Ave	Bicycle Lane	\$134,482	Sufficient space, however there will be ROW and utility issues	9	0
Galesville Rd/Main St	Muddy Creek Rd	Riverside Dr	Bicycle Lane	\$249,494	Connects to jobs and a school; there will be ROW, utility and environmental issues	9	2
Owensville Rd	Muddy Creek Rd	MD 2 (Solomons Island Rd)	Bicycle Lane	\$646,906	Connects to few destinations; sufficient space, however, there will be utility and environmental issues	8,9	2
East Bay Front Rd	MD 2 (Solomons Island Rd)	MD 256 (Deale Churchton Rd)	Bicycle Lane	\$834,921	Sufficient space; however, there is a significant bridge crossing	9	2
Drum Point Rd	Deale Rd	Owings Beach Rd	Bicycle Lane	\$134,590	Significant ROW constraints, utilities will need to be moved; lack of direct connection to destinations	9	0
Deale Rd	MD 256 (Deale Churchton Rd)	MD 2 (Solomons Island Rd)	SUP	\$16,251,721	Significant constraints	9	2
Masons Beach Rd	MD 256 (Deale Churchton Rd)	Mason Ave/1st Ave	SUP	\$1,403,903	Significant constraints	9	0

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Parole - Bicycle Infrastructure and shared-use Paths							
Bestgate Rd	Existing SUP west of Generals Hwy	N Bestgate Rd	SUP	\$3,208,920	Sufficient space, will be ROW and some utility issues; sidewalks exist in segments but should be upgraded to SUP width	7	2
Riva Rd	Speicher Dr	West St	SUP	\$3,008,363	Bicycle lanes on Riva Road from Annapolis High School to West Street on both sides of the road have been studied; sidewalks exist on east side and CIP funded sidewalks on the west side of Riva Rd between Speicher Dr and 2715 Riva Road; sufficient space for SUP but there will be utility and ROW issues; where there are sidewalks at least one side of Riva Rd should be upgraded to be SUP of 11'; connects to jobs, transit, etc.	7	2
Housley Rd	Annapolis Ridge Rd including the traffic circle	Generals Hwy with gaps in between	SUP	\$1,002,788	Connects to jobs, transit, park; sufficient space with some ROW acquisition and utility relocation likely; SUP and sidewalks exist in segments	7	2
Housley Rd Extension	Defense Hwy	Harry S Truman Pkwy	SUP	\$8,870,111	Connects to jobs, transit, park	7	2
Defense Hwy	West St	N River Road	SUP	\$4,375,800	Connects to jobs and transit; sufficient space, however will likely be ROW and utility relocation needs	6,7	2,3
Gateway Village Dr	Housley Rd	Generals Hwy	SUP	\$902,509	Connects to jobs, health care care	7	2
Somerville Rd	West St	MD 2 (Solomons Island Rd)	Bike Lane	\$211,526	Project included in the Poplar/South Shore Trails Study; connects to jobs, transit	7	2
Rowe Blvd	N Bestgate Rd	Kirkley Rd	SUP	\$2,279,063	Connects to jobs; overpass and slopes are constraints	7	2
Harry S Truman Pkwy	Coover Rd	Riva Rd	Bike Lane	\$363,888	Connects to jobs, services, park, transit; sufficient space with environmental work	7	2
Spruill Rd and Harry S Truman Pkwy	Harry S Truman Pkwy & Riva Rd	Spruill Rd & Womack Dr	Separated Bike Lane	\$423,793	Connects to jobs, transit	7	2
Bridge over MD 665	Spruill Rd & Womack Dr	Annapolis Harbour Center Shopping Mall	SUP	\$8,625,577	Connects to jobs; bridge will be costly	7	2,5
Annapolis Harbour Center Shopping Center SUP	Proposed bridge over MD 665	Forest Dr	SUP	\$661,840	Connects to jobs, transit	7	2,5
Admiral Cochrane Dr	Riva Rd	Solomons Island Rd	Separated Bike Lane	\$797,478	Connects to jobs, park, transit; sidewalks exist	7	2
Admiral Cochrane Dr Extension	Riva Rd	Harry S Truman Pkwy	SUP	\$1,002,788	Connects to jobs, transit	7	2
South Shore Trail	Bestgate Rd		SUP	See South Shore Trail PIA	Connects to jobs, transit, parks	6,7	0
Generals Hwy	Carriage Hill Pkwy	Bestgate Rd	SUP	\$2,734,875	Sufficient space, will be ROW and some utility issues; sidewalks exist in segments but should be upgraded to SUP width	7	2
West St	Generals Hwy	Holly Ave	SUP	\$4,056,731	Sufficient space, will be ROW and some utility issues; sidewalks exist in segments but should be upgraded to SUP width	7	2
Jennifer Rd	West St	Medical Pkwy	SUP	\$1,203,345	Sufficient space, will be ROW and some utility issues; sidewalks exist in segments but should be upgraded to SUP width; overpasses will increase cost	7	2

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Aris T Allen Blvd	Solomons Island Rd	Forest Dr	SUP	\$8,125,861	Sufficient space (although a wall on one side for almost half of the segmnet will challenge design); lack of direct access to destinations	7	5
Parole Sidewalks							
Solomons Island Rd	John Hanson Hwy	Lake Shore Dr		\$3,801,600	Sufficient space in many segments; ROW and utilities will be issues	7	2
Harry S Truman Pkwy	Parole Town Center Boundary	Harry S Truman Pkwy when it diverts west, north of Riva Rd		\$1,663,200	Transit stops, sufficient space, will be utility and ROW and environmental issues	7	2
Riva Rd	Street east of Admiral Cochrane Dr	Admiral Cochrane Dr		\$237,600	Connects to jobs; sufficient space, will be utility and ROW issues; there is already a sidewalk on one side, this street requires sidewalks on both sides	7	2
Riva Rd	MD 665	West of Hearne Rd		\$237,600	Connects to jobs and transit; sufficient space, will be ROW issues; there is already a sidewalk on one side, this street requires sidewalks on both sides	7	2
Annapolis Center	Hearne Ct	Annapolis Harbour Ctr Dr & Solomons Island Rd		\$475,200	Connects to jobs and transit; private owner partnership opportunity	7	2
Solomons Island Rd	Jennifer Rd	Admiral Cochrane Dr		\$950,400	Connects to jobs; sufficient space, will be utility and ROW issues	7	2
Old Solomons Island Rd	West St	Forest Dr		\$950,400	Sufficient space; ROW and utilities will be issues	7	3
Generals Hwy	Knollwood Dr	Bestgate Rd		\$950,400	Connects to jobs; sufficient space; will be utility, ROW, some environmental issues	6,7	2
Old Admiral Ct	End of Road	Admiral Dr		\$475,200	Already SUP on some of this segment; significant ROW constraints	7	2
Housley Rd	Generals Hwy	West of Generals Hwy		\$475,200	Sidewalks exist on one side, some parts on both sides; overpass issues	7	2
West St	Jennifer Rd	Riva Rd		\$950,400	Sidewalks exist on one side, a small part on both sides; ROW & utility constraints	7	2
Somerville Rd	West St	Old Solomons Island Rd		\$950,400	Sidewalks exist on one sdie; ROW & utility constraints	7	2,3
Odenton Bicycle Infrastructure/ shared-use Path							
MD 175 (Annapolis Rd)	Blue Water Blvd	Odenton Rd	SUP	\$6,381,375	Sufficient space for most of the corridor, however there will be ROW, utility and environmental constraints; there is also a bridge and a cemetary which will likely require additional phasing; connects to jobs, transit, other destinations	2,5	0
Telegraph Rd	Old Mill Rd	MD 175 (Annapolis Rd)	SUP	\$2,962,781	Sufficient space with ROW, utility and environmental issues; underpass constraints	5	0
Burns Crossing Rd/ Sappington Station Rd	WB&A Road	MD 175 (Annapolis Rd)	SUP	\$8,159,044	Study complete	3,5	0
SUP Along Railroad	Telegraph Rd	Odenton Rd	SUP	\$401,115	Sufficient space	3,5	2
N Patuxent Rd	Becknel Ave	Odenton Rd	SUP	\$200,558	Sufficient space with ROW acquisition required; connects to transit	5	2
SUP Over Light Rail Tracks	End of Odenton Rd (west of N Patuxent Rd)	End of Odenton Rd (east of Town Center Blvd)	SUP	\$5,390,986	Access to transit; expensive	5	0
Becknel Avenue	Odenton Rd	Piney Orchard Pkwy/WB&A Trail	Bike Lane	\$365,046	Access to transit and trail	5	2

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
MD 32 (Patuxent Fwy)	East of Mapes Rd	MD 175 (Annapolis Rd)	SUP	\$6,381,375	Sufficient space along freeway; access to jobs	2	0
Annapolis Rd	Telegraph Rd	Crain Hwy	SUP	\$2,607,248	Connects to schools, jobs, transit, health care; ROW and utility constraints	5	2
Baldwin Rd	Berger St	Annapolis Rd	Bike Lane	\$96,283	In existing County plans	5	0
Odenton Sidewalks							
Lokus Rd	Betson Ct	End of road		\$712,800	Sufficient space though ROW required and utility relocation likely required; connects to jobs	5	0
Betson Ct	Lokus Rd	Urban Ave		\$237,600	Sufficient space though ROW required; connects to jobs	5	0
MD 175 (Annapolis Rd)	Telegraph Rd	Odenton Rd		\$2,138,400	Sufficient space though ROW and utility relocation required; connects to jobs, school, transit; there is a bridge and cemetary which will likely require additional phasing	5	0
Odenton Rd	N Patuxent Rd	MD 175 (Annapolis Rd)		\$2,851,200	Connects to transit, jobs and a school; will be ROW constraints and likely utility relocation	5	2
Becknel Rd	End of road	End of road		\$712,800	Connects to transit and jobs; will be ROW constraints	5	2
N Patuxent Rd	Becknel Rd	Waugh Chapel Rd		\$1,663,200	Connects to jobs; will be ROW constraints	5	2
Monterey Ave	Odenton Rd	June Dr		Project is funded	Connects to a school; will be ROW constraints	5	2
Holiday St	Odenton Rd	Greenwood St		\$237,600	Connects to jobs; ROW constraints	5	2
Greenwood St	Odenton Rd	Hammond Ln		\$237,600	Connects to jobs; ROW constraints	5	2
Hammond Ln	Greenwood St	Higgins Dr		Project is funded	Connects to jobs; ROW constraints	5	2
Higgins Dr	S of Hammond Ln	End of road		Project is funded	ROW constraints and environmental issues; proximate to a school	5	2
Monie Rd	Maple Ridge Ln	Higgins Dr		Project is funded	ROW constraints; proximate to a school	5	2
Winer Rd	Maple Ridge Ln	Higgins Dr		\$237,600	ROW constraints; proximate to a school	5	2
Hillcrest Rd	Maple Ridge Ln	Higgins Dr		\$237,600	2nd - ROW constraints; proximate to a school	5	2
Baldwin Rd	Annapolis Rd	End of road		\$237,600	2nd - ROW required	5	0
Rita Dr	Baliol Ln	South of June Dr		\$950,400	Proximate to a school; will be ROW constraints	5	2
King Malcolm Ave	Odenton Rd	North of Queen Anne Ave		\$1,188,000	Proximate to a school; will be ROW constraints	5	2
Bruce Ave	Odenton Rd	North of Queen Anne Ave		\$1,188,000	Feasibility study is funded; proximate to a school; will be ROW constraints	5	2
Winer Rd	Bruce Ave	Williamsburg Ln		\$237,600	Proximate to a school; will be ROW constraints	5	2
Glen Burnie Bicycle Infrastructure/ shared-use Paths							
8th Ave NW	MD 648 (Baltimore Annapolis Blvd)	MD 3 (Crain Hwy N)/8th Ave NE	Separated Bike Lane	\$557,025	Sufficient space in most segments, except for two stream crossings where either bridge expansion or pathway through stream valley crossing is required	1,3	4,5
N Thompson Ave/ Stiemly Ave	N Thompson Ave & E Furnace Branch Rd	Stiemly Ave & Biddle Rd	Separated Bike Lane	\$557,025	Sufficient space, connects to jobs, school	3	2

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
MD 648 (Baltimore Annapolis Blvd)	Eastern St	E Furnace Branch Rd	Separated Bike Lane	\$2,080,553	Sufficient space with lane reconfiguration; connects to jobs, transit, schools	3	2
3rd Ave SW & 3rd Ave SE	Newfield Rd & 3rd Ave SW	Griffith Ln & 3rd Ave SE	Separated Bike Lane	\$754,806	Sufficient Space	3	5
4th Ave SW/D St SW	D St SW & 3rd Ave SW	4th Ave SE/3rd Ave SE	Separated Bike Lane	\$646,659	Sufficient space with removal of on-street parking; connects to or is proximate to schools	3	5
Griffith Ln	Crane Hwy	Greenway SE	Separated Bike Lane and Bike Lane	\$495,994		3	5
MD 3 (Crain Hwy N and S)	Crain Hwy N & Ritchie Hwy	Crain Hwy S & Veterans Hwy	SUP	\$14,808,072	Sufficient space with ROW acquisition and utility removal needed; connects to jobs and school	3	5
5th Ave SW	Stewart Ave	5th Ave SW/ Crain Hwy S	Bike Lane	\$167,805	Sufficient space and connects to a school	3	2,5
5th Ave SE	5th Ave SW/ Crain Hwy S	MD 2 (Ritchie Hwy)	SUP	\$898,066	Sidewalk exists; ROW constraints; could complete a phase east of Oakwood Dr, where sufficient space exists although ROW acquisition is likely and utilities would need to be moved	3	2,5
Quarterfield Rd	Crain Hwy S	Old Stage Road	SUP	\$1,203,345	Sidewalk exists; ROW constraints	3	2
Aquahart Rd/Fitzallen Rd	Aquahart Rd & Crain Hwy S	Upton Rd (Fitzallen Rd)	Separated Bike Lane	\$1,084,245	Connects to transit, B&A Trail, school, jobs, services, health care, retail; sidewalk exists; ROW and some environmental constraints	3	2,5
Thelma Dr	Elizabeth Ln	Crain Hwy S	Separated Bike Lane	\$898,066	Sidewalk exists; ROW constraints	3	2.00
N/A	Stewart Ave & Old Stage Rd	Thelma Dr & Elizabeth Ln	SUP	\$911,625	ROW constraints	3	2
Eastern Rd	MD 648 (Baltimore Annapolis Blvd)	Longwood Ave	SUP	\$319,069	Utility, ROW, environmental constraints	1	4
MD 176 (Dorsey Rd)	I-97	MD 648 (Baltimore Annapolis Blvd)	SUP	\$1,805,018	SUP exists on a portion of this segment although a portion should be upgraded; sidewalks also exist on portions but should be upgraded; some utility and ROW constraints	1	2,4,5
MD 3 (Crain Hwy)	Furnace Branch Rd	5th Ave SW/5th Ave SE	SUP	\$1,604,460	Significant ROW issues in many places; sidewalks exist in most segments; extensive ROW acquisition and utility relocation will be required; connects to jobs, transit, school	3	2,5
Stewart Ave	Old Stage Rd	5th Ave SW	Bike Lane	\$223,282	Sidewalk exists; ROW constraints	3	2,5
Glen Burnie Sidewalks							
MD 270 (E Furnace Branch Rd)	MD 3 (Crain Hwy)	Stewart Ln		\$712,800	Sufficient space, however ROW and utility relocation will add to costs and time	3	2,5
8th Ave	West of MD 3	MD 648 (Baltimore Annapolis Blvd)		\$1,900,800	Sufficient space, some ROW costs, some environmental issues	3,1	4,5
MD 2 (Ritchie Hwy)	O St NE	Amberly Rd		\$1,188,000	Sidewalks exist on one side but should be upgraded; sidewalks should be built on other side; access to transit, jobs, school	3	2,3
MD 648 (Baltimore Annapolis Blvd)	West of Amberly Rd	Binkey Rd		\$712,800	Sidewalks exist on one side but should be upgraded; sidewalks should be built on other side; access to transit, jobs, proximate to a school; utility relocation potential; ROW will likely need to be acquired	3	2

Route Name	Terminus 1	Terminus 2	Facility Type	Cost Estimate	Opportunities and Constraints	RPA	Vulnerable Population Index Score
Range Rd	Greenway Rd SE	West of Greenway Rd SE		\$237,600	Sufficient space; ROW acquisition needed; potential utility relocation	3	2,5
5th Ave SW	Stewart Ave	Crain Hwy S		\$950,400	Significant ROW constraints however roadway is a priority for sidewalks given traffic volume and proximity to destinations	3	2,5
D St SW	4th Ave SW	3rd Ave SW		\$237,600	Close to schools; ROW acquisition needed	3	5
Glen Heights Ave	Stewart Ave	Main Ave SW		\$712,800	Significant ROW constraints, however sidewalks are needed given proximity to schools	3	2
Main Ave SW	Elizabeth Ln	Crain Hwy S		\$950,400	Close to school, ROW and utility constraints	3	2
Elizabeth Ln	Stewart Ave	Thelma Ave		\$950,400	Close to school, ROW and utility constraints	3	2
Old Stage Rd	South of Jay Court	Thelma Ave		\$237,600	Close to school, ROW needed	3	2
Thelma Ave	Old Stage Rd	Elizabeth Ln		\$712,800	Connects to school, ROW and utility constraints	3	2
MD 2 (Ritchie Hwy)	MD 270 (E Furnace Branch Rd)	MD 648 (Baltimore Annapolis Blvd)		\$3,326,400	Connects to jobs and proximate to transit; sufficient space in many segments however ROW and utilities will be issues; the preference for this segment would be a SUP recommended in the North County PIA on one side of Crain Hwy and sidewalks on the other; sidewalks exist on one side in many segments	3	2,5
MD 648 (Baltimore Annapolis Blvd)	8th Ave NW	Linden Ln NW		\$475,200	Sidewalks exist on one side but need to be upgraded; sidewalks needed on both sides of the street	1,3	2,4,5
Linden Ln NW	Central Ave	Georgia Ave NW		\$712,800	Sidewalks installed from MD 648 to Georgia Ave in 2021; direct connection to destinations limited	3	5
Georgia Ave NW	Linden Ln NW	Maple Ln NW		\$237,600	Sidewalks installed from Linden Ln to Crain Hwy in 2021; direct connection to destinations limited	3	5
MD 648 (Baltimore Annapolis Blvd)	MD 2 (Ritchie Hwy)	West of Glen Rd		\$950,400	Sidewalks exist on one side (sometimes 2) but should be upgraded; access to transit, jobs, a school; utility relocation and ROW acquisition required	3	2
Oakwood Rd	5th Ave SE	Aquahart Rd		\$1,188,000	Sidewalks exist in segments but should be upgraded to Town Center standards; significant ROW and utility constraints	3	2
Pine Terrace	Oakwood Rd	Greenway Rd SE		\$475,200	Significant ROW and utility constraints	3	2
Barbara Moeller Way	MD 648 (Baltimore Annapolis Blvd)	Post 40 Rd		\$237,600	Sidewalks exist but should be upgraded to Town Center standards	3	5

Opinions of probable cost were developed by identifying major pay items and establishing rough quantities based on a linear foot calculation by facility type to determine a rough order of magnitude cost. Additional pay items have been assigned approximate lump sum prices based on a percentage of the anticipated construction cost. Planning-level cost opinions usually include a 30% contingency to cover items that are undefined or are typically unknown early in the planning phase of a project but this cost estimator assumes 10%. We can not confirm what unit costs are based. Cost opinions do not include easement and right-of-way acquisition; permitting, inspection, or construction management; engineering, surveying, geotechnical investigation, environmental documentation, special site remediation, escalation, or the cost for ongoing maintenance. This cost opinion also does not include contingencies for drainage, erosion and sediment control, and utilities which we usually include in our planning level opinions. The overall cost opinions are intended to be general and used only for planning purposes. Toole Design Group, LLC makes no guarantees or warranties regarding the cost estimate herein. Construction costs will vary based on the ultimate project scope, actual site conditions and constraints, schedule, and economic conditions at the time of construction.

Appendix G: May 17, 2022 Public Meeting Presentation



WALK&ROLL ANNE ARUNDEL!

www.aacounty.org/walkroll

May 17th Meeting Agenda

- Why are we here?
- What we've done so far
- What we've seen and heard
- Designing the network
- How you can get involved



WALK&ROLL ANNE ARUNDEL!

Who we are and why we're here

- Anne Arundel County wants to be "The Best Place" to walk, bike, or roll
- Not just about walking and bicycling, but making the County's streets safer and more comfortable for everyone



Photo: Brian O'Doherty

What we've done so far

- Fieldwork
- Reviewing past plans, traffic data, and crash data
- Interactive webmap to collect community feedback
- Initial network development



Starting a conversation

- Where to create new walking and bicycling facilities
- What types of facilities to build
- Creating a countywide network and local networks in Town Centers
- Recommended policies
- Which projects to prioritize creating first
- We will work closely with the City of Annapolis to ensure connections

Building on past County plans



2003 PBMP

2013 PBMP

Plan 2040

Move Anne Arundel!

Vision Zero

Draft Plan

Crash data



- Between 2013-2017 an average of 8 people died on streets in the County yearly
- 58% of pedestrian crashes occurred on State roads
- 59% of bicycle involved crashes occurred on County roads

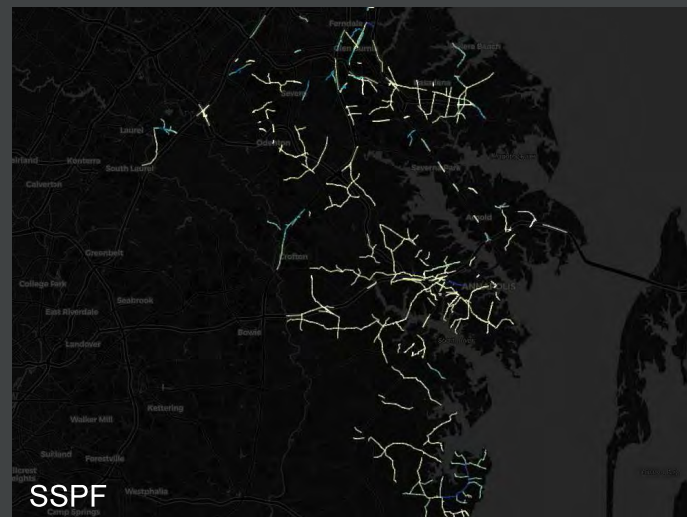
Interactive webmap + survey

- Open through May 27
- www.aacounty.org/walkroll
- We've heard from over **800 people** since February



Safer Streets Priority Finder

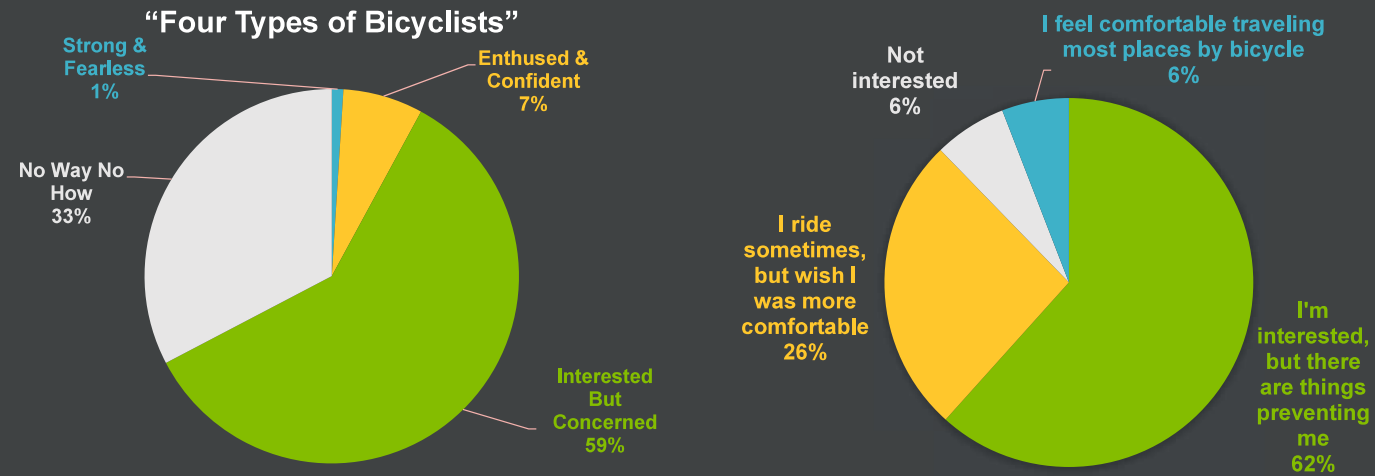
- Using 2016-2020 crash data to identify similar places where crashes could occur in the future
- High-risk corridors include MD 648, MD 2, MD 3, and MD 177
- <https://www.saferstreetspriorityfinder.com/>



What we've heard

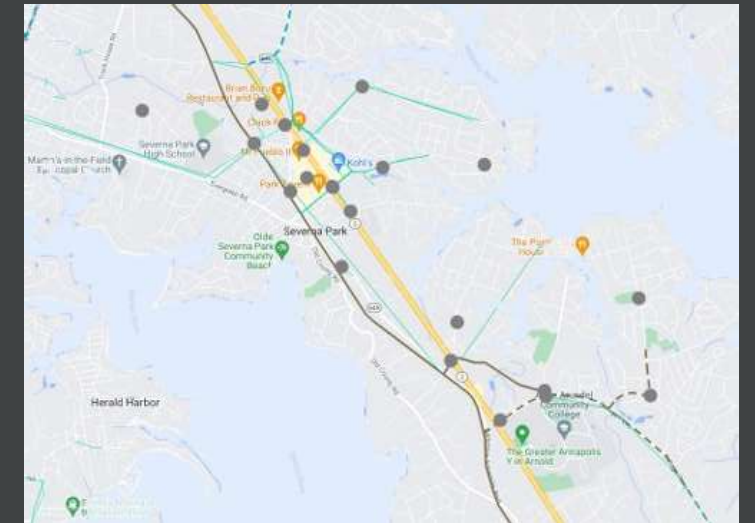
- **40%** walk or roll on a regular basis
- **35%** bike on a regular basis
- **39%** say they feel safe walking or biking in their neighborhood
- **96%** say more sidewalks, bike lanes, or trails would encourage them to walk or bike more

What we've heard

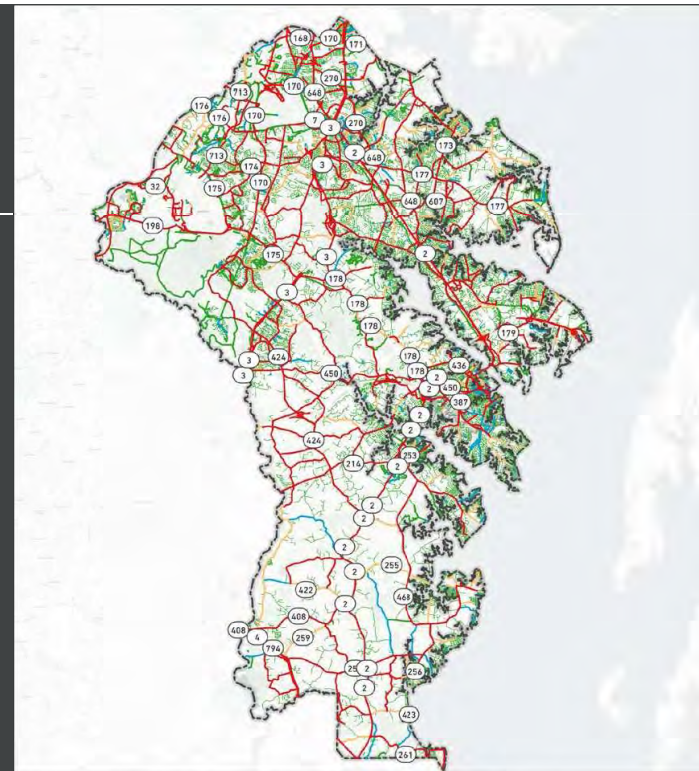


What we've heard

- Need better connections to existing trails like the B&A
- Main roads like MD 2 or MD 3 are barriers to walking and bicycling
- Desire to make neighborhood collector roads safer for walking and biking



Level of Traffic Stress

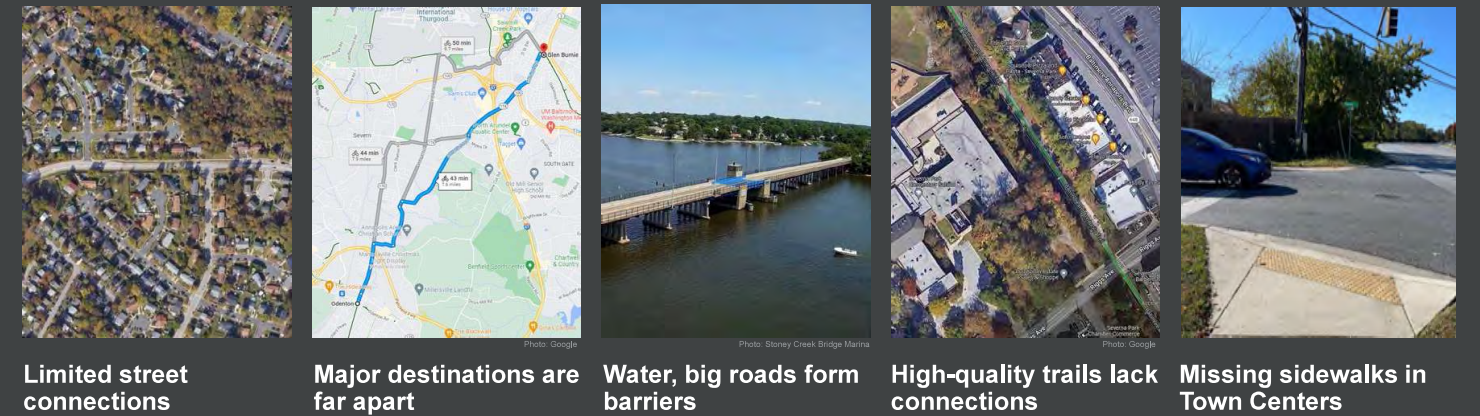


Anne Arundel County Bicycle Level of Traffic Stress

- LTS 1 (Green)
- LTS 2 (Blue)
- LTS 3 (Yellow)
- LTS 4 (Red)

0 1 2 3 4 5 mi

Takeaways



Limited street connections

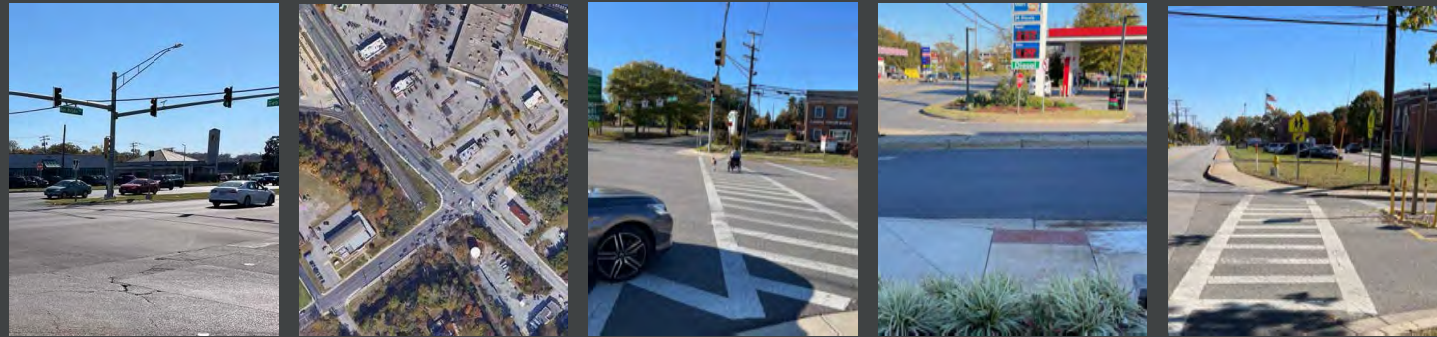
Major destinations are far apart

Water, big roads form barriers

High-quality trails lack connections

Missing sidewalks in Town Centers

Proposed pedestrian network goals



Focus on high-speed, high-volume roads

Connect to transit, jobs, and daily needs

Improve crossings and create new ones

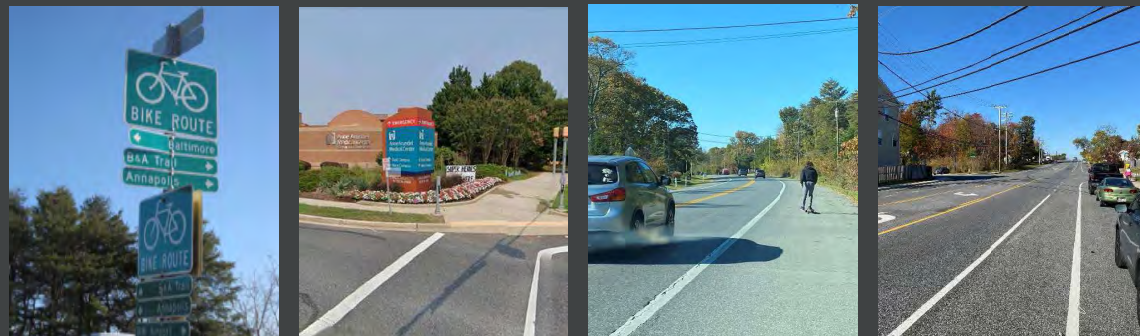
Support completing Town Center sidewalks

Promote Safe Routes to School

Equity

- Walk & Roll Anne Arundel! envisions an equitable transportation network
- Making it easier and safer for people to travel without a car means more people can access economic opportunities & daily needs

Proposed bicycle network goals



Make existing trails a spine for new ones

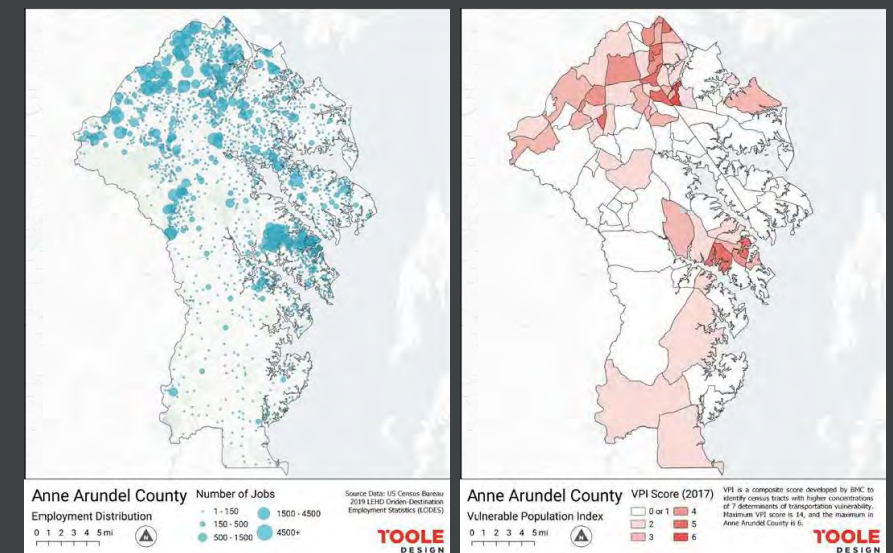
Connect to transit, jobs, and daily needs

Fit urban, suburban, and rural roads

Connect and improve existing facilities

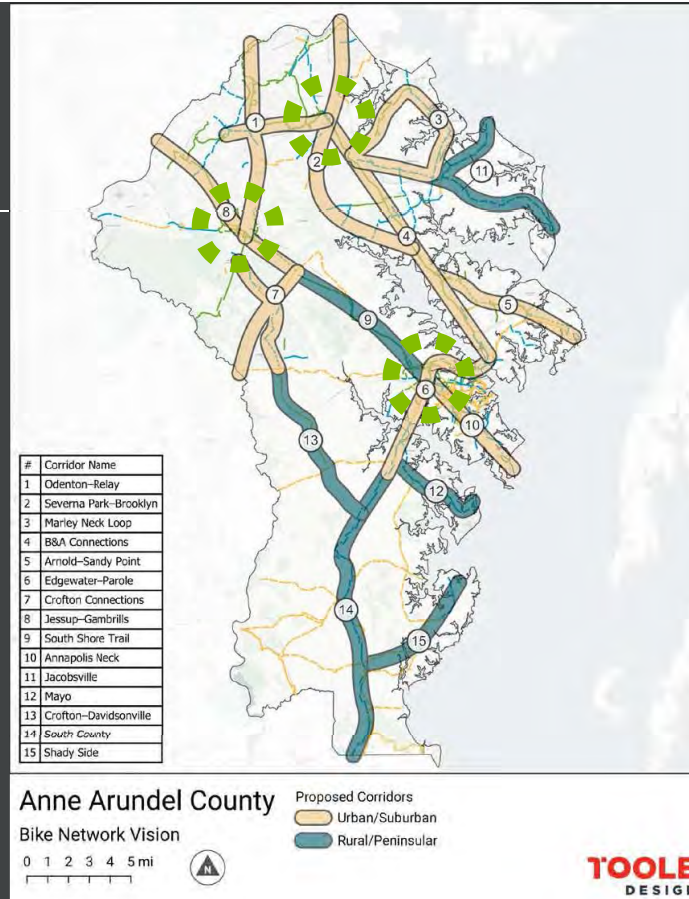
Equity

- Anne Arundel County's jobs and its most vulnerable populations overlap around Annapolis and North County
- Few jobs are accessible to lower-income residents in South County

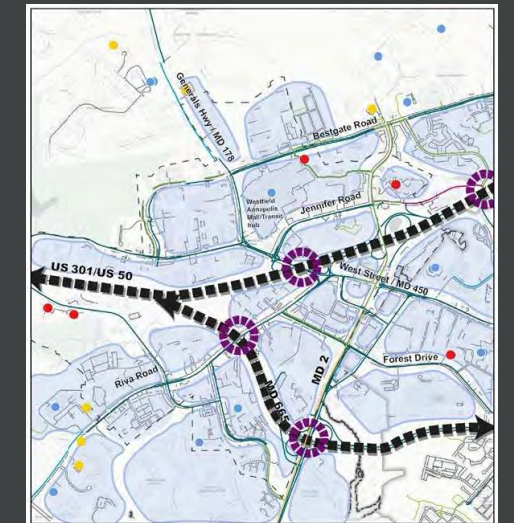
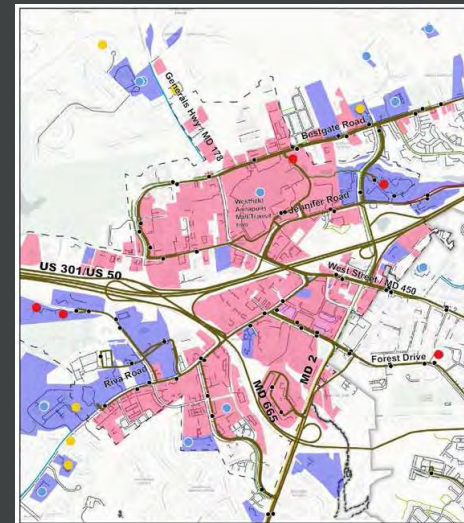


What could the Walk & Roll network look like?

- We're looking at 15 County Corridors for potential improvements
 - Urban/Suburban and Rural/Peninsula corridors
- Starting a conversation
- Specific streets and treatments will be identified later



Parole Town Center



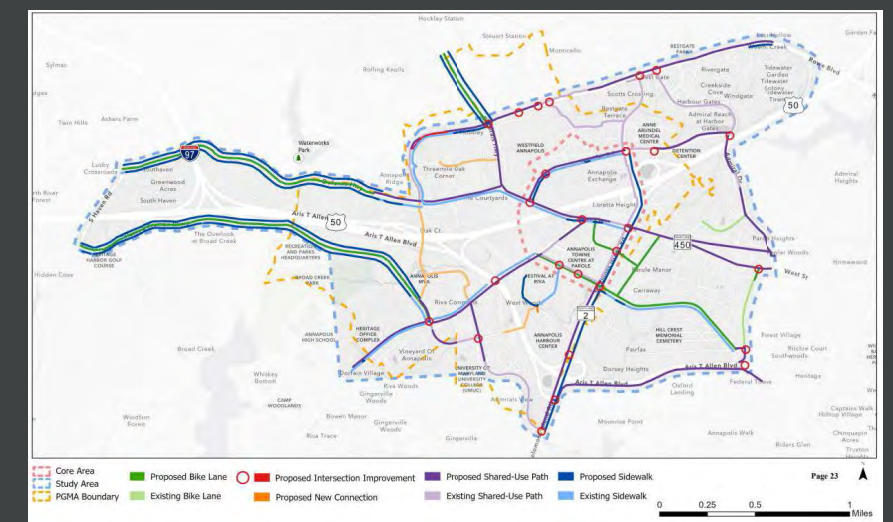
Town Centers

- We're also looking at local streets in the County's three Town Centers
 - Glen Burnie
 - Odenton
 - Parole

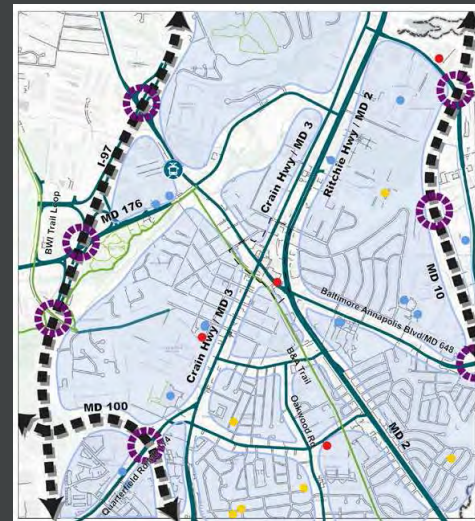
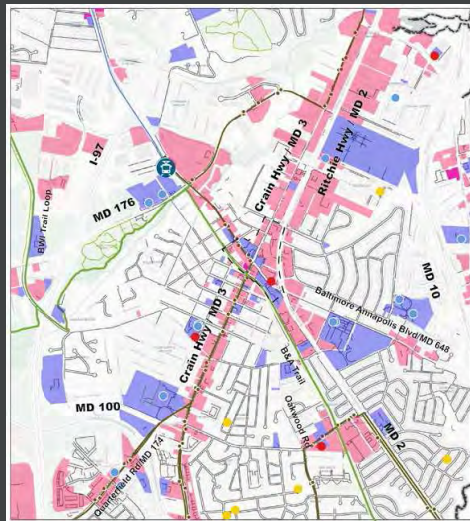


Parole Mobility Study (2021)

- Recommendations for new shared-use paths, sidewalks, bike lanes, and intersection improvements
- This plan will build on those efforts



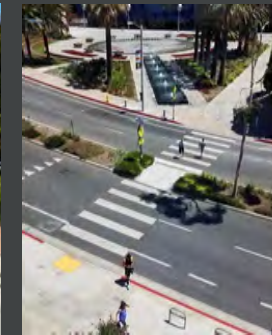
Glen Burnie Town Center



Pedestrian investments



Sidewalks



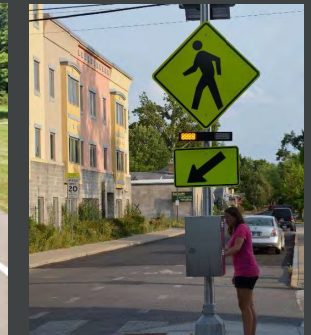
Crosswalks



Traffic Calming

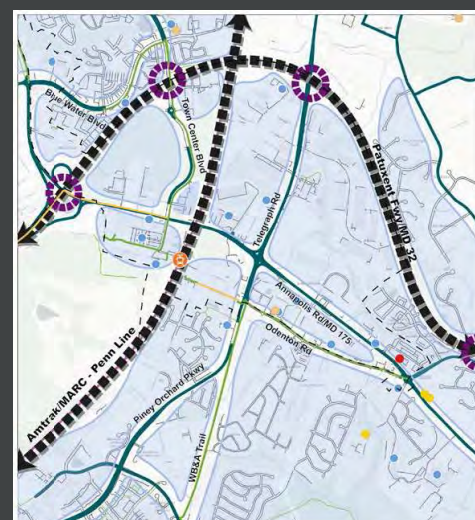
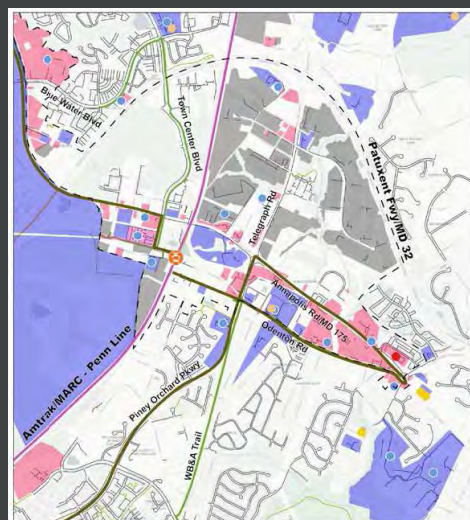


Pedestrian Lane



Signs and Signals

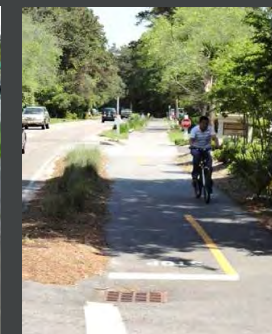
Odenton Town Center



Bicycle investments



Signed Route



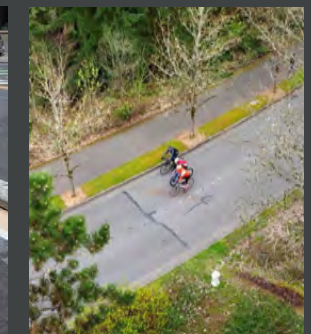
Sidepath



Bike Lane



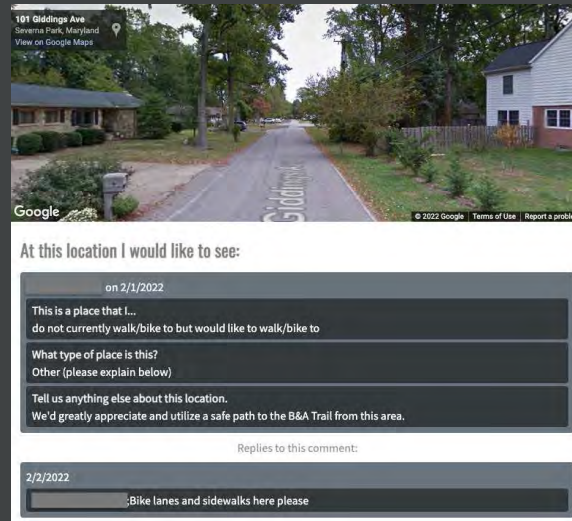
Separated Bike Lane



Trail

How can I get involved?

- Visit our website at www.aacounty.org/walkroll
- Take our survey
- Comment on our interactive webmap by 5/27/22
- Additional community meetings this summer



WALK&ROLL ANNE ARUNDEL!

www.aacounty.org/walkroll

Timeline

Winter/Spring 2022: **Public outreach**

Spring/Summer 2022: **Network development**

Summer 2022: **Writing the plan**

Summer/Fall 2022: **Submit plan to County Council**

Fall 2022: **County Council votes to approve**

Appendix H: Web Map and Survey Summary Memorandum

MEMORANDUM

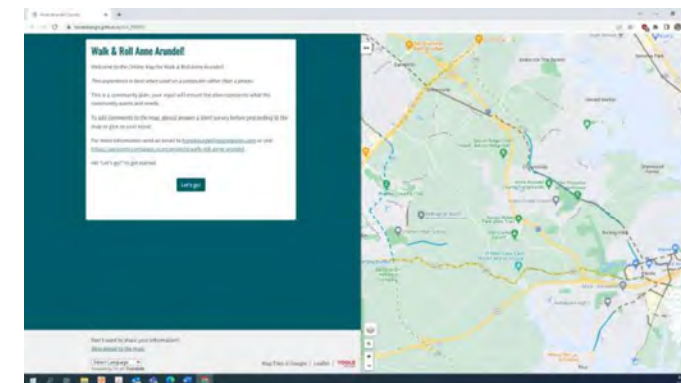
July 30, 2022

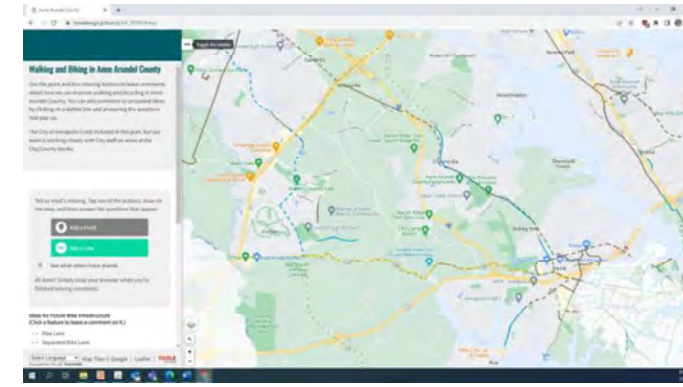
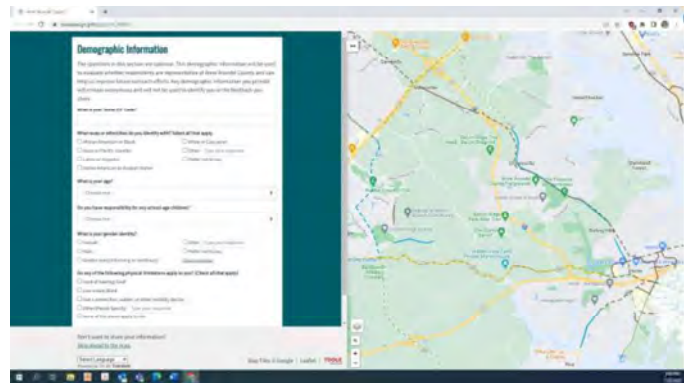
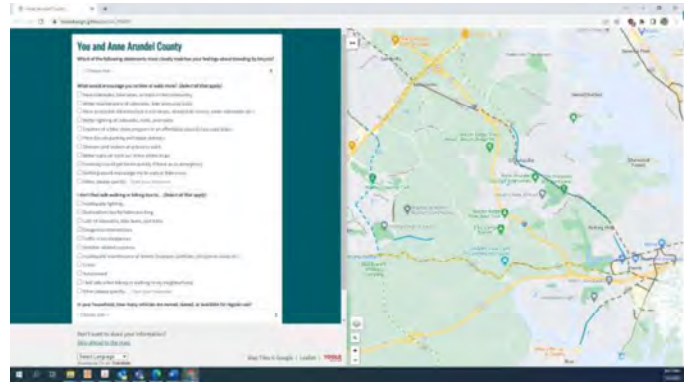
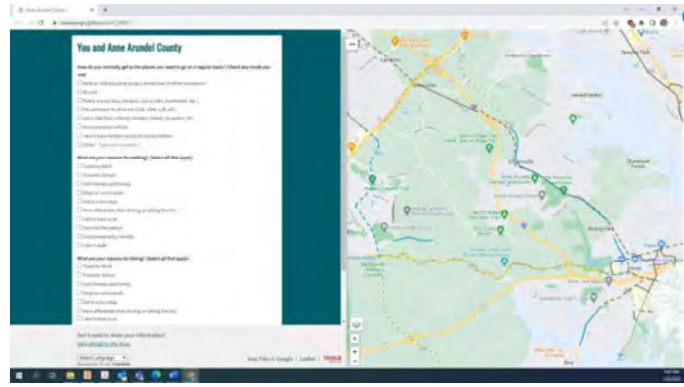
To: Tanya Asman
Organization: Anne Arundel County
From: Toole Design Group
Project: Walk & Roll Anne Arundel!

Re: Webmap and Survey Process and Results

Webmap and Survey Process

The project team developed and deployed a survey and online public feedback map. The map provided members of the public an opportunity to tell us where they walk, bike, or roll, and where they would like to see improvements to make the experience safer and more comfortable. On the interactive webmap, members of the public were also able to mark places where the walking, bicycling, or rolling experience could be better. The online interactive map is viewable at https://tooledesign.github.io/AA_PBMP/ and the graphics below provide more information.





Webmap and Survey Summary

- Total number of responses: **812**
- Majority of respondents were white (74%)
- Most represented age groups were 35-44, 45-54, and 55-64 (about 22% each)
- Majority of households have 2 cars (56%)
- More respondents did not have school-aged children (59%)
- 92% of respondents did not report any disabilities, 2.6% reported being hearing impaired, 1.5% utilize a wheelchair or walker, 2% other, 0.8% visually impaired
- Almost every respondent reported that having more sidewalks, bike lanes, or trails in the community would encourage them to walk or bike more (95.6%)
 - More than a quarter of respondents also reported the following would encourage them to walk or bike more:
 - More accessible infrastructure
 - Better lighting of sidewalks, trails, and roads
 - Better signs so they know where to go
- Two of the largest reported inhibitors to walking and biking were the lack of sidewalks, bike lanes, and trails (74.6%) and traffic being too dangerous (71.2%)
 - More than a quarter of respondents also reported the following inhibitors prevent them from walking and biking:
 - Dangerous Intersections
 - 39% of respondents said they feel safe when biking or walking in their neighborhood
- Majority of respondents reported using a personal vehicle to get places (96.5%)
 - Walk or roll – 40%
 - Bike – 34.78%
 - Family or friend – 5.22%
 - Pay someone (Uber, Lyft, Taxi) – 4.84%
 - Public Transit – 3.2%
 - Other – 1.74%
 - No reliable access -0.5%

Timestamp	name	email	commentType	commentID	Tell Us Anything About This Location	Tell us what you think of this existing route
12/14/21 11:38			route	1639499884260	I'd love for there to be a bridge	
12/14/21 11:44	Kurt Riegel	kurt.riegel@gmail.com	welcome-survey			
12/14/21 11:52	Kurt Riegel	kurt.riegel@gmail.com	point	1639500741803	Naval stadium kindly added sidewalk connecting path encircling stadium property, with Farragut Rd. PROBLEM. It is perpendicular to Farragut, putting cyclists in conflict with high speed traffic. Ten feet of the end of the connecting path should be PARRALEL to Farragut , pointed at the sidewalk for safety.	
12/14/21 12:11	Kurt Riegel	kurt.riegel@gmail.com	point	1639501871462	The entire length of Ridgely is unsafe for both pedestrians and bicyclists. Best would be a physically separated shared use path the entire length, joined with the sidewalk crossing Weems Creek. Next best would be clearing the shoulders of dirt and vegetation which horribly render the shoulder unusable and unsafe. Neighbors have asked state to fix this for years, to no effect.	
12/14/21 12:14	Luis Lucchini	lrlucchini07@gmail.com	welcome-survey			
12/14/21 12:17	Luis Lucchini	lrlucchini07@gmail.com	point	1639502220670	This corridor will connect a bunch of people to useful businesses including downtown. Please consider proposing a protected bike lane instead due to the high speed limits and amount of vehicles including large trucks and irresponsible driving.	
12/14/21 12:17	Luis Lucchini	lrlucchini07@gmail.com	welcome-survey			
12/14/21 12:19			welcome-survey			
12/15/21 10:44			welcome-survey			
12/15/21 11:31			welcome-survey			
12/15/21 12:35			welcome-survey			
12/16/21 7:58	ursula wilder	ursulawilder@yahoo.com	welcome-survey			
12/16/21 8:13	ursula wilder	ursulawilder@yahoo.com	route	1639660426592	Telegraph and 174 is getting to be a problem,	
12/16/21 9:20			welcome-survey			
12/16/21 10:09	Jon Korin	korin.jon@gmail.com	welcome-survey			
12/16/21 10:11	Jon Korin	korin.jon@gmail.com	welcome-survey			
12/16/21 10:16	Jon Korin	korin.jon@gmail.com	point	1639667772559	This is a major gap in the bike network between the B&A Trail and USNA Bridge bike lanes and downtown Annapolis and the existing trail at the USNA Stadium. The ideal solution here is SUPs from the bridge to the stadium and to St Johns College	
12/16/21 10:19	Jon Korin	korin.jon@gmail.com	route	1639667949027	This is a major gap in the envisioned AA Trail network and should have a safe trail connector from Arundel Mills to the BWI Trail	
12/16/21 12:14	Connor	Connorrinehamer@gmail.com	welcome-survey			
12/16/21 12:16	Connor	Connorrinehamer@gmail.com	point	1639674993845	713 has a distinct lack of sidewalks, they exist in spots on the road but aren't cohesive	
12/16/21 12:18	Connor	Connorrinehamer@gmail.com	point	1639675099989	Making this location a 4 way intersection would ease congestion, make it safer to traverse for people living in both neighborhoods, and possibly improve visibility in order to make turns safely in vehicles	
12/16/21 12:19	Connor	Connorrinehamer@gmail.com	point	1639675180866	Dangerous intersection for bikers and vehicles due to the lack of traffic calming and curve of road	

12/16/21 12:24	Connor	Connorrinehamer@gmail.com	welcome-survey			
12/16/21 13:32	Alex Pline	alex@teampline.org	welcome-survey			
12/16/21 13:48	Alex Pline	alex@teampline.org	point	1639680524325	The bollards that comprise this "protected bike lane" are actually counter productive. The ends are "pinched" so it's impossible to enter or exit easily. There is a lot of debris inside and the plastic pieces are often broken and create sharp debris. For what it's worth, the history of this infrastructure as an attempt at traffic calming (a visual trick to appear to narrow the lane) to try and prevent cars from entering the downhill with speed due to several high visibility fatal crashes. That said, it doesn't really work and makes it more difficult for cyclists because it forces them into the travel lane. It should be removed.	
12/16/21 13:52	Alex Pline	alex@teampline.org	point	1639680730354	This 8' shared use path is rarely used because users are forced to cross highway ramps where cars are moving at highway speeds. Also it is not well maintained.	
12/16/21 13:54	Alex Pline	alex@teampline.org	point	1639680871388	This intersection has no signalized crosswalk and is very dangerous for anyone walking.	
12/16/21 13:58	Alex Pline	alex@teampline.org	route	1639681080330	Bestgate road needs a shared use path on at least one side. It is a heavily used connection for cyclists to get west and north out of Annapolis for recreational purposes, services Waterworks park and the only convenient way to get to the Westfield Mall.	
12/16/21 14:07	Alex Pline	alex@teampline.org	point	1639681662279	All minor trail crossings should have the stop requirement reversed so vehicles on the street must stop and trail has right of way. For all practical purposes, this is the current behavior for virtually all trail users and drivers. Ultimately this creates too much ambiguity and reversing the stop would sync the law with current behavior.	
12/16/21 14:23	Alex Pline	alex@teampline.org	point	1639682637278	Parole Town Center, while internally people friendly, is bounded by 4-8 lane arterials that act as a "moat" around the "smart growth" development. These boundaries make it inhospitable to walk or bike to, especially at night. Even where there are signalized level crossings, they are all auto priority and very inconvenient for non-auto users.	
12/16/21 14:25	Alex Pline	alex@teampline.org	point	1639682750777	There is literally no pedestrian or bike connection between these two shopping centers. The only way to access Harbor Center is via vehicle on MD 2/665.	
12/16/21 14:29	Alex Pline	alex@teampline.org	point	1639682961140	The MD 2/665 interchange is high speed, highway geometry auto infrastructure. In order to avoid it if you are on a bicycle you must divert (at a minimum) several miles Riva Rd or more than 20 for a lower stress recreational route. There needs to be a shared use path that traverses the interchange without level crossings across the ramps as they are unsafe.	
12/16/21 14:38	Alex Pline	alex@teampline.org	point	1639683489859	While the bridge is closed to vehicles, it is still safely passible for cyclists. Many people have created "goat paths" around the barriers. This should be officially allowed and the pedestrian/bike openings through the barriers created as well as the road on the Prince Georges County side cleared of vegetation (it will be closed off in another year or two by overgrowth).	

12/16/21 14:46	Alex Pline	alex@teampine.org	point	1639683997421	There needs to be an above grade crossing of MD 3. The Pinellas Trail in St. Petersburg FL, arguably one of the best urban trails in the country, has many above grade crossings that make it pleasant and contiguous. The most amazing example is where the elevated trail spans multiple arterials and is a good analog for this MD 3 crossing. See: https://goo.gl/maps/av5tkfjEkxMzFZgq8	
12/16/21 14:58			welcome-survey			
12/16/21 15:15			welcome-survey			
12/16/21 15:25	Alex Pline	alex@teampine.org	point	1639686345196	State signed bike route down MD 2 should be removed. This is not an appropriate bike route for *any* cyclist.	
12/16/21 20:03	Neal	leathn@gmail.com	welcome-survey			
12/17/21 5:01	Benjamin Nace	ben2701@verizon.net	welcome-survey			
12/18/21 15:20			route	1639858842507	We really could use sidewalks and a bike lane southbound on Telegraph Rd. Preferably starting at Deson Dr, but at least starting at Minnetonka Rd and going down to the intersection at Reece Rd, and then West on Reece Rd at least to the elementary school. Especially with the three new retail spaces going in at the Telegraph Rd and Reece Rd intersection, it would be nice if we could have a safe way to walk from the Minnetonka Rd neighborhood down to it, the existing shopping center, The McDonald's, and the school.	
12/20/21 13:08			route	1640023727636	This section of MD2 is 6 lanes wide but carries less than 20k vehicles/day. With a reduction in vehicle lanes it could easily accommodate separated bike/ped infrastructure and dedicated transit lanes with room to spare, no need to acquire new right of way. - These improvements would also make the area safer. This area sees a high volume of pedestrian injuries and deaths. - There is also the possibility of a convenient connection to existing B&A Trail at/near MD648.	
12/22/21 14:17			welcome-survey			
12/22/21 14:31	Brian Cahalan	brianc@annapolis.gov	welcome-survey			
12/22/21 14:34	Brian Cahalan	brianc@annapolis.gov	route	1640201658323	Getting from the Southshore trail to Water Works Park needs a 10' dedicated bike/ped path	
12/22/21 14:37	Brian Cahalan	brianc@annapolis.gov	point	1640201825595	Need a bike/ped path from the B&A Trail into St. Johns and also over to the Poplar Trail	
12/27/21 23:28	Stoney Simons	stoney.simons@gmail.com	welcome-survey			
1/6/22 8:32			welcome-survey			
1/6/22 8:33			route	1641475986982	Bicycle access to Riva Park	
1/6/22 8:34			point	1641476074681	It would be nice to be able to get to this park by walking or biking but with a safe protected route	
1/6/22 8:36			route	1641476167042	An Edgewater to Parole walking and biking connection would be nice but it must be safe from the high speed traffic	
1/6/22 10:40			welcome-survey			
1/6/22 13:05			welcome-survey			
1/7/22 13:26			welcome-survey			

1/10/22 9:03			welcome-survey			
1/10/22 10:38			welcome-survey			
1/10/22 10:40	hugh	kelley	welcome-survey			
1/10/22 10:40	hugh	kellye	welcome-survey			
1/10/22 10:45			welcome-survey			
1/10/22 10:46			welcome-survey			
1/10/22 10:51			welcome-survey			
1/10/22 10:54			welcome-survey			
1/10/22 10:54			welcome-survey			
1/10/22 10:57			welcome-survey			
1/10/22 10:58			welcome-survey			
1/10/22 11:00			welcome-survey			
1/10/22 11:00			welcome-survey			
1/10/22 14:09			welcome-survey			
1/10/22 21:21			welcome-survey			
1/10/22 21:29			route	1641868162215	There is an existing shared use path off the road here	
1/10/22 21:32			route	1641868321854	This is an existing county road (three Sisters) that connects to a path to Lake Shore Elementary School and a dirt path on county owned land that could be used to provide a safe path off Mountain Road to Loopers Field, cross North Shore Road, and connect to the Boulevard Park Community.	
1/10/22 21:35			route	1641868508146	Is this at all realistic to have a bike lane proposed on Mountain Road where there is ZERO shoulder or right of way?	
1/10/22 21:37			route	1641868621045	This already exists as a signed bike lane	
1/10/22 21:39			feature	1641868745095		There is no shoulder or right of way here. Is it realistic? I think we should be looking at an alignment off Mountain Road
1/10/22 21:40			route	1641868848311	Possible alignment of a path off Mountain Road	
1/10/22 21:43			route	1641868983421	Possible use of existing streets and rights of way that were never paved to create a route through this neighborhood parallel to Mountain Road, which is a dangerous state highway with little to no shoulder	

1/10/22 21:46			feature	1641869212347		West of Hickory Point Road, there are big problems with lack of shoulder and right of way. This is also a state highway so county has no influence. I do not think anything will ever be built by the state, except possibly east of Hickory Point Road where there is a shoulder. I'd like to see county try and do something south of Mountain Road to the west of Hickory Point, as shown in my other comments.
1/10/22 21:49			route	1641869342300	Possibly create a safe connector between 2 county parks, and also give access to Magothy Beach Road.	
1/11/22 14:00			welcome-survey			
1/17/22 9:41			welcome-survey			
1/17/22 10:13			welcome-survey			
1/17/22 10:17			welcome-survey			
1/17/22 10:17			welcome-survey			
1/17/22 10:18			welcome-survey			
1/17/22 10:18			welcome-survey			
1/17/22 10:18			welcome-survey			
1/17/22 10:19			welcome-survey			
1/17/22 10:21			welcome-survey			
1/17/22 10:31			welcome-survey			
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1/17/22 10:32			welcome-survey			
1/17/22 10:32			welcome-survey			
1/17/22 10:33			welcome-survey			
1/17/22 10:33			welcome-survey			
1/17/22 10:34	hugh	kelley	welcome-survey			
1/17/22 10:37			feature	1642433841323		vilbuiei nsdv
1/17/22 10:37			route	1642433861989	asevs	
1/17/22 10:38			point	1642433887249	zsd aesf	
1/17/22 10:43			draw-response	1642433861989		
1/18/22 14:38			welcome-survey			
1/21/22 16:13			welcome-survey			
1/21/22 16:17			welcome-survey			
1/21/22 17:25			welcome-survey			
1/24/22 10:37			welcome-survey			
1/24/22 11:06			welcome-survey			

1/24/22 13:43			welcome-survey			
1/24/22 14:06			welcome-survey			
1/24/22 14:06			welcome-survey			
1/25/22 8:34			welcome-survey			
1/25/22 8:36			welcome-survey			
1/25/22 11:10			welcome-survey			
1/25/22 12:00			welcome-survey			
1/25/22 12:06			welcome-survey			
1/25/22 12:09			route	1643130582357	It would be great to have a crosswalk added from St Anne's School of Annapolis to the other side of Arundel on the Bay Rd. Currently my kids have to walk up to Hillsmere to use that crosswalk as I won't let them cross this busy road without being in a cross walk. Currently the construction is an issue for them going on their own so I walk them everyday. There are several families in my neighborhood (Blackwalnut Cove) who would use this option to get to school at St Annes.	
1/25/22 12:10	Justin Akers	pwaker20@gmail.com	welcome-survey			
1/25/22 12:11			welcome-survey			
1/25/22 12:12			welcome-survey			
1/25/22 12:13	Diane	dianelbahr@gmail.com	welcome-survey			
1/25/22 12:15	Shelby Towers	mamashel247@yahoo.com	welcome-survey			
1/25/22 12:15			route	1643130948593	A proper walking path down Arundel on the Bay Rd would be AMAZING! There are so many residents along this road that would love to have a safe dedicated path to walk on. It's a beautiful road that leads to a great park. I live in Blackwalnut Cove and my kids would LOVE to be able to ride their bikes to their friends houses in Fishing Creek Farm and Oyster Harbor and to visit their grandparents in Watergate. In my parents community of Watergate there are a lot of retirees who walk a lot, but they do multiple laps of the neighborhood because you can't access proper walking paths further along Arundel on the Bay road. The road is too narrow with no shoulder or sidewalks.	
1/25/22 12:15			welcome-survey			
1/25/22 12:16			welcome-survey			

1/25/22 12:18	Justin Akers	pwaker20@gmail.com	route	1643131102972	I would love to see this area prioritized for a sidewalk down the entire stretch and also a bike lane. There are thousands of townhouses and single family homes recently built and in progress along this stretch. I run on Marley Neck but the deeply sloped narrow shoulder isn't safe for cycling. If the shoulder were widened or the sidewalk ran all the way to the mall (preexisting stretches of walkways do exist) then it would provide access to shopping, B&A trail, light rail and recreation. There isn't much existing infrastructure in the right of way and not much private property which would be disturbed. This would also provide safe travel routes for cyclists/walkers who attend Marley Middle School, Marley Elementary School, Marley Glen Special School, and Calvary Baptist Church Academy.	
1/25/22 12:20			welcome-survey			
1/25/22 12:30			point	1643131829360	It would be nice if there was a bike/walking path from Linthicum Park to the already existing BWI trail. Right now it is not safe to bike/walk from the park to the BWI trail as there is a lot of traffic.	
1/25/22 12:33			point	1643132033028	It would be nice if there was a walking/biking trail that connected the library to the BWI trail. This would make the library more accessible to walkers and bikers.	
1/25/22 12:42			welcome-survey			
1/25/22 12:43			welcome-survey			
1/25/22 12:46			welcome-survey			
1/25/22 12:49			welcome-survey			
1/25/22 12:51			feature	1643133118002		I live in Coldwater Reserve Xing and it would be great if this proposed trail connected to the entrance for Coldwater. Burns Crossing Road doesn't have any shoulder in several places so it's dangerous to try to bike to a trail. You have to ride on the road. It would be very helpful to have a safe way to get to the existing WB&A trail in Odenton. I have to drive my bike there and leave my car in the school parking lot currently.
1/25/22 12:52			welcome-survey			
1/25/22 12:53			point	1643133207430	It would be nice if there way a way to have a walking/biking trail that went by this shopping area similar to how the B&A trail in Severna Park goes by the little shopping plaza that has Big Bean Coffee and other stores. The stores have set up tables outside by the trail so you can sit and eat your food/drink you coffee, etc.	
1/25/22 13:00	Francis D Lamont	hdlamo01@aacounty.org	welcome-survey			

1/25/22 13:02			welcome-survey			
1/25/22 13:06			welcome-survey			
1/25/22 13:07			welcome-survey			
1/25/22 13:17			welcome-survey			
1/25/22 13:29			welcome-survey			
1/25/22 13:36			welcome-survey			
1/25/22 13:39			feature	1643135969457		This is a great connection from Crofton to Annapolis, Bacon Ridge, and Waterworks Park. Regularly used by bicyclists. Some striping or signage would help improve safety of this corridor.
1/25/22 13:41	Megan Chido	meganchido@hotmail.com	welcome-survey			
1/25/22 13:41			feature	1643136095013		Would be great to be able to safely bike from Crofton residential area to commercial areas on Defense. Very narrow shoulder and high traffic currently makes this unsafe.
1/25/22 13:42			feature	1643136138794		South Shore Trail will be a great asset when its completed.
1/25/22 13:43			feature	1643136226035		Super important to make Riva Road more accessible for bikes and walking as this area redevelops into a higher density, mixed use place.
1/25/22 13:48			welcome-survey			
1/25/22 13:51			feature	1643136668119		Mago Vista to College Pkwy and B&A Trail will be a huge connection to allow more people have access to B&A Trail and farmers market on Jones Station Rd. Current College Pkwy biking route along Colleg Pkwy near AACC is very dangerous and current sidewalk is too narrow for bikes from overgrowth
1/25/22 13:52			feature	1643136742427		Belvedere Spur along Jones Station Rd is much needed improvement. Current asphalt path is in poor shape. Intersection of Mago Vista & Jones Station Rd is narrow and not bike friendly

1/25/22 13:53			feature	1643136788702		Bike path along Mago Vista Rd would be huge. Not much space for bikes with current on street parking and patch side walk coverage
1/25/22 13:55			point	1643136939527	Intersection here is narrow for bike traffic here to compete with cars. May require traffic light one day. I end up biking on Ruxhire Dr & Dauntsey Dr to go further down Mago Vista Rd to get towards Belvedere Elementary instead of using this intersection	
1/25/22 13:57			point	1643137034345	Would be nice if the bike path extended down Mago Vista past the school to this park. More people in the area use this park than Belvedere for play before/after school. Lots of kids ride their bikes all the way up & down Mago Vista Rd to go to school or to play at Mago Vista Park and either dodge traffic on the street or use the sidewalk	
1/25/22 14:01			draw-response	1639667772559		
1/25/22 14:15			welcome-survey			
1/25/22 14:15			welcome-survey			
1/25/22 14:23			welcome-survey			
1/25/22 14:25	Hugh	hugh.v.hayes@gmail.com	welcome-survey			
1/25/22 14:27	Hugh	hugh.v.hayes@gmail.com	feature	1643138860146		Please make this a protected bike lane, a painted bike lane on the street is not enough.
1/25/22 14:28	Hugh	hugh.v.hayes@gmail.com	feature	1643138886410		Please make this a protected bike lane, a painted bike lane on the street is not enough.
1/25/22 14:30	Hugh	hugh.v.hayes@gmail.com	feature	1643138987564		I would love a trail here, please build this asap. This road is so dangerous to ride on.
1/25/22 14:30	Hugh	hugh.v.hayes@gmail.com	feature	1643139002323		I would love a trail here, please build this asap. This road is so dangerous to ride on.
1/25/22 14:30	Hugh	hugh.v.hayes@gmail.com	feature	1643139015329		I would love a trail here, please build this asap. This road is so dangerous to ride on.
1/25/22 14:31	Mikayla Ferrand	Mikayla.smith.15@gmail.com	welcome-survey			
1/25/22 14:31	Hugh	hugh.v.hayes@gmail.com	feature	1643139086904		Please make this a protected bike lane, not just a painted bike lane.
1/25/22 14:31	Hugh	hugh.v.hayes@gmail.com	feature	1643139101388		Please make this a protected bike lane, not just a painted bike lane.
1/25/22 14:32	Hugh	hugh.v.hayes@gmail.com	feature	1643139112909		Please make this a protected bike lane, not just a painted bike lane.
1/25/22 14:32	Hugh	hugh.v.hayes@gmail.com	feature	1643139154980		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:32	Hugh	hugh.v.hayes@gmail.com	feature	1643139163637		I would love a trail here, please build this ASAP. Thanks

1/25/22 14:33	Hugh	hugh.v.hayes@gmail.com	feature	1643139175624		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:33	Hugh	hugh.v.hayes@gmail.com	feature	1643139187431		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:33	Hugh	hugh.v.hayes@gmail.com	feature	1643139199767		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:33	Hugh	hugh.v.hayes@gmail.com	feature	1643139210988		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:33	Hugh	hugh.v.hayes@gmail.com	feature	1643139221040		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:34	Hugh	hugh.v.hayes@gmail.com	feature	1643139229812		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:34	Hugh	hugh.v.hayes@gmail.com	feature	1643139237409		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:34	Hugh	hugh.v.hayes@gmail.com	feature	1643139247260		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:34	Hugh	hugh.v.hayes@gmail.com	feature	1643139265056		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:35	Hugh	hugh.v.hayes@gmail.com	feature	1643139295080		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:35	Hugh	hugh.v.hayes@gmail.com	feature	1643139301926		I would love a trail here, please build this ASAP. Thanks
1/25/22 14:35			welcome-survey			
1/25/22 14:36	Hugh	hugh.v.hayes@gmail.com	feature	1643139387874		No one is going to ride on this road just because it is a signed bike route. This should be a continued trail from 665 to the proposed trail on Riva Road.
1/25/22 14:36	Hugh	hugh.v.hayes@gmail.com	feature	1643139399812		No one is going to ride on this road just because it is a signed bike route. This should be a continued trail from 665 to the proposed trail on Riva Road.
1/25/22 14:37	Hugh	hugh.v.hayes@gmail.com	feature	1643139433053		I would love a trail here, please build ASAP. Thanks
1/25/22 14:37	Hugh	hugh.v.hayes@gmail.com	feature	1643139443584		I would love a trail here, please build ASAP. Thanks
1/25/22 14:38	Hugh	hugh.v.hayes@gmail.com	feature	1643139512291		I would love a trail here, please build ASAP. Thanks
1/25/22 14:38	Hugh	hugh.v.hayes@gmail.com	feature	1643139524835		I would love a trail here, please build ASAP. Thanks
1/25/22 14:39	Hugh	hugh.v.hayes@gmail.com	feature	1643139537069		I would love a trail here, please build ASAP. Thanks
1/25/22 14:39	Hugh	hugh.v.hayes@gmail.com	feature	1643139548754		I would love a trail here, please build ASAP. Thanks
1/25/22 14:39	Hugh	hugh.v.hayes@gmail.com	feature	1643139557650		I would love a trail here, please build ASAP. Thanks

1/25/22 14:39	Hugh	hugh.v.hayes@gmail.com	feature	1643139572080		I would love a trail here, please build ASAP. Thanks
1/25/22 14:40	Hugh	hugh.v.hayes@gmail.com	feature	1643139609922		I would love a trail here, please build ASAP. Thanks
1/25/22 14:40	Hugh	hugh.v.hayes@gmail.com	feature	1643139628376		I would love a trail here, please build ASAP. Thanks
1/25/22 14:40	Hugh	hugh.v.hayes@gmail.com	feature	1643139636529		I would love a trail here, please build ASAP. Thanks
1/25/22 14:40	Hugh	hugh.v.hayes@gmail.com	feature	1643139646393		I would love a trail here, please build ASAP. Thanks
1/25/22 14:41	Hugh	hugh.v.hayes@gmail.com	feature	1643139657255		I would love a trail here, please build ASAP. Thanks
1/25/22 14:41	Hugh	hugh.v.hayes@gmail.com	feature	1643139665660		I would love a trail here, please build ASAP. Thanks
1/25/22 14:41	Hugh	hugh.v.hayes@gmail.com	feature	1643139676092		I would love a trail here, please build ASAP. Thanks
1/25/22 14:41	Hugh	hugh.v.hayes@gmail.com	feature	1643139697605		I would love a trail here, please build ASAP. Thanks
1/25/22 14:42	Hugh	hugh.v.hayes@gmail.com	feature	1643139707430		I would love a trail here, please build ASAP. Thanks
1/25/22 14:42	Hugh	hugh.v.hayes@gmail.com	feature	1643139719844		I would love a trail here, please build ASAP. Thanks
1/25/22 14:43	Hugh	hugh.v.hayes@gmail.com	feature	1643139780794		
1/25/22 14:45	Hugh	hugh.v.hayes@gmail.com	route	1643139925594	I would love to see a trail here along King George Street to the Naval Academy Bridge and to the corner of Annapolis Street and Taylor Ave.	
1/25/22 14:45			welcome-survey			
1/25/22 14:50	Hugh	hugh.v.hayes@gmail.com	route	1643140186337	I would like to the B&A Trail extended south, over the Naval Academy Bridge to the intersection of Taylor Ave and Annapolis Street. And a section that also heads down King George Street to the bridge. The B&A Trail is so close to being connected to Annapolis but right now I have to ride/walk in the shoulder of a road with cars driving +50MPH. It is not safe enough for me to bring my kids with me or would I ever allow them to ride on this road by themselves. If the Trail was extended all the way south into Annapolis, it would make this route much safer and open it up to a lot more riders. Thanks	
1/25/22 14:51	Hugh	hugh.v.hayes@gmail.com	route	1643140304972	This should be a Trail.	
1/25/22 14:54	Hugh	hugh.v.hayes@gmail.com	route	1643140449382	Only a few people would ever ride their bike in the road along route 2 because of how fast the cars drive. This needs to be a protected bike lane or a trail to open up this route to more riders trying to get between Annapolis and Edgewater.	
1/25/22 14:55	Hugh	hugh.v.hayes@gmail.com	feature	1643140511805		I would love a trail here. Thanks
1/25/22 14:55	Hugh	hugh.v.hayes@gmail.com	feature	1643140522042		I would love a trail here. Thanks
1/25/22 14:55	Hugh	hugh.v.hayes@gmail.com	feature	1643140532798		I would love a trail here. Thanks
1/25/22 14:56	Hugh	hugh.v.hayes@gmail.com	feature	1643140558805		I would love a trail here. Thanks

1/25/22 14:56	Hugh	hugh.v.hayes@gmail.com	feature	1643140575306		I would love a trail here. Thanks
1/25/22 14:57	Hugh	hugh.v.hayes@gmail.com	feature	1643140606616		
1/25/22 14:57	Hugh	hugh.v.hayes@gmail.com	feature	1643140614845		
1/25/22 14:57	Hugh	hugh.v.hayes@gmail.com	feature	1643140622779		
1/25/22 14:57	Hugh	hugh.v.hayes@gmail.com	feature	1643140631238		
1/25/22 15:03	Hugh	hugh.v.hayes@gmail.com	draw-response	1639667772559		
1/25/22 15:07			draw-response	1640201825595		
1/25/22 15:10			route	1643141410876	The proposed trail along Rowe Blvd should go all the way to the intersection of Bladen Street and College Ave.	
1/25/22 15:12			draw-response	1639686345196		
1/25/22 15:14			feature	1643141628265		Make this a protected bike lane please.
1/25/22 15:15			feature	1643141704385		This needs to be a trail or protected bike lane. This is too busy of a road for just a painted bike lane.
1/25/22 15:15	Anson Liu	ansonvliu@gmail.com	welcome-survey			
1/25/22 15:17	Anson Liu	ansonvliu@gmail.com	feature	1643141846067		Great idea, needed to provide a safe route from Reece Rd neighborhoods to Blue Water blvd grocery location and to odenton waugh chapel location.
1/25/22 15:17	Anson Liu	ansonvliu@gmail.com	feature	1643141854541		Great idea, needed to provide a safe route from Reece Rd neighborhoods to Blue Water blvd grocery location and to odenton waugh chapel location.
1/25/22 15:17			route	1643141863229	Extend the B&A Trail south across the Naval Academy Bridge down Taylor Ave until it connects to the Poplar Trail. Also have the trail go down King George Street to St. Johns College.	
1/25/22 15:18	Anson Liu	ansonvliu@gmail.com	feature	1643141898523		Great idea, currently a dangerous corner for bikes/running and walking.
1/25/22 15:19	Anson Liu	ansonvliu@gmail.com	feature	1643141941855		Great idea, currently no safe way to access the existing bike route along the highway from annapolis rd.
1/25/22 15:20	Anson Liu	ansonvliu@gmail.com	feature	1643141997811		Great idea, would make commuting and biking between Ft Meade, Severn, and Odenton safer and more pleasant without needing to dodge cars at the highway entrance and exit.
1/25/22 15:20			welcome-survey			
1/25/22 15:20			point	1643142006621	Baltimore and Annapolis Trail needs to be extended south into Annapolis. The current setup of riding in the shoulder of the road along side cars driving +50mph is not safe.	

1/25/22 15:21	Anson Liu	ansonvliu@gmail.com	feature	1643142081772		Will allow easy biking and connection from homes outside fort meade to safely travel to the mall.
1/25/22 15:22	Bambi Mejorado	1mejorado1@att.net	welcome-survey			
1/25/22 15:23			route	1643142176428	Extend a Trail from the B&A Trail that heads East along Church Road and connects to the new Broadneck Trail being built along College Pkwy. This would create a safe trail connection to the Broadneck Trail for people coming from the South.	
1/25/22 15:23	Anson Liu	ansonvliu@gmail.com	feature	1643142208607		Great idea, would allow me to run or bike to work at National Business Parkway from Reece Rd. This highway overpass with no bike lane is a main impediment preventing me and many others who work at Fort Meade/NSA from biking to work and forces us to drive a short distance with multiple stop lights and contributing to traffic when we would rather bike or walk.
1/25/22 15:24	Anson Liu	ansonvliu@gmail.com	feature	1643142268559		Would be a good connector so that an exercise loop can be safely completed between annapolis rd, reece rd, and severn rd.
1/25/22 15:24			point	1643142284948	This intersection is horrible for people walking. There are not even crosswalks across all the lanes. Needs to be improved for people walking/biking.	
1/25/22 15:25	Bambi Mejorado	1mejorado1@att.net	feature	1643142314016		Busy road with lots of traffic will bike lane be safe
1/25/22 15:25	Anson Liu	ansonvliu@gmail.com	feature	1643142316226		Much needed connector for allowing residents in Severn to walk to bike to the mall or grocery stores. Currently I have to dodge cars in the dark on this road to get to the Safeway.
1/25/22 15:26	Anson Liu	ansonvliu@gmail.com	feature	1643142384732		Current road has no shoulder for biking or walking, near impossible to walk from Dorsey Rd to Telegraph Rd for exercise.
1/25/22 15:27	Anson Liu	ansonvliu@gmail.com	feature	1643142424906		Good idea, would create a safe exercise and commuting route between BWI and Severn (telegraph rd)
1/25/22 16:08			welcome-survey			
1/25/22 16:11			route	1643145083657	It would be nice to have some type of connection from the Severna Park Area to the South Shore Trail.	

1/25/22 16:26			welcome-survey			
1/25/22 16:33			route	1643146369873	Make this a protected bike lane. Thanks	
1/25/22 16:35			route	1643146529676	Please extended the proposed Trail along Forest Drive all the way to PAL Park. Thanks	
1/25/22 16:35			welcome-survey			
1/25/22 16:42			welcome-survey			
1/25/22 16:43	C gillikin	Cgillikin300@hotmail.com	welcome-survey			
1/25/22 16:44	Jeffrey M Adler	jeff@southernpotteries.com	welcome-survey			
1/25/22 16:45			point	1643147115258	I see pedestrians almost daily on this road with no shoulder or sidewalk, it is a windy, hilly road and traffic goes 40+mph, very dangerous. People also ride bikes here. Is a great direct route from Cape St. Claire to Annapolis but too dangerous for pedestrians and bike riders. Kids could ride bikes to high school if it were safer.	
1/25/22 16:45	John Barbour	barbour.john1@gmail.com	welcome-survey			
1/25/22 16:47	Anne M Asquino	amasquino@gmail.com	welcome-survey			
1/25/22 16:50	Jeffrey M Adler	jeff@southernpotteries.com	route	1643147449239	There is a huge amount of residential development along Marley Neck Blvd. and an opportunity exists to provide safer biking to Marley Station mall and access to the B&A trail.	
1/25/22 16:55	Bill Semenick	bsemenick@verizon.net	welcome-survey			
1/25/22 18:06	Derrick	derrickdavisphotography@gmail.com	welcome-survey			
1/25/22 18:54			welcome-survey			
1/25/22 18:57			feature	1643155055438		I think a lot of people who work in the NBP area live close and would be willing to bike if there were safe trails to bike on. I think this is a great idea!
1/25/22 18:59			welcome-survey			
1/25/22 18:59	Adrian	adrianlopez102779@hotmail.com	welcome-survey			
1/25/22 19:01			welcome-survey			
1/25/22 19:02	Alexander Meller	alexander.meller@gmail.com	welcome-survey			
1/25/22 19:04	Alexander Meller	alexander.meller@gmail.com	route	1643155439420	This is the direct route to get out of Annapolis to the south. It is INCREDIBLY dangerous and high stress and I only use it because detours around it are only slightly less stress/dangerous and add considerable distance to the route.	
1/25/22 19:04			point	1643155434436	This part of West Street is really bad for biking and walking, missing sidewalks. Putting in a trail here would be great.	
1/25/22 19:05	Alexander Meller	alexander.meller@gmail.com	route	1643155546617	This is one of the very few roads in the Parole area that feels safe to ride a bike on. A clean shoulder would be nice.	
1/25/22 19:06			feature	1643155591104		This needs to be a protected bike lane, not just a painted bike lane.
1/25/22 19:09			welcome-survey			
1/25/22 19:10			draw-response	1639667949027		

1/25/22 19:12	Alexander Meller	alexander.meller@gmail.com	route	1643155964653	This is part of the detour to get around route 2 past the Harbour Center Mall, but it is almost as bad as route 2. Going northwest this is uphill, so cyclists either exhaust themselves trying to maintain speed, or risk being hit by inattentive speeding motorists who are more focused on turning into the Town Center Mall, Festival at Riva Mall, or other mall. However, this is the "best" route to Riva Road, which is also terrible for cyclists until out of town, but is the only alternative to route 2 ...	
1/25/22 19:14	Alexander Meller	alexander.meller@gmail.com	route	1643156049083	Not great, but actually a better route to go in this direction than any of the alternatives. Motorists speed here.	
1/25/22 19:14			feature	1643156086137		
1/25/22 19:17	Alexander Meller	alexander.meller@gmail.com	route	1643156245896	Very typical Annapolis... a bike lane/walking path that is short and has terrible connections to anything. This only works for the people who live on this path and have kids going to the school. The exit at the hotel is absurd. This could work if it went all the way from State Circle to north of route 50, so bypassing cycling on West Street.	
1/25/22 19:18	Alexander Meller	alexander.meller@gmail.com	route	1643156300182	This route does not exist, but should.	
1/25/22 19:19	Alexander Meller	alexander.meller@gmail.com	route	1643156377054	A necessary route to get out of Annapolis in this direction, but not safe and not low stress. This is too risky/stressful for a casual cyclist.	
1/25/22 19:20			welcome-survey			
1/25/22 19:22	Alexander Meller	alexander.meller@gmail.com	route	1643156528518	It is ABSURD that there is no route across the Bay open to cyclists. Heat maps of commonly used cycling routes will show significant activity on either side of the bridge, but cyclists are not allowed to cycle on it. This is despite language in the Maryland transpiration law saying the roads are varying users not just cars and trucks.	
1/25/22 19:25	Alexander Meller	alexander.meller@gmail.com	route	1643156710052	This section of College Parkway has been made rather worse for cycling by the shoulder being taken away (where it used to exist) to allow motorists to pass a car turning left. Several sections along this piece are very high stress and dangerous. Sooner or later a cyclist is going to be hit and killed at one of these.	
1/25/22 19:27	Jen	Jnnykm915@gmail.com	welcome-survey			
1/25/22 19:28	Alexander Meller	alexander.meller@gmail.com	route	1643156905723	One again we have a key route out of Annapolis to go west, while horrible, dangerous and stressful when near Parole, there is a narrow shoulder along the section I have highlighted. It is not wide enough and disappears in a few places leaving the cyclist in the lane, in some cases climbing a hill slowly, with speeding motorists.	
1/25/22 19:29		Eliase37@gmail.com	welcome-survey			
1/25/22 19:29	Alexander Meller	alexander.meller@gmail.com	route	1643156982107	Extremely stressful, very dangerous on a bike. But if you want to go north on General's Highway (which has a shoulder most of the time) you have only Bestgate road as an alternative to get there.	
1/25/22 19:32			welcome-survey			

1/25/22 19:32	Alexander Meller	alexander.meller@gmail.com	route	1643157160233	Not for the faint of heart cyclist!! But again, if you want to go west from Annapolis it is this or 450. You can easily be killed on either one. The worst section is eastbound and wanting to turn left onto N Bestgate Rd and then right on Ridgely (lower stress than continuing to Rowe Blvd), but you are climbing a hill, hoping to make it across three lanes to the left turn lane while motorists speed by.	
1/25/22 19:35			route	1643157289784	This could be a potential path to connect the Broadneck Trail to the B&A Trail if the B&A Trail was extended farther south all the way into Annapolis.	
1/25/22 19:36	Alexander Meller	alexander.meller@gmail.com	route	1643157379855	Whoever thought to put a separate bike lane here the way it was done is an IDIOT!!!! The lane is not cleaned out, is essentially blocked at the south eastern end, and the low barrier won't stop a car, but is nearly ideal for catching a bike wheel and crashing you. I will NEVER use this bike lane at the bridge because it doesn't work and is dangerous (I have seen cyclists crash on this). The irony is that the rest of Ridgely Rd, without a bike lane or other treatment, is low traffic enough that it works for cyclists. I just ignore the stupid bike lane at the bridge.	
1/25/22 19:38	Alexander Meller	alexander.meller@gmail.com	route	1643157518411	What goofball specified a full width shoulder for part of Rowe Blvd and eliminated it at the bridges. This is super dangerous for cyclists the cyclist has to move into the traffic lane when approaching the bridge. Probably safer to just take the lane all the way, but that enrages some motorists.	
1/25/22 19:39	Alexander Meller	alexander.meller@gmail.com	route	1643157569054	Shoulder disappears at bridge. Dangerous for cyclists.	
1/25/22 19:42	Alexander Meller	alexander.meller@gmail.com	route	1643157771654	This is a major route for cyclists to get out of Annapolis to the southwest. Parts of Riva have a full width shoulder, but it disappears in places such that cyclists have to use the lane and may surprise motorists when they do so. In particular the narrow shoulder with steel grates at the drains on the bridge across the South River is a hazard for cyclists as they have to climb the crest of the bridge, which slows them. Eastbound motorists want to accelerate as soon as they leave the 30 mph zone on the west side of the bridge.	
1/25/22 19:45	Alexander Meller	alexander.meller@gmail.com	route	1643157907899	You have got to be F'ing kidding me!!! Someone thought a less than full width shoulder/bike lane would work here? I use this because there is almost no alternative when I want to go this direction, but it is TERRIBLE for cyclists. I have yet to see a motorist obeying the speed limit through Edgewater (except during traffic jams) and the "lane" marked with paint is far too narrow. This should be a barrier protected bike lane and rather wider.	
1/25/22 19:47	Alexander Meller	alexander.meller@gmail.com	route	1643158023298	At one point this was almost a reasonable route for cycling. But then part of the shoulder width was taken away and cyclists are dodging the cars in the right lane. For a brief period of time the intersection at 214 and 468 may have actually met standards for all users (so including cyclists). It no longer does. But again, no other safe low stress way to get to 468 and ride south.	

1/25/22 19:48	Alexander Meller	alexander.meller@gmail.com	route	1643158120573	I SHOULD be able to ride this way out to the parks and the water. I rode this once or twice, no more. Far too dangerous with speeding motorists and no suitable shoulder. Also extremely dangerous for walking and there was a fatal hit and run on this section of road in the last 2-3 years.	
1/25/22 19:50	Alexander Meller	alexander.meller@gmail.com	route	1643158218813	Typical BS, put a bike lane in because you can, not because it is needed here. And it is short and completely F'd up at the church entrance, where labeling encourages cyclists to use the right turn lane, WHICH IS ILLEGAL UNDER MARYLAND LAW!!!! What IDIOT drew this up?	
1/25/22 19:53	Alexander Meller	alexander.meller@gmail.com	route	1643158434554	The obvious route out of Annapolis Neck but far too dangerous for cyclists. And sidewalks are incomplete. And there is a school on it. WTF!!!!!! This is an abomination and the county is busy making it worse with development policies. You have a few short sections of alternative adjacent bike path but they do not connect to anything and do not get. you anywhere. USELESS!!!!!!	
1/25/22 19:57			welcome-survey			
1/25/22 20:04	Alexander Meller	alexander.meller@gmail.com	route	1643159074365	There should be a barrier separated bike lane along 665 at least out to route 2 if not further. This is the most direct route out of town in this direction and cyclists seeking to go south could use it to get to the route 2 shoulder.	
1/25/22 20:10			welcome-survey			
1/25/22 20:12			welcome-survey			
1/25/22 20:13			feature	1643159624631		Route should continue north/south piney orchard
1/25/22 20:14			feature	1643159651802		
1/25/22 20:16			welcome-survey			
1/25/22 20:16			feature	1643159803772		
1/25/22 20:28	Marissa	Rissanicole27@gmail.com	welcome-survey			
1/25/22 21:00	Angmcarey@gmail.com	Angmcarey@gmail.com	welcome-survey			
1/25/22 21:08			welcome-survey			
1/25/22 21:09	Joshua Hockenberry	joshuahockenberry@gmail.com	welcome-survey			
1/25/22 21:10	Val	Apollo8719@gmail.com	welcome-survey			
1/25/22 21:11			feature	1643163058551		Can this also be a walking route?
1/25/22 21:14	Kim Linonis	klinonis@gmail.com	welcome-survey			
1/25/22 21:14	Sherris Mundell	smundell71@yahoo.com	welcome-survey			
1/25/22 21:14	Joshua Hockenberry	joshuahockenberry@gmail.com	welcome-survey			
1/25/22 21:15	Val	Apollo8719@gmail.com	route	1643163355398	I would like to be able to use this for walking and biking. Please add walkways/ bike lanes along this route	
1/25/22 21:18	Sherris Mundell	smundell71@yahoo.com	point	1643163488343	A part of this route for me would go through Baltimore city near the Royal Farms on Fort Smallwood Road. Is the county willing to partner with the city to repair that road and consider a bike lane that goes over the bridge as well as safety in the industrial area near the Coast Guard Yard?	

1/25/22 21:18	Andrea	alclayville@hotmail.com	welcome-survey			
1/25/22 21:19	Sherris Mundell	smundell71@yahoo.com	welcome-survey			
1/25/22 21:30			welcome-survey			
1/25/22 21:42			welcome-survey			
1/25/22 21:47			welcome-survey			
1/25/22 21:49			welcome-survey			
1/25/22 22:00	Andrew Beyea	andrew.beyea@gmail.com	welcome-survey			
1/25/22 22:31			welcome-survey			
1/25/22 23:14		neals1820@netzero.net	welcome-survey			
1/25/22 23:57	Marvin E Green	marvin.e.green@gmail.com	welcome-survey			
1/26/22 0:13	Marvin E Green	marvin.e.green@gmail.com	feature	1643174001893		Route 250. Perfect. Just what is needed for access between boating community and grocery store. Consider running the segment from Bay Front Rd., south to Happy Harbor bridge, along the shoreline or residential streets - enhancing areas recreational appeal. I've ridden there and there is only one place where one has to go through a gate.
1/26/22 0:16	Marvin E Green	marvin.e.green@gmail.com	feature	1643174210447		Route 248 & 250. Perfect. Just what is needed for access between boating community and grocery store. Consider running the segment from Bay Front Rd., south to Happy Harbor bridge, along the shoreline or residential streets - enhancing areas recreational appeal. I've ridden there and there is only one place where one has to go through a gate.
1/26/22 4:25			welcome-survey			
1/26/22 5:52	Valerie	valerie2lotte@gmail.com	welcome-survey			
1/26/22 5:53	Valerie	valerie2lotte@gmail.com	welcome-survey			
1/26/22 5:55			welcome-survey			
1/26/22 6:08			welcome-survey			
1/26/22 6:25			welcome-survey			
1/26/22 6:26			welcome-survey			
1/26/22 6:29			welcome-survey			
1/26/22 6:32			welcome-survey			

1/26/22 6:33			welcome-survey			
1/26/22 6:34			welcome-survey			
1/26/22 6:38			welcome-survey			
1/26/22 6:39			welcome-survey			
1/26/22 6:42			welcome-survey			
1/26/22 6:42			welcome-survey			
1/26/22 6:47			welcome-survey			
1/26/22 6:48	WAYNE SWEENEY	irishwakems@gmail.com	welcome-survey			
1/26/22 6:50	CAITLIN REISS	cait.reiss@gmail.com	welcome-survey			
1/26/22 6:53			welcome-survey			
1/26/22 6:56	WAYNE SWEENEY	irishwakems@gmail.com	draw-response	1643140449382		
1/26/22 6:58		onemowertime@comcast.net	welcome-survey			
1/26/22 6:58	Nadia	Nadia_alayash@yahoo.com	welcome-survey			
1/26/22 6:59	WAYNE SWEENEY	irishwakems@gmail.com	draw-response	1643158120573		
1/26/22 7:01	Warren Ramsey	warrenramsey@comcast.net	welcome-survey			
1/26/22 7:05			welcome-survey			
1/26/22 7:06			welcome-survey			
1/26/22 7:10			point	1643199048191		
1/26/22 7:15			welcome-survey			
1/26/22 7:16			welcome-survey			
1/26/22 7:17			welcome-survey			
1/26/22 7:17			welcome-survey			
1/26/22 7:17			welcome-survey			
1/26/22 7:19			welcome-survey			
1/26/22 7:22			welcome-survey			
1/26/22 7:23			welcome-survey			
1/26/22 7:24	Mandy Jellerichs	mandyjellerichs@yahoo.com	welcome-survey			
1/26/22 7:24			welcome-survey			
1/26/22 7:25			welcome-survey			
1/26/22 7:29			welcome-survey			
1/26/22 7:31			welcome-survey			
1/26/22 7:35			welcome-survey			
1/26/22 7:37			welcome-survey			
1/26/22 7:38		Robcurry3@gmail.com	welcome-survey			

1/26/22 7:40	Georgie Berkinshaw	gberkinshaw@cbmove.com	welcome-survey			
1/26/22 7:40			feature	1643200833875		I thought this was already in the works. Please build as soon as possible. Thanks.
1/26/22 7:42	Robert W Lawinger	Fudgecity@gmail.com	welcome-survey			
1/26/22 7:42			feature	1643200933834		Critical path to get to B&A trail. Please build. This road as is, is too dangerous to ride without separate trail.
1/26/22 7:45			feature	1643201115527		Needed to get to Marine Corps stadium. If the stadium had secure bike storage (for EBikes), preferably in area that is monitored against theft, the public bus service from the stadium to downtown could be utilized
1/26/22 7:50	Rick Hutzell	rhutzell3441@gmail.com	welcome-survey			
1/26/22 7:51			welcome-survey			
1/26/22 7:53			welcome-survey			
1/26/22 7:54			point	1643201676524	Arundel on the Bay Rd. Needs a bike lane for kids to bike to school and PAL Park safely	
1/26/22 7:54	Rick Hutzell	rhutzell3441@gmail.com	route	1643201690519	A bike lane on on Arundel on the Bay Road would connect Thomas Point Park to Bay Ridge Road.	
1/26/22 7:55			welcome-survey			
1/26/22 8:01			welcome-survey			
1/26/22 8:13			welcome-survey			
1/26/22 8:15	stacey	ttwoshea@comcast.net	welcome-survey			
1/26/22 8:16			welcome-survey			
1/26/22 8:16			welcome-survey			
1/26/22 8:16	stacey	ttwoshea@comcast.net	welcome-survey			
1/26/22 8:17			welcome-survey			
1/26/22 8:17	stacey	ttwoshea@comcast.net	welcome-survey			
1/26/22 8:20			welcome-survey			
1/26/22 8:21			welcome-survey			
1/26/22 8:28	Wendy hickok	Whickok@gmail.com	welcome-survey			
1/26/22 8:33	Cara Moulds	caramoulds@gmail.com	welcome-survey			
1/26/22 8:36			welcome-survey			
1/26/22 8:39	Thomas Wear	thomaswear@gmail.com	welcome-survey			
1/26/22 8:40			welcome-survey			
1/26/22 8:41			welcome-survey			

1/26/22 8:42			welcome-survey			
1/26/22 8:42			welcome-survey			
1/26/22 8:43	Cara Moulds	caramoulds@gmail.com	point	1643204583976	brick sidewalks in desperate need of repair. extremely dangerous to walk on sidewalks. most residents use the middle of the street.	
1/26/22 8:44			welcome-survey			
1/26/22 8:45	Cara Moulds	caramoulds@gmail.com	point	1643204711995	narrow brick sidewalks in need of repairs to level and make safe. Narrow street with heavy traffic at times.	
1/26/22 8:47	David Raser	daveraser@gmail.com	welcome-survey			
1/26/22 8:50			welcome-survey			
1/26/22 8:50	David Raser	daveraser@gmail.com	route	1643205028774	All the townhouses and apartments around the shopping center have nowhere to connect to any bike or walking trail.	
1/26/22 8:50			welcome-survey			
1/26/22 8:52	David Raser	daveraser@gmail.com	route	1643205120218	It would be nice to connect more communities to the WB&A Trail.	
1/26/22 8:53	Manpreet K Singh	mksingh331@gmail.com	welcome-survey			
1/26/22 8:56	Manpreet K Singh	mksingh331@gmail.com	route	1643205363170	Cross walk light would be needed. People speed on this road all the time.	
1/26/22 8:57			welcome-survey			
1/26/22 9:05	Jennifer Dustin	jenniferldustin@gmail.com	welcome-survey			
1/26/22 9:06			welcome-survey			
1/26/22 9:07			welcome-survey			
1/26/22 9:08	Tom Mann	itmann50@aacounty.org	welcome-survey			
1/26/22 9:15	Martha Turner	marthaturner297@yahoo.com	welcome-survey			
1/26/22 9:26	Bill Hays	whays2@verizon.net	welcome-survey			
1/26/22 9:31			welcome-survey			
1/26/22 9:32			welcome-survey			
1/26/22 9:33			welcome-survey			
1/26/22 9:33	Bill Hays	whays2@verizon.net	point	1643207636016	from the observation area to stewart ave is one place that could be designated for walking only. Bikers could complete the airport loop by riding a short distance on the shoulder of dorsey rd .	
1/26/22 9:34	JASON SPITZER	Jason.spitzer@gmail.com	welcome-survey			
1/26/22 9:35			feature	1643207712802		Mountain Rd needs sidewalks. There have been multiple pedestrian deaths on this road!
1/26/22 9:39			welcome-survey			
1/26/22 9:40	Douglas Potash	stonesdoug@aol.com	welcome-survey			
1/26/22 9:41	Bill Hays	whays2@verizon.net	point	1643208106739	another suggestion for a walking only trail. From the begging of the trail on Dosey rd south to Marley station Mall. The bikers could park at Marley station mall parking lot and ride south bound from there all the way to Annapolis.	
1/26/22 9:49	Todd Price	toddprice67@gmail.com	welcome-survey			
1/26/22 9:51			welcome-survey			

1/26/22 9:54	Todd Price	toddprice67@gmail.com	feature	1643208883104		Defense Hwy has heavy traffic, lots of curves and high speeds. It needs to be widened to have dedicated bike lanes. Signage and sharrows would not be enough to make me feel safe biking here.
1/26/22 9:56			route	1643208963775		
1/26/22 10:04	Todd Price	toddprice67@gmail.com	draw-response	1639667772559		
1/26/22 10:04			route	1643209489308	I love running this route, but there's no protection against speeding cars.	
1/26/22 10:05			welcome-survey			
1/26/22 10:08	Joshua E Falk	joshfalk@gmail.com	welcome-survey			
1/26/22 10:10			welcome-survey			
1/26/22 10:11			point	1643209866519	I think Robinson Road is one of the most dangerous road crossings on the B&A Trail. Vehicles regularly speed on this stretch of road and often don't seem to care or notice pedestrians or bikes about to cross. There should be some type of traffic calming approaching the crosswalk.	
1/26/22 10:14	Chris Cole	ccolecl@hotmail.com	welcome-survey			
1/26/22 10:18	Brendan Kelly	brendanpatrickkelly@gmail.com	welcome-survey			
1/26/22 10:19			welcome-survey			
1/26/22 10:19	Chris Cole	ccolecl@hotmail.com	draw-response	1643147115258		
1/26/22 10:22	Karin Meyers	Kgmeyers@comcast.net	welcome-survey			
1/26/22 10:24			welcome-survey			
1/26/22 10:27			welcome-survey			
1/26/22 10:34	William White	wawhite4@comcast.net	welcome-survey			
1/26/22 10:35	Christa Dittmar	dittmars9@gmail.com	welcome-survey			
1/26/22 10:37			welcome-survey			
1/26/22 10:43	William White	wawhite4@comcast.net	draw-response	1643140449382		
1/26/22 10:45	Lis	elisabethengle@gmail.com	welcome-survey			
1/26/22 10:47			point	1643212027692	A safe crossing at this location to access schools and parks. Support implementation of roundabout design.	
1/26/22 10:47	Lis	elisabethengle@gmail.com	point	1643212044444	Bus stop. There is very minimal lighting here and I find that I have to be standing and waving at the bus driver for them to even notice me. Lighting would be amazing, but a bus stop that isn't right on the circle would also be ideal. I have missed the bus a few times because drivers turn right on red (legal) and don't even look at the stop. It is significantly worse in the winter/in the dark.	
1/26/22 10:47			welcome-survey			
1/26/22 10:48			route	1643212109716	Add sidewalk for walking, biking to school or recreation.	

1/26/22 10:48	William White	wawhite4@comcast.net	point	1643212120495	Forest Drive does not have safe bike lanes from Aris T Allen to Bay Ridge. This makes the communities accessed by Forest Drive isolated when it comes to cycling. A dedicated bike path along Forest Drive and connecting to a bike path going south on Rte. 2 would greatly expand cycling opportunities for Annapolis residents both for commuting and for recreation.	
1/26/22 10:49	Lis	elisabethengle@gmail.com	point	1643212174155	Bus stop and cross walk. There needs to be more lighting here! Even when following cross walk laws and waiting for lights, cars won't see me so they almost hit me. I should not be fearing for my life while crossing a street. I highly encourage anyone to try to cross West and then Chinquapin around 6:30pm at night and see the mess that occurs. The traffic getting onto 665 causes major back ups and makes crossing super unsafe, in addition to the poor lighting.	
1/26/22 10:49			route	1643212183283	Add sidewalk for access to school and recreation.	
1/26/22 10:50	William White	wawhite4@comcast.net	welcome-survey			
1/26/22 10:50	William White	wawhite4@comcast.net	welcome-survey			
1/26/22 10:52			route	1643212346394	Add path for bike ped access to retail and recreation.	
1/26/22 10:53			route	1643212427008	Add sidewalk for access to retail, transit and recreation.	
1/26/22 10:54			welcome-survey			
1/26/22 10:56	William White	wawhite4@comcast.net	point	1643212594248	There is currently little to no safe option for cycling after one reaches the intersection of Spa Rd. when heading west on Hilltop Lane. A dedicated bike path extending to Forest Drive and connecting to a dedicated bike path on Forest Dr. would greatly expand cycling opportunities for commuting and recreation for Eastport residents.	
1/26/22 10:56			route	1643212609901	Sidewalk for access to recreation and retail.	
1/26/22 10:57			point	1643212634316	Safe crossing to B&A	
1/26/22 11:00	William White	wawhite4@comcast.net	draw-response	1643157907899		
1/26/22 11:01	William White	wawhite4@comcast.net	draw-response	1643140449382		
1/26/22 11:03	William White	wawhite4@comcast.net	feature	1643213031323		A trail connecting Rt. 2 with Forest Dr. would be fantastic. Please make this a priority.
1/26/22 11:04	Mastin T. Fowler	mastodfow@gmail.com	welcome-survey			
1/26/22 11:08			welcome-survey			
1/26/22 11:18			welcome-survey			
1/26/22 11:26			point	1643214381031		
1/26/22 11:28			welcome-survey			
1/26/22 11:32			welcome-survey			
1/26/22 11:35			feature	1643214905699		Safe path to education, recreation and work.
1/26/22 11:36	Andrew Smith		welcome-survey			
1/26/22 11:38			route	1643215083894	Add contiguous sidewalk along furnace branch and ordinance to connect BA to Route 2.	
1/26/22 11:40	NANCY COVINGTON		welcome-survey			

1/26/22 11:53	Cortney Gardner	Cortg@comcast.net	welcome-survey			
1/26/22 12:01	Lori Kemp	lkemp775@gmail.com	welcome-survey			
1/26/22 12:04	Lori Kemp	lkemp775@gmail.com	feature	1643216665327		Please build the trail. I see too many runners and bikers on College Parkway. It is unsafe for runners, and questionable for bikers.
1/26/22 12:08	Lori Kemp	lkemp775@gmail.com	feature	1643216891829		Drivers go way too fast on this road. Someone on a bicycle has already died here due to an inattentive motorist. A dedicated lane will provide safe passage for hikers and bicyclists.
1/26/22 12:10			welcome-survey			
1/26/22 12:10	Wil Scott	wscott003@verizon.net	welcome-survey			
1/26/22 12:11	Lori Kemp	lkemp775@gmail.com	feature	1643217113691		Please create another safe way to cross from College Parkway to the B&A trail. Bikers don't want to go a quarter mile in the opposite direction to cross the road.
1/26/22 12:13	Wil Scott	wscott003@verizon.net	feature	1643217209031		This is one of the most dangerous roads in my part of the county. It must have a paved and separated bike lane to attract even experienced cyclists.
1/26/22 12:13	Wil Scott	wscott003@verizon.net	feature	1643217232315		This is one of the most dangerous roads in my part of the county. It must have a paved and separated bike lane to attract even experienced cyclists.
1/26/22 12:14			welcome-survey			
1/26/22 12:15			feature	1643217299159		I live in this area and would use this very regularly (including for shopping), provided there are dedicated bike lanes/safety measures in place. Lots of cars come down this road.
1/26/22 12:15	Wil Scott	wscott003@verizon.net	feature	1643217351789		High speed traffic and heavy volume make this a challenging road. A paved and separated bike lane is needed for increased use.
1/26/22 12:16			feature	1643217359121		Looking forward to completion of the full South Shore Trail!
1/26/22 12:16			welcome-survey			
1/26/22 12:17			point	1643217444253		
1/26/22 12:17			welcome-survey			
1/26/22 12:17			feature	1643217473427		

1/26/22 12:18	Wil Scott	wscott003@verizon.net	feature	1643217506369		This section should utilize the old railroad right of way to separated cyclists from vehicles.
1/26/22 12:19			feature	1643217577708		Would need to widen/expand road to include both dedicated bike lane and sidewalks. Very fast moving traffic on this stretch of road.
1/26/22 12:20			feature	1643217598127		
1/26/22 12:20	Wil Scott	wscott003@verizon.net	feature	1643217618241		Another dangerous road with heavy traffic. Must have widen shoulders and/or separated bike lane. Improvements should generated increase cycling.
1/26/22 12:20	Valerie Toulotte	valerie2lotte@gmail.com	welcome-survey			
1/26/22 12:20			feature	1643217638515		
1/26/22 12:21			feature	1643217684492		
1/26/22 12:22	Valerie Toulotte	valerie2lotte@gmail.com	point	1643217767366	from 424/rt3 intersection, wide shoulder/bike lane on CONWAY to get to 2 rivers and the rail trail.	
1/26/22 12:23			welcome-survey			
1/26/22 12:25			feature	1643217931434		Would like to see this extend up to WB&A Road to then connect into the BWI trail
1/26/22 12:26	Valerie Toulotte	valerie2lotte@gmail.com	point	1643217982651	a separate, 2 way, bike lane could be installed down rt. 3 to South, Home Depot and North, Waugh Chapel. bicyclists and pedestrians use the fast and slow lane narrow shoulders and it is super dangerous	
1/26/22 12:26			route	1643218002134	There is no bike lane in this part of Generals Highway and no shoulder to bike on. SHA has a sign up that says "Bike Lane Ends" just south of the intersection with Trout Rd.	
1/26/22 12:26	Mastin T. Fowler	mastodfow@gmail.com	draw-response	1643142006621		
1/26/22 12:27			feature	1643218029411		Can't wait for this to be completed!
1/26/22 12:28			welcome-survey			
1/26/22 12:29	Valerie Toulotte	valerie2lotte@gmail.com	point	1643218141451	a bridge crosswalk to get to the shopping centers, Giant, Blue Dolphin, Aldi, and the old Kmart	
1/26/22 12:32	Valerie Toulotte	valerie2lotte@gmail.com	welcome-survey			
1/26/22 12:32			feature	1643218351246		
1/26/22 12:35			welcome-survey			
1/26/22 12:35			welcome-survey			
1/26/22 12:38			welcome-survey			
1/26/22 12:39			feature	1643218748224		I will use this trail to commute to work on a daily basis when it is completed. The only reason I don't commute by bike now is because biking on parts of Generals Highway are not safe for cyclists. We need a separate trail.

1/26/22 12:39	Crissy Fuentes	crissyfuentes51@gmail.com	welcome-survey			
1/26/22 12:53			welcome-survey			
1/26/22 12:54			welcome-survey			
1/26/22 12:58			welcome-survey			
1/26/22 12:59	Debra Angevine	debangevine@gmail.com	welcome-survey			
1/26/22 12:59	Denise Messineo	denise.messineo@gmail.com	welcome-survey			
1/26/22 13:00			welcome-survey			
1/26/22 13:00	Denise Messineo	denise.messineo@gmail.com	feature	1643220048096		Would be a great route if we had wide shoulders!
1/26/22 13:02			welcome-survey			
1/26/22 13:04	Leslie Price	leslie_reid_price@yahoo.com	welcome-survey			
1/26/22 13:04	Mark Wildonger	markwildonger@gmail.com	welcome-survey			
1/26/22 13:07	Leslie Price	leslie_reid_price@yahoo.com	feature	1643220465417		Traffic on College Parkway can be heavy. A bike lane seems feasible and would help connect with the rest of the bike trails
1/26/22 13:08	L Mantha	mantha6_l@aol.com	welcome-survey			
1/26/22 13:12			welcome-survey			
1/26/22 13:17			welcome-survey			
1/26/22 13:17			welcome-survey			
1/26/22 13:24			welcome-survey			
1/26/22 13:28			welcome-survey			
1/26/22 13:28			welcome-survey			
1/26/22 13:29			welcome-survey			
1/26/22 13:32			welcome-survey			
1/26/22 13:33			welcome-survey			
1/26/22 13:44			welcome-survey			
1/26/22 13:44	Doug Benedict	dougb259@gmail.com	welcome-survey			
1/26/22 13:45			feature	1643222739532		would make it easier for me to get to the Metro station
1/26/22 13:46			feature	1643222793670		would like to be able to connect into the larger bike system in Annapolis
1/26/22 13:47			welcome-survey			
1/26/22 13:48	Doug Benedict	dougb259@gmail.com	route	1643222994243	I can ride the bike trail from Annapolis to work without having to ride on the Veteran's Hwy.	
1/26/22 13:49	Sandra Bishop	sandra.bishop@gmail.com	welcome-survey			

1/26/22 13:50			feature	1643223021725		Would much rather see a bike lane added beside the current road. This proposed path goes through dark and unlit areas, whereas a new lane along route 2 would benefit from street lights. This route also goes through multiple neighborhoods, which increases the potential for opposition or future problems with homeowners.
1/26/22 13:51			welcome-survey			
1/26/22 13:52			point	1643223148604	Community has sidewalks in only the back portion and they abruptly stop until you reach Taylor Ave. Finishing these sidewalk sections would be beneficial for community members.	
1/26/22 14:00	Sandra Bishop	sandra.bishop@gmail.com	route	1643223620881	I travel to Giant from trail and the only options are narrow sidewalk along McKinsey or through the parking lot of Park Plaza. This is also the main route from neighborhoods behind Giant/Kohl's to the bike trail.	
1/26/22 14:01	Sandra Bishop	sandra.bishop@gmail.com	route	1643223697082	I have seen another person on adult trike riding along this stretch heading to shopping center.	
1/26/22 14:03	Sandra Bishop	sandra.bishop@gmail.com	route	1643223818652	Planning to use this route more frequently when new grocery store (Aldi) opens soon. Only option is narrow sidewalk shared with pedestrians. Difficult to navigate on tandem with blind daughter.	
1/26/22 14:05	Sandra Bishop	sandra.bishop@gmail.com	point	1643223935851	No bike racks and property management not responding to requests to be bike friendly.	
1/26/22 14:06	Sandra Bishop	sandra.bishop@gmail.com	point	1643224013188	No bike racks and property management not responding to requests to be bike friendly.	
1/26/22 14:09	Sandra Bishop	sandra.bishop@gmail.com	point	1643224146301	This is a crossing point from the trail on the right to the rear entrance to Park Plaza shopping center. A crosswalk (preferably raised to slow traffic coming into shopping area) would make this safer.	
1/26/22 14:13	Brenna	bemahoney14@gmail.com	welcome-survey			
1/26/22 14:14	Sandra Bishop	sandra.bishop@gmail.com	point	1643224465387	Crossing at this location from the bike route on Jones Station Road to AACC is very awkward because of the 90 degree turn in the sidewalk in the bottom left corner of this satellite picture. Riding on our tandem bike with my blind daughter, we can't cross "like a car" because we don't trigger the signal. To use the "beg button" to get the light to change, we have to ride on the sidewalk/crosswalk and the sharp turn required on the college side of College Parkway is impossible on a tandem and not easy on a regular 2-wheel bike. It was also be difficult for my other disabled daughters to navigate on their recumbent and upright adult trikes.	

1/26/22 14:15	Brenna	bemahoney14@gmail.com	feature	1643224525295		There is a segment from N Lawrence to N Bestgate without any sidewalks or protected walking/biking area and the area is really dangerous with pedestrians given the speed of the cars. I support adding a trail/sidewalk in this location
1/26/22 14:16	Sandra Bishop	sandra.bishop@gmail.com	point	1643224600401	The trail from Jones Station to this crossing point is obscured by tall bushes making it difficult for bikes to check for traffic heading towards College Parkway along Peninsula Farm Rd and equally difficult for drivers to see bikes.	
1/26/22 14:17	Brenna	bemahoney14@gmail.com	feature	1643224673895		Strongly support having more trails added to the Annapolis area
1/26/22 14:20	Brenna	bemahoney14@gmail.com	feature	1643224812199		The intermittent sidewalk coverage along Housley makes it dangerous for pedestrians and vehicles. I am in support of making this continuously covered.
1/26/22 14:20	Sandra Bishop	sandra.bishop@gmail.com	draw-response	1643137034345		
1/26/22 14:23	Sandra Bishop	sandra.bishop@gmail.com	draw-response	1643209866519		
1/26/22 14:25			welcome-survey			
1/26/22 14:29	Corina Tamas	corina.email@gmail.com	welcome-survey			
1/26/22 14:30	Corina Tamas	corina.email@gmail.com	route	1643225418897		
1/26/22 14:36	Bradley W Middleton	bwmiddleton@hotmail.com	welcome-survey			
1/26/22 14:42	Derek	dackyle@yahoo.com	welcome-survey			
1/26/22 14:47			welcome-survey			
1/26/22 14:49			feature	1643226581801		This would help a lot more people be able to bike to work and for exercise, by extending this trail. Right now it isn't safe for my daughter to ride as it is too dangerous.
1/26/22 14:50			feature	1643226627249		This connects to many smaller communities and would allow more people to access College Parkway and beyond.
1/26/22 14:53	Catherine Carbullido	09catherine@gmail.com	welcome-survey			
1/26/22 15:00	Catherine Carbullido	09catherine@gmail.com	feature	1643227256412		Creating a safe walk/bike trail that connects to the BWI trail would be great.

1/26/22 15:01	Catherine Carbullido	09catherine@gmail.com	feature	1643227277359		Creating a safe walk/bike trail that connects to the BWI trail would be great.
1/26/22 15:08			welcome-survey			
1/26/22 15:08	Philip J Weiser	pjweiser@verizon.net	welcome-survey			
1/26/22 15:09	Catherine Carbullido	09catherine@gmail.com	feature	1643227743971		We need more greenway trails to connect communities.
1/26/22 15:09	Catherine Carbullido	09catherine@gmail.com	feature	1643227783051		We need more greenway trails to connect communities!
1/26/22 15:11			welcome-survey			
1/26/22 15:12	Danielle Sokol	danicecily@me	welcome-survey			
1/26/22 15:13		jesgerbig8@gmail.com	welcome-survey			
1/26/22 15:16	Chris Williams	5chriscraig@gmail.com	welcome-survey			
1/26/22 15:18	Danielle Sokol	danicecily@me	feature	1643228319474		I support a separated bike and walking path connecting the peninsula to the schools
1/26/22 15:19	Danielle Sokol	danicecily@me	feature	1643228388608		I support separated walking and biking paths to connect the peninsula to "central" Edgewater.
1/26/22 15:20			welcome-survey			
1/26/22 15:20	Danielle Sokol	danicecily@me	feature	1643228433016		I support walking and biking paths in this busy corridor of Edgewater
1/26/22 15:21			welcome-survey			
1/26/22 15:22			welcome-survey			
1/26/22 15:22	Cyndi Murphy	cyndimillermurphy@icloud.com	welcome-survey			
1/26/22 15:23	Cheyenne Dominguez	cheydominguez@gmail.com	welcome-survey			
1/26/22 15:26	Debra Gage	rpgage00@aacounty.org	welcome-survey			
1/26/22 15:29	Cheyenne Dominguez	cheydominguez@gmail.com	point	1643228979785	The corner of Cypress Creek Road and Ritchie HWY is an access point for 360+ homes located in Cypress Creek. In Cypress Creek we have sidewalks. However, once we reach Ritchie HWY, we cannot walk/bike because the sidewalks end. Please add a sidewalk along Ritchie HWY to connect Cypress Creek RD to McKinsey Rod. This would enable thousands of residents to access the shopping center on foot/bike. Also, there is St. John School and Church, and close to access to B&A trail so this intersection is especially important yet overlooked as a potential asset to connectability in our community.	
1/26/22 15:31	Chris Williams	5chriscraig@gmail.com	route	1643229112544	Connecting Odenton (WBA Trail) to BWI (BWI Trail). Maybe not running along side Route 170, but maybe on a side road.	

1/26/22 15:32	Debra Gage	rpgage00@aacounty.org	feature	1643229154020		I support signed bike routes, but what we really need are routes away from vehicle traffic. I would never ride my bike on rt 424. Many places have no shoulder and the vehicles go way too fast.
1/26/22 15:33			welcome-survey			
1/26/22 15:33	Ken Jacobs	Pbbother@gmail.com	welcome-survey			
1/26/22 15:34			welcome-survey			
1/26/22 15:34	Cheyenne Dominguez	cheydominguez@gmail.com	route	1643229274970	Please add a safe walking/biking path or sidewalk. This very short connection but would allow literally thousands of residents in Cypress Creek (over 360 homes) to access the Giant/Kohls shopping plaza on foot or bike. Additionally, walkers and bikers could access St. John's Church and School on foot. A small stretch of safe sidewalk here could significantly reduce traffic issues along this stretch.	
1/26/22 15:35	Ted J Sheils	tsheil01@yahoo.com	welcome-survey			
1/26/22 15:36	Matt	matthew.gibboney@gmail.com	welcome-survey			
1/26/22 15:36	William Scerbo	bilcrab@gmail.com	welcome-survey			
1/26/22 15:37	Luke McDowell	lukemcd@gmail.com	welcome-survey			
1/26/22 15:37	Ken Jacobs	Pbbother@gmail.com	welcome-survey			
1/26/22 15:37	William Scerbo	bilcrab@gmail.com	welcome-survey			
1/26/22 15:37	Debra Gage	rpgage00@aacounty.org	feature	1643229454222		Again, I support a signed bike route because it's better than nothing. What we need are routes that are not on paved roads. I would never ride my bike on rt 408 because of the lack of shoulder, hills, turns, and dappled shade make it difficult to be seen by vehicles.
1/26/22 15:38			welcome-survey			
1/26/22 15:38	Melissa Beaudry		welcome-survey			
1/26/22 15:39	Luke McDowell	lukemcd@gmail.com	feature	1643229583218		This area is a great idea for a trail. The current roads for reaching the existing South Shore trail are much too dangerous if approaching from the south. This trail to connect General's Highway to the South Shore trail near Waterbury road is much needed and a great idea.
1/26/22 15:40			welcome-survey			

1/26/22 15:40	Luke McDowell	lukemcd@gmail.com	feature	1643229643454		There are a lot of bikes on this part of Generals Highway, but parts of the highway don't seem safe because of very narrow shoulders and difficult intersections. Building this part of the South Shore trail is a great idea.
1/26/22 15:40	Ken Jacobs	Pbbother@gmail.com	point	1643229657015	This Road (Sunrise Beech) serves the rather large community of Arden on the Severn. It is the only access to Generals Highway, shopping and other destinations. It needs to have an adequate shoulder on both sides to encourage bicycle use.	
1/26/22 15:41	Matt	matthew.gibboney@gmail.com	route	1643229708016	Bike commuting to National Business Parkway from most South/East is difficult based on the lack of a safe (wide shoulder/bike lane) over the Baltimore-Washington Parkway. I have taken this route, but only at low traffic times and plenty of daylight.	
1/26/22 15:42	Jeanne Ward	jeanneward2005@yahoo.com	welcome-survey			
1/26/22 15:42			route	1643229763349	A designated bike /walking path along this road & through the intersection. Currently there is a wide shoulder	
1/26/22 15:43	Ken Jacobs	Pbbother@gmail.com	welcome-survey			
1/26/22 15:43	William Scerbo	bilcrab@gmail.com	welcome-survey			
1/26/22 15:44	Linda Hassell	Lindahassell38@yahoo.com	welcome-survey			
1/26/22 15:44	Ted J Sheils	tsheil01@yahoo.com	draw-response	1643145083657		
1/26/22 15:45	Ken Jacobs	Pbbother@gmail.com	point	1643229910821	Please consider posting speed limit signs. It is preferred that bicycles to use road, but if they use the trail, speed should be limited. This should be true of all trails that encourage bicycle use.	
1/26/22 15:45	Matt	matthew.gibboney@gmail.com	draw-response	1643217444253		
1/26/22 15:45	Jeanne Ward	jeanneward2005@yahoo.com	route	1643229951903	This road is a beautiful ride but so scary I can't do it.	
1/26/22 15:49			welcome-survey			
1/26/22 15:49	Ted J Sheils	tsheil01@yahoo.com	route	1643230161566	Would like to see River Road widened for a bike lane. It is a scenic road with only 2 or 3 houses fronting the road and would connect Herald Harbor (a community of over 800 homes) to other bike lanes.	
1/26/22 15:51			welcome-survey			
1/26/22 15:53			point	1643230397997	Sidewalks are too narrow to be a bike lane. Traffic too heavy to use road in lieu of bike lane.	
1/26/22 15:54			welcome-survey			
1/26/22 15:55			welcome-survey			
1/26/22 15:56	Linda Hassell	Lindahassell38@yahoo.com	draw-response	1643209866519		
1/26/22 15:59			welcome-survey			
1/26/22 15:59			point	1643230782022	I would like to see this part of the trail connected to the WB&A trail in PG county.	
1/26/22 16:01			point	1643230885925	Adding trail parking somewhere along Conway Rd would be helpful. I currently use the Patuxent Rd. parking lot.	

1/26/22 16:02			route	1643230923951	Bike lanes and other routes should interconnect, so making bike lanes lead to something like the B&A Trail will be helpful for getting around.	
1/26/22 16:04			route	1643231081911	It would be great to connect athletic complexes like Lake Shore to existing bike lanes/paths so they can be reached by bike	
1/26/22 16:04	Jane P. Gurganiou s	janeg98@gmail.com	welcome-survey			
1/26/22 16:09	Ted J Sheils	tsheil01@yahoo.com	route	1643231375534	Crownsville Road and Honeysuckle are lightly trafficed and would make a good bike lane. This would provide a north/south connection between Generals Highway and Defense Highway. The middle of Honeysuckle is also where the Waterworks Bike Trail terminates.	
1/26/22 16:11			feature	1643231526765		This is a very busy intersection and a lot of walkers use the library. A trail is needed.
1/26/22 16:12	Ted J Sheils	tsheil01@yahoo.com	route	1643231565082	This is the main artery of the community and most heavily trafficed and connects most streets with Bonaparte which leads to the beach. There is already enough room here on the eastbound side for a sidewalk.	
1/26/22 16:13			welcome-survey			
1/26/22 16:14			feature	1643231683197		A trial is needed to make the area more safe for pedestrians.
1/26/22 16:19			welcome-survey			
1/26/22 16:19	Mike Ramey	ramey2020@hotmail.com	welcome-survey			
1/26/22 16:19			feature	1643231980618		
1/26/22 16:20	Nate	Nate.nbindustries@gmail.com	welcome-survey			
1/26/22 16:21			feature	1643232079062		
1/26/22 16:21			feature	1643232103030		
1/26/22 16:22			feature	1643232137603		
1/26/22 16:22	Mike Ramey	ramey2020@hotmail.com	feature	1643232148881		Right now E College Parkway is very very dangerous for biking. Narrow road, no shoulders with too much traffic going too fast. Totally support a bike line here. Bike lane also needed for when traffic for the bay bridge backs up here.
1/26/22 16:22	Carrie Zlatos	carrie.fiarman@gmail.com	welcome-survey			
1/26/22 16:23	Mike Ramey	ramey2020@hotmail.com	feature	1643232210944		Support bike lane here. This is a nice bike ride, but the road is narrow and some areas have no shoulders.
1/26/22 16:24	Lucy Bishop	lulibi4@gmail.com	welcome-survey			
1/26/22 16:24			welcome-survey			
1/26/22 16:25	Carrie Zlatos	carrie.fiarman@gmail.com	route	1643232343845	Kids who live in the Hillcreek neighborhood cannot safely cross to Northeast Highschool or Tick Neck Park. The crosswalk is on the opposite side of the road and there's no safe way to access it.	

1/26/22 16:27	Carrie Zlatos	carrie.fiarman@gmail.com	route	1643232438186	Kids who live in the Hillcreek neighborhood cannot safely cross to Northeast Highschool or Tick Neck Park. The crosswalk is on the opposite side of the road and there's no safe way to access it. I often see pedestrians running across traffic to access it and there are ALWAYS accidents at this intersection. it is extremely dangerous. Would also be great to be able to utilize the park and not have to drive there.	
1/26/22 16:28	Lucy Bishop	lulibi4@gmail.com	route	1643232495810	I don't drive because of CP, but would ride my e-trike to commute to work if there was a safe (separated) bike lane along Robinson and Benfield. The sidewalks are too narrow for my trike. This is a route with lots of students that could use safer bike routes too (going to either SPMS or SPHS). I want to be independent, but can't use my e-trike to get to work or shopping (along Ritchie & Mckinsey) safely.	
1/26/22 16:29	Sandra Anne Newman	snewman2@verizon.net	welcome-survey			
1/26/22 16:30	Jim Critchfield	jcritch20@verizon.net	welcome-survey			
1/26/22 16:31	Barbara Shycoff	barbara.shycoff@mac.com	welcome-survey			
1/26/22 16:31	Lucy Bishop	lulibi4@gmail.com	route	1643232689631	I use the bike trail for recreation and would like to use to commute to work, but my work hours aren't limited to daylight hours only. Would like the trail to be available after dark with trail lights for commuting. There are no other safe ways to get from Robinson Road to Jones Station Road by bike.	
1/26/22 16:31	Heidi Daverede	adakian@aol.com	welcome-survey			
1/26/22 16:32	Carrie Zlatos	carrie.fiarman@gmail.com	route	1643232762248	There is no sidewalk to connect the kids in Hillcreek neighbor/Rock Creek Community to Duvall Highway and the high school. Many kids will need this to be able to access the highschool	
1/26/22 16:33	Isabella Shycoff	isabellafirth@mac.com	welcome-survey			
1/26/22 16:34	Kim	kagould@hotmail.com	welcome-survey			
1/26/22 16:34	Lucy Bishop	lulibi4@gmail.com	draw-response	1643142176428		
1/26/22 16:38	Jim Critchfield	jcritch20@verizon.net	feature	1643233098733		This route is currently used quite frequently for bicycle traffic and needs a bicycle lane for safety. There are not many alternate routes through side streets that would allow for safe travel along this route.
1/26/22 16:38	Kim	kagould@hotmail.com	feature	1643233126136		This is a high-need area for more bike-friendly traffic, especially for traffic going to/from the Naval Academy Bridge.
1/26/22 16:39	Kim	kagould@hotmail.com	feature	1643233154554		This is a high-need area for more bike-friendly traffic, especially for traffic going to/from the Naval Academy Bridge.

1/26/22 16:39	Kim	kagould@hotmail.com	feature	1643233192222		This is a high-need area for more bike-friendly traffic, especially for traffic going to/from the Naval Academy Bridge and Downtown.
1/26/22 16:40	Kim	kagould@hotmail.com	feature	1643233211436		This is a high-need area for more bike-friendly traffic, especially for traffic going to/from the Naval Academy Bridge.
1/26/22 16:40	Kim	kagould@hotmail.com	feature	1643233229788		This is a high-need area for more bike-friendly traffic, especially for traffic going to/from the Naval Academy Bridge.
1/26/22 16:41	Heidi Daverede	adakian@aol.com	point	1643233273486	Route 256 is very hilly and curvy. Some stretches have side medians where you can safely walk or bike, but it's not consistent in the stretch between route 2 and Route 258. I live on the street and would walk or bike to the park by the elementary school and to the restaurants towards Deale and Herrington harbor. This is a beautiful stretch of road. Traffic volumes have increased with cars and commercial trucks speeding through. Not safe to walk or bike. If you bike and can't complete the climb up a hill, you could easily get hit by a car if you lose your balance or dismount your bike to walk the hill. Walk-in and biking is not for the faint of heart right now. Living so close to the water, it's a shame you have to use a car to travel 1/4 mile to enjoy the sights and opportunities.	
1/26/22 16:43			welcome-survey			
1/26/22 16:47			welcome-survey			
1/26/22 16:50	Barbara Shycoff	barbara.shycoff@mac.com	point	1643233825615	It is very dangerous trying to get from the city of Annapolis to the pool on Reva Road by bicycle along this stretch of Reva Road. Between the exit ramps, overpass, and traffic pattern around where 665 comes into Reva Road, I feel like I am taking my life into my hands to try and ride to the pool.	
1/26/22 16:55	Barbara Shycoff	barbara.shycoff@mac.com	draw-response	1639501871462		
1/26/22 17:03			welcome-survey			
1/26/22 17:03	Barbara Shycoff	barbara.shycoff@mac.com	draw-response	1643140449382		
1/26/22 17:04	Barbara Shycoff	barbara.shycoff@mac.com	draw-response	1643142006621		
1/26/22 17:04			feature	1643234687257		I support completing this trail asap
1/26/22 17:06	Pat Caldeira	patxcal@gmail.com	welcome-survey			
1/26/22 17:07			welcome-survey			

1/26/22 17:07			feature	1643234848526		Our children have to travel this route to meet some of their friends. Although the shoulder is wider, it still is a dangerous road as I have seen people passing with cars on both the shoulder and the middle lane.
1/26/22 17:08	Karen Owings	karenowings@verizon.net	welcome-survey			
1/26/22 17:08			feature	1643234935450		This would ease travel to Kinder Park since there is no shoulder on this road and short sight lines
1/26/22 17:10	Barbara Shycoff	barbara.shycoff@mac.com	draw-response	1639667772559		
1/26/22 17:11			feature	1643235079322		This bike lane needs to be longer than shown as the sidewalks, where available, can be quite narrow, especially near to Jumpers Hole.
1/26/22 17:12			welcome-survey			
1/26/22 17:12	Barbara Shycoff	barbara.shycoff@mac.com	draw-response	1643230782022		
1/26/22 17:13			feature	1643235199216		It would be nice t have a bike path from Severna Park to Odenton. There are no good routes at this time.
1/26/22 17:15		cathywilsonhair@verizon.net	welcome-survey			
1/26/22 17:20	Ed Caldeira	Ed@firsttimequality.com	welcome-survey			
1/26/22 17:28	Jacob Osborne	jacob.osborne1990@gmail.com	welcome-survey			
1/26/22 17:29	Darla Powell	Darlapal@aol.com	welcome-survey			
1/26/22 17:34			welcome-survey			
1/26/22 17:34	Kristen Franklin	kristenjfranklin@gmail.com	welcome-survey			
1/26/22 17:35	Kristen Franklin	kristenjfranklin@gmail.com	welcome-survey			
1/26/22 17:36			welcome-survey			
1/26/22 17:38	Kristen Franklin	kristenjfranklin@gmail.com	route	1643236737306		This route is very busy and there are no walking paths or bike lanes. It is also near an elementary school and very few kids can walk there even though it may be in walking distance
1/26/22 18:06			welcome-survey			
1/26/22 18:06			welcome-survey			
1/26/22 18:11		Jthmd11@gmail.com	welcome-survey			
1/26/22 18:14		Jthmd11@gmail.com	welcome-survey			
1/26/22 18:15		Jthmd11@gmail.com	route	1643238936602		
1/26/22 18:15		Jthmd11@gmail.com	welcome-survey			
1/26/22 18:16		Jthmd11@gmail.com	route	1643239005746		
1/26/22 18:47			welcome-survey			

1/26/22 18:54	Anna Aho	aho.alb@gmail.com	welcome-survey			
1/26/22 18:57			welcome-survey			
1/26/22 19:01			route	1643241705383		
1/26/22 19:01	Anna Aho	aho.alb@gmail.com	route	1643241710191	A bit scary, especially with kids. Traffic moves quickly over the bridge and drivers are impatient with riders on the road. Unsure if we should ride on the sidewalk over the bridge?	
1/26/22 19:04			route	1643241873830	This route connects from Homewood Germantown to the Annapolis end of the B&A Trail. It would be nice to connect where the bike lane over the Navy bridge ends to the Poplar Trail. It can be dangerous on Taylor Ave. due to the narrow road and speeding vehicles.	
1/26/22 19:08	Annette Findlay	annette.findlay@gmail.com	welcome-survey			
1/26/22 19:08			route	1643242107644	If the poplar trail could be linked to water works park from the Homewood Germantown area - then riders could safely use the trails there for hiking or mountain biking. It would be ideal for mountain biking because then you could use the Bacon Ridge connection. Parking at Bacon Ridge is a disaster (as of 2021). If users could Mountain Bike from more neighborhoods (Homewood please!) - then it would alleviate the parking limitations at Waterworks Park and Bacon Ridge.	
1/26/22 19:10	Annette Findlay	annette.findlay@gmail.com	feature	1643242225930		The route is fine, but I don't understand where you propose to put the bike lane? The shoulder in the picture is way too narrow and too close to traffic
1/26/22 19:12	Douglas Walcutt	doug.walcutt@gmail.com	welcome-survey			
1/26/22 19:12	Annette Findlay	annette.findlay@gmail.com	feature	1643242374463		again, I can't tell where the bike lane would be and how it would be separated from traffic, especially on Mountain Rd. People drive badly there anyway, often swerving into the shoulder to avoid people turning or otherwise stopped.
1/26/22 19:14	Annette Findlay	annette.findlay@gmail.com	feature	1643242473743		It would be wonderful to have a way to get from the Duval Highway onto this route. Duval doesn't even have sidewalks in most places - nevermind a bike lane!
1/26/22 19:15	Douglas Walcutt	doug.walcutt@gmail.com	feature	1643242535039		love this but i am concerned about the intersection at route 3. i would also be more interested in a safe passage to Waugh chapel shopping area
1/26/22 19:16			welcome-survey			

1/26/22 19:17			route	1643242640573	Quiet Waters park has great paved and unpaved trail areas for biking. It is dangerous biking on spa spa creek and EXCEPTIONALLY dangerous biking on Forest Drive. It would be really great to build a bike lane with physical dividers down Forest Drive. If it connected in from the poplar over to spa then to Forest in a safe way - then more people would use the park area. There is a lot of traffic and business off of Forest. I can't stress enough how important it would be to have a dedicated bike lane WITH physical barrier on forest to make it work. Shared or painted lines will not work or be safely adhered to by motorists.	
1/26/22 19:17	Douglas Walcutt	doug.walcutt@gmail.com	feature	1643242670737		This is perfect except that riding bikes that are the same as the road are dangerous. needs to be clear and separate with no chance of path violation from cars. just facts. i will not ride roads. people make mistakes or feel entitled over bikes.
1/26/22 19:20	Douglas Walcutt	doug.walcutt@gmail.com	feature	1643242807409		same as the odenton passage to waugh chapel. great idea. but make sure that the passage to the shops is super safe and not in a road. cars and bikes generally dont mix well. there are already deaths associated with people crossing rt 3 to get there or home so this is important
1/26/22 19:31	Andreana E.	superfluaslitteras@gmail.com	welcome-survey			
1/26/22 19:32	Douglas Walcutt	doug.walcutt@gmail.com	draw-response	1643217444253		
1/26/22 19:37	Douglas Walcutt	doug.walcutt@gmail.com	draw-response	1643230782022		
1/26/22 19:45	Douglas Walcutt	doug.walcutt@gmail.com	feature	1643244323856		bike lanes don't work. they are violated and then when that happens its bad. the sides lanes always have debris making the potential of slippage or tire problems a certainty. nope - but if this were a separate divided from cars lane - I think it would be magical.
1/26/22 19:49	Douglas Walcutt	doug.walcutt@gmail.com	feature	1643244574410		separate divided lane -yes no to a bike lane , they are magnets for mayhem. but as it relates to a route from piney orchard to wuagh chapel shipping - that is needed... perhaps through the back of odenton park (gorc) is possible.

1/26/22 19:52	Jonathan Wheaton	jonathan@wheaton.space	welcome-survey			
1/26/22 19:54	Douglas Walcutt	doug.walcutt@gmail.com	feature	1643244892746		i have always supported this. its great and we should do this as fast as possible. some of our proposed trails are - dreams for our grandkids. seriously - we have plans that take decades before they are useful for longer trips - so focus on connecting to larger sections of trails are desirable. also connections from living areas to shopping dining and other parks /recreation are desirable.
1/26/22 19:57	Jonathan Wheaton	jonathan@wheaton.space	draw-response	1643217444253		
1/26/22 20:02	Douglas Walcutt	doug.walcutt@gmail.com	feature	1643245378627		support but disagree with design. do not use existing road as a bike path. it needs to be a walk-roll and not just lines that drivers ignore. opps sorry for that concussion, didn't see you. super wide sidewalk and separation from car traffic is better. redo the roads to expand the sidewalk on one side.. somewhat like the Odenton trail. I see people using that all the time in almost the entire length.
1/26/22 20:03	Christy Yeager	christyyeager171@gmail.com	welcome-survey			
1/26/22 20:08			welcome-survey			
1/26/22 20:08	Douglas Walcutt	doug.walcutt@gmail.com	draw-response	1643205120218		
1/26/22 20:10	Michelle	Michelledrob@yahoo.com	welcome-survey			
1/26/22 20:11			route	1643245869120	There are no safe pedestrian crossings in this area between the housing on the east side of route 3 and the shopping area on the west side. Many people cross here but it is not safe. Someone was killed recently trying to cross in this area. Having a shopping center within walking distance of a highly populated residential area with no safe way to cross the street is irresponsible.	
1/26/22 20:13	Douglas Walcutt	doug.walcutt@gmail.com	route	1643246016315	there is an existing trail that runs to gorc perhaps we can go from gorc to the other suggested segment running to waugh chapel. overall this would be a high quality segment that provide for safe passage to the the shopping dining area. and the dog park	
1/26/22 20:13			route	1643246019203	Common route to join two trails. I WISH we could get WB&A marked with sharrows or even better .. a bike lane.	

1/26/22 20:13			route	1643246021931	These biking/walking paths should be connected. The e residences in piney orchard, four seasons and chaplegate do not have safe biking/walk-in access to the shopping center	
1/26/22 20:24			route	1643246641734	The path along Waugh chapel road should continue to the shopping center. Piney orchard, four seasons and chaplegate do not currently have continuous bike/pedestrian access to the shopping center without traveling in the road beside fast moving traffic	
1/26/22 20:26	Jonathan Wheaton	jonathan@wheaton.space	feature	1643246816847		will be a great addition to being able to travel between destinations.
1/26/22 20:27	Jonathan Wheaton	jonathan@wheaton.space	feature	1643246851287		
1/26/22 20:27	Jonathan Wheaton	jonathan@wheaton.space	feature	1643246861670		
1/26/22 20:27	Jonathan Wheaton	jonathan@wheaton.space	feature	1643246871998		
1/26/22 20:28			route	1643246902652	There are no safe pedestrian crossings in this area. People cross route 3 from the housing on the east side to get to work or to shop on the west side all the time but it is not safe. A pedestrian was killed here recently. Having a large shopping center within walking distance of a highly populated neighborhood without any safe places to cross route 3 is irresponsible.	
1/26/22 20:28			welcome-survey			
1/26/22 20:28	Jonathan Wheaton	jonathan@wheaton.space	feature	1643246921140		Keeping a clear lane for bikes would make this often traveled route safer for cyclists.
1/26/22 20:31			welcome-survey			
1/26/22 20:37	Jonathan Wheaton	jonathan@wheaton.space	feature	1643247446522		Yes!
1/26/22 20:38	Jonathan Wheaton	jonathan@wheaton.space	feature	1643247488031		
1/26/22 20:38	Jonathan Wheaton	jonathan@wheaton.space	feature	1643247526825		This will be a great addition I will use often when finished!
1/26/22 20:52	Joshua Campbell	lunchboxsenior@gmail.com	welcome-survey			
1/26/22 21:01	CHRISTINE F HINTON	therunningcoach@comcast.net	welcome-survey			
1/26/22 21:02			welcome-survey			
1/26/22 21:04			feature	1643249070795		This road currently is too narrow even to walk without cars having to maneuver out of their lane. A bike lane in each direction is needed.
1/26/22 21:11			feature	1643249474273		Coming off the NA Bridge, there is currently no safe riding option. The road is narrow. The sidewalk is narrow, often with pedestrians and trees/bushes overgrown and impeding the way. Dedicated bike path much needed here.

1/26/22 21:11	Brian Kemmet	briankemmet@gmail.com	welcome-survey			
1/26/22 21:19			welcome-survey			
1/26/22 21:19	Elizabeth Sroka	Libbysroka59@gmail.com	welcome-survey			
1/26/22 21:20			welcome-survey			
1/26/22 21:24	Scott Greenip	sgreenip@yahoo.com	welcome-survey			
1/26/22 21:29	Scott Greenip	sgreenip@yahoo.com	route	1643250554625	This route would connect Crofton to the W,B and A trail that will eventually connect from BWI to DC.	
1/26/22 21:31	Scott Greenip	sgreenip@yahoo.com	route	1643250706946	It would be good to connect Crofton to Bowie with a protected bike lane or separated bike lane on Route 3 and then on Route 450 to Race Track Road.	
1/26/22 21:33	Aaron Klebanoff	adkleba@aol.com	welcome-survey			
1/26/22 21:37	william pickens	wgpickens@hotmail.com	welcome-survey			
1/26/22 21:37	william pickens	wgpickens@hotmail.com	welcome-survey			
1/26/22 21:41	Scott Greenip	sgreenip@yahoo.com	route	1643251291586	A separated and protected bike lane or trail along Route 450 would provide a good route from Crofton to Annapolis that would be safer than the road. Although it has shoulders along most of the route, a separated and protected lane would make this much safer.	
1/26/22 21:44			welcome-survey			
1/26/22 21:47	william pickens	wgpickens@hotmail.com	draw-response	1643207636016		
1/26/22 21:54	Scott Greenip	sgreenip@yahoo.com	route	1643252054991	A trail or separated and protected bike lane route that goes along the Patuxent River along Patuxent River Road then then along Sands Road would provide a very scenic route and could eventually connect with the old Chesapeake Railway rail trail or another trail in Southern MD.	
1/26/22 22:16	Julie Farrell	russianbolero@verizon.net	welcome-survey			
1/26/22 22:20			welcome-survey			
1/26/22 22:22	Julie Farrell	russianbolero@verizon.net	route	1643253765308	There are no sidewalks. This is the only way to walk/bike to Glen Burnie from Brooklyn Park. It is very unsafe. Speed limit for large part of the route is 45 mph for cars, and there are Beltway entrance crossings, where I personally know at least one person who was killed by a car while crossing over it. The sidewalks that do exist are not cleared in winter. The safest way to walk is on the median, which is grassy and/or snowy and not easy to walk on. Also not wheelchair accessible nor safely bikeable. There are no banks in Brooklyn Park and few stores, so we need to be able to walk/bike to Glen Burnie. They closed the Bank of America in Brooklyn also.	
1/26/22 22:30	Julie Farrell	russianbolero@verizon.net	route	1643254219454	There is not a continuous sidewalk or safe walking route from Hammonds Lane turning left onto B&A Blvd to walk to Linthicum from Brooklyn Park to access the stores and North County High School, and St. Phillip Neri. Also if you finished the sidewalk on Hammonds Lane, the kids in Sunnyfield could walk to the middle school and not need a bus since it's a very short walk.	
1/26/22 22:34	Tim Stevens	tmsteve@verizon.net	welcome-survey			

1/26/22 22:37	Tim Stevens	tmsteve@verizon.net	feature	1643254676165		Much needed trail. It is a high use road with limited bike lane/safety area vs. the typical traffic speed and volume. It is a major biking thoroughfare because it is an access to many other routes heading southward in the county.
1/26/22 22:39	Julie Farrell	russianbolero@verizon.net	route	1643254750852	It is not safe to walk from Checkers to Veteran's Memorial Park at Maple Road because there are no sidewalks, and the cars speed along Route 170. Also the Linthicum light rail stations bring criminals to the area so we don't necessarily want to walk near the stations. Also crossing the Beltway is like the wild west there. Maybe Jersey walls could be put up to make a safer walking path? Also they did all that work to put in bike lanes that no one uses. How about a pedestrian lane? Also they never clean the bike lanes so they are full of gravel etc. We feel like they are trying to keep us trapped in Brooklyn Park by having every exit from Brooklyn Park into the rest of Anne Arundel County be unwalkable. We have no desire to walk into Baltimore City due to crime.	
1/26/22 22:46	Tim Stevens	tmsteve@verizon.net	feature	1643255185787		I am very frustrated that the bridge construction to link the WB&A Trail to the trail in PG County is taking so long. The foot-dragging by specific government organizations is as infuriating as it is inexcusable. This is an extremely valuable connector, not only for the newer communities that have sprung up around the area, but for many other communities north and east to interconnect with trail systems going toward Washington, DC - important commuter travel routes (both work and school, notably to the college).

1/26/22 22:55	Tim Stevens	tmsteve@verizon.net	feature	1643255741929		High volume, high speed route with commuter traffic that is often aggressive. Glad there are wide shoulders and plans for additional bike/pedestrian infrastructure. Highly recommend some cross-walks and additional safety measures at the corner of 170 and 174. Have almost been hit several times by cars turning and not paying attention to bicyclists crossing. I am a confident rider, but this is a place I feel uneasy traversing currently.
1/26/22 23:02	Tim Stevens	tmsteve@verizon.net	feature	1643256145957		The route across the 295 overpass definitely needs additional bike/pedestrian infrastructure to make it safe. Anyone who works at or near Ft. Meade and wants to bike knows that there are limited access points, all with high volume and high speed traffic. This area is a choke point that is extremely high risk for vulnerable road users. Highly recommend protected bike/pedestrian lanes, some cross-walks and additional safety measures. Have almost been hit several times by cars entering or exiting 295 as I traverse the overpass. I am a confident rider, but this is a place I feel uneasy riding currently.
1/26/22 23:02		scrchic811@aol.com	welcome-survey			
1/26/22 23:22			welcome-survey			
1/27/22 3:45			welcome-survey			
1/27/22 3:49			point	1643273348913	Please help finish the bridge over the river to connect to the trails in PG county	
1/27/22 5:18			welcome-survey			
1/27/22 5:19			feature	1643278742819		
1/27/22 5:57	Doug Baldrige	doug.raytheon@gmail.com	welcome-survey			
1/27/22 6:58			welcome-survey			
1/27/22 7:33	Catherine	Catherinedea@realterm.com	welcome-survey			
1/27/22 7:48	Carol Mary PATTON	CAROL.M.PATTON@GMAIL.COM	welcome-survey			
1/27/22 8:28			welcome-survey			

1/27/22 8:29			welcome-survey			
1/27/22 9:01	Susan D Flynn	susandurf@gmail.com	welcome-survey			
1/27/22 9:09			welcome-survey			
1/27/22 9:10	Joseph Gorman	joseph10.gorman@gmail.com	welcome-survey			
1/27/22 9:12			welcome-survey			
1/27/22 9:20			point	1643293246431	our community can not walk safely to the shopping center on Rte 175. We need sidewalks on Holiday and Greenwood streets. There may be enough street to make a bike path. This is a arterial walk way for our community Maple Ridge and neighboring community Chapel Gate not to mention Piney Orchard. long overdue infill !!	
1/27/22 9:26			welcome-survey			
1/27/22 9:30			feature	1643293850191		This area could really use some dedicated space for bikes - while there is bike space north of 255 and once you pass the curve by Jesse Jay's the road is narrow with little to no shoulders let alone bike space between those two points.
1/27/22 9:44			welcome-survey			
1/27/22 9:52			welcome-survey			
1/27/22 9:52	LYNDA KROH	lyndakroh@gmail.com	welcome-survey			
1/27/22 10:05	John Cooke	jgcooke@alum.mit.edu	welcome-survey			
1/27/22 10:14	Michael Rulf	Michael.Rulf@yahoo.com	welcome-survey			
1/27/22 10:17	Lisa Eliot	lisae9051@gmail.com	welcome-survey			
1/27/22 10:22	Lisa Eliot	lisae9051@gmail.com	draw-response	1643218141451		
1/27/22 10:23	Lisa Eliot	lisae9051@gmail.com	draw-response	1642433887249		
1/27/22 10:29	Lisa Eliot	lisae9051@gmail.com	draw-response	1643205028774		
1/27/22 10:31	Lisa Eliot	lisae9051@gmail.com	draw-response	1643250554625		
1/27/22 10:33	Lisa Eliot	lisae9051@gmail.com	feature	1643297635611		Safer bike lane 424 up to Waugh Chapel Rd is great - We walk it all the time!
1/27/22 10:35	Lisa Eliot	lisae9051@gmail.com	welcome-survey			
1/27/22 10:35	Lisa Eliot	lisae9051@gmail.com	welcome-survey			
1/27/22 10:35	Lisa Eliot	lisae9051@gmail.com	welcome-survey			
1/27/22 10:52	Christina C Bittner	readonread@verizon.net	welcome-survey			
1/27/22 11:10	Sean Foley	Sean.a.foleyeng@gmail.com	welcome-survey			
1/27/22 11:14			welcome-survey			
1/27/22 11:16			feature	1643300194560		
1/27/22 11:18	Sean Foley	Sean.a.foleyeng@gmail.com	welcome-survey			
1/27/22 11:18			feature	1643300332480		
1/27/22 12:44	Florence AUGust		welcome-survey			
1/27/22 12:47	Florence AUGust		welcome-survey			

1/27/22 14:14			welcome-survey			
1/27/22 14:28			welcome-survey			
1/27/22 14:31			route	1643311912692	would be great if we could bike to the farmers market under the JFX. and/or get to the baltimore bike paths to the north	
1/27/22 14:31			point	1643311918240	This intersection is difficult to navigate as a pedestrian due to number of cars turning at any given time.	
1/27/22 14:33			point	1643312015444	I see a lot of people using the route to bike or walk to and from the train station. This particular route seems like an important route for commuting as well as recreation.	
1/27/22 14:38			point	1643312323604	This is a terrible road with big trucks, bad visibility and a pot hole bumpy railroad crossing. i am sure it is a neither district will bother to fix on the county line location. Please encourage baltimore to add a bike path to the bridge (not realistic) and fix the pot holes on/in the road in front of curtis bay. bad for trucks, cars, bikes and no place to walk at all.	
1/27/22 14:39			point	1643312388924	This is a terrible road with big trucks, bad visibility and a pot hole bumpy railroad crossing. i am sure it is a neither district will bother to fix on the county line location. Please encourage baltimore to add a bike path to the bridge (not realistic) and fix the pot holes on/in the road in front of curtis bay. bad for trucks, cars, bikes and no place to walk at all. hawkins road is terrible,	
1/27/22 14:42			route	1643312572613	would be great if a loop existed. having hawkins pt rd improved with a bike path	
1/27/22 17:16			welcome-survey			
1/27/22 17:18			welcome-survey			
1/27/22 17:29			welcome-survey			
1/27/22 17:29			welcome-survey			
1/27/22 17:30			welcome-survey			
1/27/22 17:33			point	1643322799042	For the proposed college parkway side road it appears the path is on the road. There is no way you can have this type of path on the road or barely on shoulder. Any path needs to be far off the roadway. There are currently extremely close calls of cars/bike contact because road biking is currently being used. Any plan has to have side path like path on college parkway	
1/27/22 18:16	Kristen Heist	kristen.heist@gmail.com	welcome-survey			
1/27/22 18:19	Kristen Heist	kristen.heist@gmail.com	feature	1643325549656		Please complete this trail ASAP!! I would love a safe, convenient route to ride a bike to Sandy Point or to get to the shopping center in Cape Saint Claire.
1/27/22 18:20	Kristen Heist	kristen.heist@gmail.com	feature	1643325615645		Please complete ASAP! The sidewalks by 7-11 aren't ideal for biking.
1/27/22 18:20	Kristen Heist	kristen.heist@gmail.com	feature	1643325649556		Please complete ASAP!

1/27/22 18:22	Kristen Heist	kristen.heist@gmail.com	feature	1643325725000		Please complete this section of trail! It would be wonderful to connect the B&A trail and Sandy Point with a safe trail for running and biking.
1/27/22 18:22	Kristen Heist	kristen.heist@gmail.com	feature	1643325739531		
1/27/22 18:23	Kristen Heist	kristen.heist@gmail.com	feature	1643325803037		
1/27/22 19:21			welcome-survey			
1/27/22 19:54			welcome-survey			
1/27/22 20:25	Peter McGraw	creek50@verizon.net	welcome-survey			
1/27/22 21:47			welcome-survey			
1/27/22 21:49			welcome-survey			
1/27/22 22:16	Joanna Lobl	joannalobl@gmail.com	welcome-survey			
1/27/22 22:36			welcome-survey			
1/27/22 23:06	Rhen Barbacow	rhen.barbacow@gmail.com	welcome-survey			
1/27/22 23:13	Rhen Barbacow	rhen.barbacow@gmail.com	route	1643343199983	A bicycle trail connecting Governor Stone Pkwy to Obrecht Rd so the communities in between can safely get to the B&A trail.	
1/27/22 23:27	Roy Barbacow	r_barbacow2@acs.org	welcome-survey			
1/27/22 23:30	Roy Barbacow	r_barbacow2@acs.org	route	1643344217185	A safe place for my children to ride a bike or run that connects to the Baltimore/Annapolis trail and for me to ride my bike to get a bus to DC.	
1/28/22 0:56			welcome-survey			
1/28/22 0:58	Brandon	brandon.k.buchs@gmail.com	welcome-survey			
1/28/22 1:00	Brandon	brandon.k.buchs@gmail.com	feature	1643349649994		
1/28/22 1:02			welcome-survey			
1/28/22 8:54			welcome-survey			
1/28/22 9:04			welcome-survey			

1/28/22 9:31			route	1643380298467	<p>I absolutely love that there's a MUP connecting Rockenbach to Mapes to allow broad access to Fort Meade by bicycle. That's awesome. However, every time there's any snow of note, the snowplows on 175 shove all of the snow onto the MUP because the MUP is right next to the curb. In addition to being a LOT more snow than what falls naturally, it's all compacted and slushy, so it freezes into a giant brick and takes forever to melt. It would be really helpful if, after the snow stops and the SHA plows clear 175, the county (or whoever) could take a Bobcat or something and clear even a narrow band on the MUP for use. This is especially important because Fort Meade often takes an extra day to clear Rockenbach enough to re-open the Rockenbach gate, making Mapes the only viable entrance. The bike lanes (once they've been cleared, which to SHA's credit has been fairly prompt this year) help a fair bit during off-peak times, but with potentially slick roads presenting extra risk of cars sliding sideways, it feels very risky using them at rush hour in sub-frozen temperatures.</p> <p>Also, the concrete patches over the drain pipes from the road freeze faster and stay frozen much longer. A little salt on those after the snow stops</p>	
1/28/22 9:38			feature	1643380699809		This will be very helpful. Ridge is an important connector between 175 and Arundel mills, but the inconsistent shoulders, single lane in each direction, poor visibility around curves and hills, and high speed make it dicey for cyclists during busy hours. A bike lane will help substantially.
1/28/22 9:38			feature	1643380725360		This will be very helpful. Ridge is an important connector between 175 and Arundel mills, but the inconsistent shoulders, single lane in each direction, poor visibility around curves and hills, and high speed make it dicey for cyclists during busy hours. A bike lane will help substantially.
1/28/22 10:31			welcome-survey			
1/28/22 11:24			welcome-survey			
1/28/22 11:52			welcome-survey			
1/28/22 11:53			welcome-survey			
1/28/22 13:10			welcome-survey			
1/28/22 15:23			welcome-survey			

1/28/22 15:26			route	1643401561997	Making a bike land on B&A Blvd that connects B&A trail; Needs to have safe way to cross Rt 2. Would prefer the B&A bike lane to be separated from road traffic; it would not be safe enough to bike on with family otherwise.	
1/28/22 15:44	Kim Franklin	Board@ArdenCommunity Association.com	welcome-survey			
1/28/22 15:44			route	1643402696634	Separated bike lane or sidewalk for access to shopping on north side of Rt 2	
1/28/22 15:45			route	1643402755170	Bike lane connecting B&A trail to north side Rt 2 shopping	
1/28/22 15:46			route	1643402808239	Bike lane connecting B&A trail to north side Rt 2 shopping	
1/28/22 15:50	Kim Franklin	Board@ArdenCommunity Association.com	route	1643403043151	There are walking trails throughout this approximately 1,700' area between two sections of Arden on the Severn (Section 5 and Section 2). There is no way to safely walk/bike to Section 5 of Arden on the Severn from the other Sections 1-4 causing a huge disconnect within the community. Children can't get to friends in Section 5 and visa versa unless driven by an adult. Adults can't easily walk the dirt trails that can be treacherous. A walk/bike trail would solve this problem. Property between Section 5 and Section 2 is State owned.	
1/28/22 15:59			welcome-survey			
1/28/22 16:24			welcome-survey			
1/28/22 16:38			welcome-survey			
1/28/22 16:48	gary a mentesana	gmentesana@gmail.com	welcome-survey			
1/28/22 16:50			route	1643406613374	Adding a bike lane or sidewalk would better connect the communities along Wilson Rd, Ridgely Ave, and Bestgate Rd to the West Annapolis shopping area. It's currently within walking/biking distance - however doesn't feel safe with the traffic and narrow road. These neighborhoods already have many joggers, walkers, and is dog friendly. Making that area safer to bike/walk would better connect the Annapolis community and would be beneficial to local shops and promote healthy exercise.	
1/28/22 16:51	gary a mentesana	gmentesana@gmail.com	point	1643406669734	The bridge over Joyce Lane has been closed, which restricts our access to the south end of the bike trail, including 450 and Annapolis.	
1/28/22 16:52	gary a mentesana	gmentesana@gmail.com	point	1643406774852	Having a safe way to cross Route 2 and connect the B&A Trail with the Broadneck/College Parkway Trail would be a tremendous improvement for those that bike, run and walk.	
1/28/22 16:55	gary a mentesana	gmentesana@gmail.com	feature	1643406906152		This bike trail would be a huge improvement, given the quality and the numerous cars on the existing road, which does not have a shoulder.
1/28/22 16:55	gary a mentesana	gmentesana@gmail.com	feature	1643406933265		

1/28/22 16:57	gary a mentesana	gmentesana@gmail.com	feature	1643407073336		Having a bike trail along Bestgate Road would be a tremendous improvement and enhance safety for those that bike on a largely non-existent shoulder.
1/28/22 16:58	gary a mentesana	gmentesana@gmail.com	feature	1643407110760		
1/28/22 16:58	gary a mentesana	gmentesana@gmail.com	feature	1643407136024		
1/28/22 16:59	gary a mentesana	gmentesana@gmail.com	feature	1643407165214		
1/28/22 17:42	Lauren Colvin	lecolvin@gmail.com	welcome-survey			
1/28/22 17:47	Lauren Colvin	lecolvin@gmail.com	point	1643410050596	This is a tricky spot as a pedestrian, yet it's connected to the East Coast Greenway.	
1/28/22 17:50	Lauren Colvin	lecolvin@gmail.com	point	1643410214686	There are grates in the shoulder making it scary to ride a bike!	
1/28/22 18:32			welcome-survey			
1/28/22 18:34	Angela M. Hayes	amrlcpa@hotmail.com	welcome-survey			
1/28/22 19:01	Eve Terran	eve.terran@gmail.com	welcome-survey			
1/28/22 19:04			welcome-survey			
1/28/22 19:07	Eve Terran	eve.terran@gmail.com	route	1643414822448	Please let us bicycle and walk on a trail that is set away from the traffic a bit. Let it be beautiful and safe from the cars. Take the model from Columbia, MD. They have walking/bicycle paths that go through parks as well as private property with easements.	
1/28/22 19:10	Eve Terran	eve.terran@gmail.com	route	1643415036659	Trails away from traffic. In the woods. Not on the 214. Easements with private property, like in Columbia, MD	
1/28/22 19:12	Eve Terran	eve.terran@gmail.com	route	1643415170692	see previous comments - off-highway paths	
1/28/22 19:15	Eve Terran	eve.terran@gmail.com	draw-response	1643208963775		
1/28/22 19:17	Eve Terran	eve.terran@gmail.com	route	1643415420558	What an awesome loop this would be!	
1/28/22 19:19	Eve Terran	eve.terran@gmail.com	route	1643415542407	lovely loop	
1/28/22 19:20	Eve Terran	eve.terran@gmail.com	route	1643415629491	Paths that are separate from the highway. Safer, more peaceful, and more beautiful.	
1/28/22 19:21	Eve Terran	eve.terran@gmail.com	route	1643415718323	lovely loop	
1/28/22 19:23	Eve Terran	eve.terran@gmail.com	route	1643415802456	connect the two parks with an easement on the private land between the parks. OR, better yet, for county to acquire some of this land, and expand/connect that parks, before it is all developed.	
1/28/22 19:27			feature	1643416049179		This would really help cyclists north of 32 get to and from Odenton
1/28/22 19:28			feature	1643416090701		This would really help cyclists north of 32 get to south Odenton
1/28/22 19:28			feature	1643416111703		This would really help cyclists north of 32 get to south Odenton
1/28/22 19:36			welcome-survey			
1/28/22 21:03			welcome-survey			
1/28/22 21:47			welcome-survey			

1/28/22 22:33			welcome-survey			
1/28/22 22:35			welcome-survey			
1/28/22 23:30			welcome-survey			
1/28/22 23:33			feature	1643430809019		I wouldn't bike or walk on this route. 198 is so busy
1/29/22 1:37	Donna Merten	dhb_215@hotmail.com	welcome-survey			
1/29/22 1:45			welcome-survey			
1/29/22 2:53	Kim radvan	Kradvan@yahoo.com	welcome-survey			
1/29/22 3:11	Kim radvan	Kradvan@yahoo.com	point	1643443894589	Build a pedestrian/bike tunnel that connects road at some point between the road Hampton inn and extended stay inn is on under Aris T Allen to the Annapolis Harbor Center. This would provide safer access than utilizing Riva rd or RT 2 (especially with all the high density apartment dwelling being built) and currently no way now to get there by bike or walk	
1/29/22 6:15	Dustin Aldridge	dahackr@gmail.com	welcome-survey			
1/29/22 6:56		reeseeyoda@yahoo.com	welcome-survey			
1/29/22 7:04			welcome-survey			
1/29/22 7:05			route	1643457954561	Provides access to the nearest grocery store from the neighborhood. There's already a dirt trail from so much use but then you have to walk on Ritchie Highway to finish the trip because there's no shoulder.	
1/29/22 7:23			welcome-survey			
1/29/22 8:00	Brendan Maltese	brendanmaltese@gmail.com	welcome-survey			
1/29/22 8:59	Claire Peterson	verula@yahoo.com	welcome-survey			
1/29/22 9:03	Tyler Scott	tlscott906@gmail.com	welcome-survey			
1/29/22 9:07	Tyler Scott	tlscott906@gmail.com	feature	1643465236099		This would be a huge benefit to the Broadneck Peninsula. There have already been cyclist casualties on this stretch of road in recent years. Completion of the Broadneck trail (Sandy point to AACC) would offer countless recreational benefits as well as allowing many residents the opportunity to bike to work/school, which is impossible to safely do for most in this area currently. The competed section of the trail has been extremely popular, but it has taken much longer than expected to finish this project in its entirety.

1/29/22 9:08	Claire Peterson	verula@yahoo.com	route	1643465283803	The sidewalk ends halfway down Ventnor Rd. If it was continued so that it actually connected to the main neighborhood it would be much safer for all the children to walk to the Chesapeake cluster of schools as well as for walkers and joggers. It would also be good to have speed humps near bodkin and the CBMS outlet to curb speeding. Sorry for the poorly drawn line I can't figure out how to fix it.	
1/29/22 9:17			welcome-survey			
1/29/22 9:18			welcome-survey			
1/29/22 9:42	Keith Riniker	keithriniker@gmail.com	welcome-survey			
1/29/22 10:01	Steven R Kirk	nitny89@yahoo.com	welcome-survey			
1/29/22 10:04	Steven R Kirk	nitny89@yahoo.com	point	1643468661781	I am eagerly waiting on the connector to the PG county trail system. The addition of this connector would greatly expand the options for weekday cycling that avoids the highly congested roads during the week.	
1/29/22 10:09	Steven R Kirk	nitny89@yahoo.com	point	1643468997793	St. George Barber Rd. is a wonderful road for cycling, but suffers due to the lack of a berm on many of the windy and elevation change sections. Thus it makes it difficult for motorists to find place to safely pass a cyclist, under even the best of circumstances. A small berm would help immensely, and give cyclists a safe place when passed by aggressive drivers.	
1/29/22 10:25			welcome-survey			
1/29/22 11:18	Wil Scott	wscott003@verizon.net	welcome-survey			
1/29/22 11:25	Wil Scott	wscott003@verizon.net	feature	1643473507836		There are adequate shoulders along most of the route from Crownsville Road to Rt. 424, but these should be marked and protected from vehicles. And, the route should be extended to the Annapolis Mall and Rt. 3.
1/29/22 11:28	Wil Scott	wscott003@verizon.net	feature	1643473687883		The section from Rt. 2 to Mayo must be an additional bike lane separated from vehicles, as traffic is too intense and too many drivers in the area can be hostile to cyclists.
1/29/22 11:30	Wil Scott	wscott003@verizon.net	feature	1643473836300		Spa Road can be easily improved to Harness Creek Road with signage.
1/29/22 11:32	Wil Scott	wscott003@verizon.net	feature	1643473958958		Only needs signage to the High's Store. Needs a dedicated bike lane beyond that, as it is narrow and has turns. Also, I have encountered hostile motorists on that tight section.

1/29/22 11:34	Wil Scott	wscott003@verizon.net	feature	1643474046686		Traffic is ok, but the road surface is neglected and dangerously potted. Paving and signage is probably all that is needed to make it safer.
1/29/22 11:36	Wil Scott	wscott003@verizon.net	feature	1643474176091		The south side of the highway is less trafficked and safer. Signage and improved maintenance are needed on both sides.
1/29/22 11:39	Wil Scott	wscott003@verizon.net	feature	1643474355500		There must be a safe, continuous route from the Town Center to Bay Ridge. This would open up commuter and recreational cycling to a large group of county residents.
1/29/22 12:02		michelle@guillermin.com	welcome-survey			
1/29/22 12:04		michelle@guillermin.com	point	1643475846293	Where access from south county to Annapolis becomes difficult.	
1/29/22 15:02			welcome-survey			
1/29/22 16:37	Fred Hutzley	fhutzley@gmail.com	welcome-survey			
1/29/22 16:44	Fred Hutzley	fhutzley@gmail.com	point	1643492672556	the foot/biking path alongside McKinsey Road from Whittler Pkwy to Leelyn Dr which is in extremely poor shape right now.	
1/29/22 16:44	Fred Hutzley	fhutzley@gmail.com	welcome-survey			
1/29/22 16:46	Fred Hutzley	fhutzley@gmail.com	welcome-survey			
1/29/22 16:52	Fred Hutzley	fhutzley@gmail.com	route	1643493160168	a bike new bike path along Leelyn Dr connecting with proposed bike lane on 648 B&A to Lake Waterford (see map) which I think would be helpful connecting the neighborhoods on the east side of Ritch Hwy together. Along with the existing sidewalks on the West side of McKinsey Rd for access to the B&A trail.	
1/29/22 18:08			welcome-survey			
1/29/22 19:27	Kitty		welcome-survey			
1/29/22 19:49			welcome-survey			
1/29/22 19:58			feature	1643504293943		I and my Cape St. Claire neighbors can't wait for this section of the bike path to Rt. 2 to be completed!
1/29/22 20:03			feature	1643504583224		Again, I and my Cape St. Claire neighbors can't wait for this section of the Broadneck Trail to be completed. We'd love to bike to Sandy Point!
1/29/22 20:10			feature	1643505016464		Again, I and my Cape St. Claire neighbors can't wait for this section of the Broadneck Trail to be completed. We'd love to bike to Sandy Point!
1/29/22 20:23		dipietrotwins@aol.com	welcome-survey			

1/29/22 20:30	Erin	forte.erin@gmail.com	welcome-survey			
1/29/22 20:33			welcome-survey			
1/29/22 20:58			welcome-survey			
1/29/22 21:02			route	1643508145547	Almost no sidewalks in the older parts of the neighborhood/ no easy way to get to the trail	
1/29/22 21:43			welcome-survey			
1/29/22 22:43			welcome-survey			
1/29/22 22:46			welcome-survey			
1/29/22 23:22			welcome-survey			
1/29/22 23:25			feature	1643516705930		This would be really helpful!
1/29/22 23:26			feature	1643516767282		This will be wonderful. We love the existing trail, and this would only make it better for biking.
1/30/22 8:07			welcome-survey			
1/30/22 8:23			welcome-survey			
1/30/22 8:26			welcome-survey			
1/30/22 8:29	Ed Caldeira	Ed@firsttimequality.com	welcome-survey			
1/30/22 8:38	Ed Caldeira	Ed@firsttimequality.com	route	1643549895944	This Underwood Road segment needs a shoulder to connect the residential areas with the Crofton High School for walking or biking. It has no sidewalk and inconsistent shoulder.	
1/30/22 8:48	Jennifer Sell	sell.jennifer.e@gmail.com	welcome-survey			
1/30/22 8:50	Jennifer Sell	sell.jennifer.e@gmail.com	route	1643550620225	This is a path that can be taken to the B&A trail, but there is no sidewalk or room in the room for bikes/pedestrians.	
1/30/22 8:52	Jennifer Sell	sell.jennifer.e@gmail.com	route	1643550739315	This is a patch that leads to the B&A trail. Due to issues with visibility and the speed at which cars drive, this is not currently safe for walking or biking.	
1/30/22 8:53	Jennifer Sell	sell.jennifer.e@gmail.com	route	1643550816323	This is a path that leads to the B&A trail but there is little visibility and cars drive very quickly, making it unsafe for cyclists/pedestrians.	
1/30/22 8:56	Jennifer Sell	sell.jennifer.e@gmail.com	route	1643551011713	This is a path that allow people to bike or walk to places of employment.	
1/30/22 8:57	Jennifer Sell	sell.jennifer.e@gmail.com	route	1643551053130	This is a path that would allow people to get to places of employment.	
1/30/22 8:59			welcome-survey			

1/30/22 9:05			feature	1643551546717		I support adding a wide shoulder to this segment of State Rt 424 between Underwood Road and State Rt 214. It is an important biking connector. This is a busy road with no shoulder in most places. Biking requires riding in the travel lane with frequent conflict. Is there proposed improvement "266" other than the segment market on the map?.
1/30/22 9:06	Ed Caldeira	Ed@firsttimequality.com	welcome-survey			
1/30/22 9:12	Ed Caldeira	Ed@firsttimequality.com	route	1643551946253	This segment of Rt 424 needs a walkway and/or shoulder to provide bike and walk access from Crofton High School and residential areas to the Bell Branch Park sports complex. Currently the in no sidewalk and or shoulder in most places.	
1/30/22 9:22	Ed Caldeira	Ed@firsttimequality.com	route	1643552534965	Patuxent River Road needs a wide shoulder on the segment between Rt 424 and Rt 214. It is an important biking connector. This is a busy road with a lot of truck traffic but no shoulder. Biking requires riding in the travel lane with frequent conflict with dump trucks.	
1/30/22 9:24			welcome-survey			
1/30/22 9:24	Ed Caldeira	Ed@firsttimequality.com	route	1643552671036	Patuxent River Road needs a wide shoulder on the segment between Rt 214 and Sands Road. It is an important biking connector. This is a busy road with a lot of truck traffic but no shoulder. Biking requires riding in the travel lane with frequent conflict with dump trucks.	
1/30/22 9:32	Ed Caldeira	Ed@firsttimequality.com	route	1643553133159	Sands Road needs a shoulder on the segment to continue the shoulder as seen the north of this point to the Sands Rd shoulder south of this point. It is an important biking connector. This is a busy road with a lot of truck traffic but no shoulder. Biking requires riding in the travel lane with frequent conflict with dump trucks.	
1/30/22 9:40	Ed Caldeira	Ed@firsttimequality.com	point	1643553621794	A dedicated left turn lane is needed heading north on Rt 2 at Harewood Rd. Biking requires standing in the traffic lane whilst waiting for breaks in busy oncoming traffic. This is a safety risk that can be mitigated with re-engineered intersection. It should be done in such a way that a shoulder remains for northbound biking.	
1/30/22 9:41	Ed Caldeira	Ed@firsttimequality.com	point	1643553714052	A dedicated left turn lane is needed heading north on Rt 2 at Polling House Rd. Biking requires standing in the traffic lane whilst waiting for breaks in busy oncoming traffic. This is a safety risk that can be mitigated with re-engineered intersection. It should be done in such a way that a shoulder remains for northbound biking.	

1/30/22 9:48			feature	1643554118819		I support adding a wide shoulder to State Rt 408 Mt Zion Rd. It is an important biking connector. This is a busy road with no shoulder in most places. Biking requires riding in the travel lane with frequent conflict with motor vehicles that find it difficult to pass without taking chances.
1/30/22 9:55			route	1643554520499	St. Stephens Church Road needs a wide shoulder on the segment between Chesterfield Rd north to Underwood Rd. It is an important biking connector. This is a busy winding road with overflow commuter traffic but no shoulder. Biking requires riding in the travel lane in frequent conflict with motor vehicles.	
1/30/22 9:58			route	1643554687810	Johns Hopkins road needs a wide shoulder on the segment between Riedel Rd east to St. Stephens Church Rd. It is an important biking connector. This is a busy winding road with overflow commuter traffic but no shoulder. Biking requires riding in the travel lane in frequent conflict with motor vehicles.	
1/30/22 10:09			point	1643555341592	Crossing Rt 3 by bike (or walk) at Riedel Rd is impossible (for me). I'd like to use my bike to shop reach recreation trails but shopping is not accessible from east of Rt3	
1/30/22 14:28			welcome-survey			
1/30/22 14:30			point	1643571019230	This is a dangerous intersection to traverse as cars speed up from the light at East West/Pasadena Road to get to the entrance to route 10. The light at East/West and Pasadena Road does not provide a safe way for pedestrians or bicycles to get across the road. Typically anyone from the community has to go to the top of the hill near Route 10 to cross so they can see the cars coming in both directions.	
1/30/22 14:30	Michael Akkerman	michaelakkerman@gmail.com	welcome-survey			
1/30/22 14:35	Michael Akkerman	michaelakkerman@gmail.com	route	1643571358333	Sidewalks were recently added on Rt. 424 in Crofton between Rt. 450 and Rt. 3. This is wonderful and has improved ability of both children and adults to get around in Crofton. Unfortunately, a heavily used athletic field complex at Bell Branch is still only accessible by car. In addition to the already proposed bike lane, which is sorely needed, a side-walk needs to be extended from the intersection of Rts 450/424 to Bell Branch Athletic Complex.	
1/30/22 15:48			welcome-survey			
1/30/22 16:03	Michael Akkerman	michaelakkerman@gmail.com	point	1643576600696	Intersection of Rt. 424 and Rt. 3 is in dire need of pedestrian crossing signals to be able to access businesses on the West side of Rt.3.	
1/30/22 16:35			welcome-survey			
1/30/22 16:40	Julie Kleckley Hummer	jjhummer@verizon.net	welcome-survey			

1/30/22 18:29			welcome-survey			
1/30/22 21:32			welcome-survey			
1/30/22 21:39			welcome-survey			
1/30/22 21:41			feature	1643596862995		There is a lot of traffic here. Bike lines would have to be separated to be safe
1/30/22 21:43			point	1643596979630	Would like a safer spot to cross the road	
1/30/22 21:43			welcome-survey			
1/30/22 21:44			point	1643597061568	Safe way to cross the road	
1/30/22 21:45			welcome-survey			
1/30/22 21:46			point	1643597178006	I would like it to be a park	
1/30/22 22:52	Maria Durand	mariafdurand@gmail.com	welcome-survey			
1/30/22 22:56	Maria Durand	mariafdurand@gmail.com	point	1643601359443	Odenton Recreational Area should be part of this plan. There are no bridges or wooden planks, so when it rains the bottom part of the park is not accessible. it should be more user friendly, there is an uncompleted bridge, trash everywhere, and downed trees.	
1/30/22 22:58	Maria Durand	mariafdurand@gmail.com	route	1643601522408	This is a great area for biking. Why is this not part of this comprehensive plan? There is a lot of land to develop with wooded paths and better kept bike trails.	
1/30/22 23:02	Maria Durand	mariafdurand@gmail.com	route	1643601767431	this park should be connected to the rest of the bike route	
1/31/22 0:30	Gaby	ms.gabrielaroque@gmail.com	welcome-survey			
1/31/22 8:46			welcome-survey			
1/31/22 8:46			feature	1643636776585		
1/31/22 8:46			feature	1643636787816		
1/31/22 8:46			feature	1643636796878		
1/31/22 9:22			welcome-survey			
1/31/22 10:12	Stephen Esmacher	esmachers@yahoo.com	welcome-survey			
1/31/22 10:32	John Criswell	jkcriswell@yahoo.com	welcome-survey			
1/31/22 10:45	Gary Metzler	gmetzler01@gmail.com	welcome-survey			
1/31/22 10:49	Gary Metzler	gmetzler01@gmail.com	feature	1643644158484		This would be great. It will provide access to Odenton and even Annapolis from the Severn Area.
1/31/22 10:53	Gary Metzler	gmetzler01@gmail.com	feature	1643644432530		My long time wish was that I could bike from Severn to Annapolis without riding on our crowded streets with distracted drivers.

1/31/22 11:00	Gary Metzler	gmetzler01@gmail.com	feature	1643644814568		I hope this segment will have a dedicated hiker/biker path. The developer of Parkside should have provided such a path for his residents to get to the shopping center and Fort Meade without having to use the bike lane in MD 175 across his frontage. I don't like being that close to traffic, far too often I have seen drivers drift into the bike lane. I realize that the 8" curb will not keep drivers on the road, but they are much less likely to jump the curb than they are to drive in the bike lane.
1/31/22 11:25			welcome-survey			
1/31/22 12:39	Teresa D. Ellenburg	teresaellenburg@gmail.com	welcome-survey			
1/31/22 12:42	Teresa D. Ellenburg	teresaellenburg@gmail.com	route	1643650985572	No shoulder to connect from West Street to Jennifer Rd.	
1/31/22 12:44	Teresa D. Ellenburg	teresaellenburg@gmail.com	route	1643651079589	Need dedicated biking lane. Sidewalk is inadequate	
1/31/22 12:49	Yvonne Jaicks	Sjaicks@verizon.net	welcome-survey			
1/31/22 13:10			welcome-survey			
1/31/22 13:15			route	1643652911079	Would like bike lane to B&A trail.	
1/31/22 14:10	Jeremy Bailey	jeremybaileymba@gmail.com	welcome-survey			
1/31/22 14:11	Jeremy Bailey	jeremybaileymba@gmail.com	point	1643656309023	A limit or ban of right turns on red lights	
1/31/22 14:12	Jeremy Bailey	jeremybaileymba@gmail.com	point	1643656342802	A ban or limitation of right turns on red	
1/31/22 14:14	Jeremy Bailey	jeremybaileymba@gmail.com	point	1643656487948	This is the grocery store closest to my house that is inaccessible by bike (in a safe manner). The intersection is dangerous.	
1/31/22 14:16	Jeremy Bailey	jeremybaileymba@gmail.com	route	1643656568554	This entire road needs to be opened up for safe biking/walking, as it connects Linthicum to Glen Burnie	
1/31/22 14:17	Jeremy Bailey	jeremybaileymba@gmail.com	point	1643656652891	This is a crosswalk across a busy road, connecting residential with transit. It would be nice if a sign, similar to what you might find in Baltimore City, explained that pedestrians have right of way at crosswalks	
1/31/22 15:33	Michael David Golden	md55golden@yahoo.com	welcome-survey			
1/31/22 16:34	Mickey Martin	michaelpatrickmartin@gmail.com	welcome-survey			
1/31/22 16:55	Rick Paukstitus	rpaukstitus@gmail.com	welcome-survey			
1/31/22 16:58	Rick Paukstitus	rpaukstitus@gmail.com	draw-response	1643553714052		
1/31/22 17:02	Rick Paukstitus	rpaukstitus@gmail.com	draw-response	1643553621794		
1/31/22 17:13	Bonni		welcome-survey			

1/31/22 17:44			welcome-survey			
1/31/22 17:46			feature	1643669206041		No way you can fit it in this area.
1/31/22 17:47			feature	1643669245205		Needs four lanes for traffic, not bikes
1/31/22 17:48			feature	1643669280721		
1/31/22 17:48			feature	1643669315448		Need four lanes for traffic, not bikes.
1/31/22 17:50			welcome-survey			
1/31/22 17:52			feature	1643669571163		A trail would be great. Just have to separate the trail from the road. Worried about how dangerous this trail will be if it is in close proximity to the road.
1/31/22 17:54			feature	1643669665411		A hiker/biker trail leading to/from the Parole area is needed.
1/31/22 17:56			feature	1643669788897		I, along with many others, walk, run, and bike in this area. Any trail added would be better than what currently exists.
1/31/22 17:58			feature	1643669913749		Rt 424 is a great road for biking but right now is very dangerous. Widening the road for a bike line would be nice, however a separate walker/biker path would be fantastic.
1/31/22 18:00			feature	1643670053845		A trail connecting Sandy Point with the B&A trail would be oh-so-nice.
1/31/22 18:02			feature	1643670174175		Just sell the Broadneck Trail idea to a politician seeking office. It will surely garner that individual a slew of votes.
1/31/22 21:04	John Daniel von Kerczek	jvonkerczek@gmail.com	welcome-survey			
1/31/22 21:33			welcome-survey			
1/31/22 21:35			route	1643682949657	would like to be able to walk to family's house with a stroller.	
1/31/22 21:59			welcome-survey			
1/31/22 22:16			welcome-survey			
1/31/22 22:16			feature	1643685408149		
1/31/22 22:18			point	1643685503468	Replace the bridge	
1/31/22 22:21	Christina Martin	Christinakmartin@gmail.com	welcome-survey			
2/1/22 0:21	Keeley West	west.keeley86@gmail.com	welcome-survey			
2/1/22 0:25			welcome-survey			
2/1/22 6:08			welcome-survey			
2/1/22 6:46			welcome-survey			
2/1/22 6:55			welcome-survey			

2/1/22 6:58	Ryan Silverberg	ryan.silverberg@gmail.com	welcome-survey			
2/1/22 7:00	Ryan Silverberg	ryan.silverberg@gmail.com	route	1643716841701	Extremely dangerous with cars. Sidewalks eroded, unstable. Bike and walk lane is so narrow it's about width of person. At points along Arundel Beach Road at curve at Folger there is no bike/walk lane for strollers. I run with daughter here and dodge cars. Please create a protected space.	
2/1/22 7:01	Kelly Parsons		welcome-survey			
2/1/22 7:02	Ryan Silverberg	ryan.silverberg@gmail.com	route	1643716948426	Very unkept and eroded walk/side walk path. This has been covered over by flooding or erosion over the years. Hasn't been paved. The bike/walk path on the road is not present. The addition of speed bumps keeps traffic down, but there isn't room to bike/walk on the road. Need dedicated path.	
2/1/22 7:03	Ryan Silverberg	ryan.silverberg@gmail.com	point	1643717011738	Need extension of sidewalk on north side of Rt 2 portion that extends safely down McKinsey.	
2/1/22 7:04	Ryan Silverberg	ryan.silverberg@gmail.com	point	1643717072077	Need addition of crossing options on south side of Rt 2 at this intersection to cross from Robinson Rd to Leelyn Dr. Currently, only option is on north side Rt 2 and the sidewalk ENDS after crossing over to Leelyn. Extremely dangerous running with daughter in stroller in street as cars turning.	
2/1/22 7:38	Eugenia Robertson	genierobertson@gmail.com	welcome-survey			
2/1/22 7:44	Kathleen Brennan	kb814@hotmail.com	welcome-survey			
2/1/22 8:07			welcome-survey			
2/1/22 8:10	Tracey Wells	traceyw727@gmail.com	welcome-survey			
2/1/22 8:19			welcome-survey			
2/1/22 8:22	Margaret Kaii-Ziegler	mkaii1@umbc.edu	welcome-survey			
2/1/22 8:25	Margaret Kaii-Ziegler	mkaii1@umbc.edu	feature	1643721922271		this road is dangerous. Needs a seperated bike lane.
2/1/22 8:26	Margaret Kaii-Ziegler	mkaii1@umbc.edu	feature	1643722006591		should be usable by runners as well.
2/1/22 8:28	Margaret Kaii-Ziegler	mkaii1@umbc.edu	feature	1643722133132		0more shoulder is needed for bicyclists. The cars on St Margarets go too fast on blind curves.
2/1/22 8:29	Margaret Kaii-Ziegler	mkaii1@umbc.edu	feature	1643722183305		resurfacing is needed.
2/1/22 8:32	Margaret Kaii-Ziegler	mkaii1@umbc.edu	route	1643722352112	This connection is needed, but more shoulder is required,	
2/1/22 8:33	Margaret Kaii-Ziegler	mkaii1@umbc.edu	route	1643722391362		
2/1/22 8:34	Margaret Kaii-Ziegler	mkaii1@umbc.edu	route	1643722463226		
2/1/22 8:36	Margaret Kaii-Ziegler	mkaii1@umbc.edu	route	1643722561422		
2/1/22 8:36	Margaret Kaii-Ziegler	mkaii1@umbc.edu	route	1643722584969		

2/1/22 8:38	Margaret Kaii-Ziegler	mkaii1@umbc.edu	route	1643722725676		
2/1/22 8:42			welcome-survey			
2/1/22 8:43			welcome-survey			
2/1/22 9:13	Mike Gomez	michaeledwardgomez@gmail.com	welcome-survey			
2/1/22 9:30	Diane	dwisbeck@yahoo.com	welcome-survey			
2/1/22 9:31			welcome-survey			
2/1/22 10:18			welcome-survey			
2/1/22 10:21	Kathryn Robinett	kathryn.robinett@gmail.com	welcome-survey			
2/1/22 10:24	Kathryn Robinett	kathryn.robinett@gmail.com	route	1643729049353	Sidewalk - there is no side walk from the elementary school to Whitehurst which is about half a mile away and walking around the bends do not feel safe especially with how quickly people drive on Arundel Beach Rd.	
2/1/22 10:34	Sarah Magnotta	sarahmagnotta@gmail.com	welcome-survey			
2/1/22 10:37	Sarah Magnotta	sarahmagnotta@gmail.com	point	1643729827135	We'd greatly appreciate and utilize a safe path to the B&A Trail from this area.	
2/1/22 11:41			welcome-survey			
2/1/22 11:46		2009hcollins@gmail.com	welcome-survey			
2/1/22 12:51	Joshua Douglas Payne	joshua.payne1025@gmail.com	welcome-survey			
2/1/22 12:54	Joshua Douglas Payne	joshua.payne1025@gmail.com	draw-response	1643717011738		
2/1/22 12:58	Joshua Douglas Payne	joshua.payne1025@gmail.com	point	1643738192164	This crossing at route 2 needs to better accommodate bikes and access to and from the B&A. This should be integrated into a bike path/lane up McKinsey.	
2/1/22 13:01	Joshua Douglas Payne	joshua.payne1025@gmail.com	route	1643738348150	This route to and from the B&A is dangerous and new/improved bike routes/paths should be built.	
2/1/22 13:06	Joshua Douglas Payne	joshua.payne1025@gmail.com	route	1643738659180	Potential location for bike/pedestrian bridge connecting both sides of Route 2 for safety/walkability. Could be connected to B&A with dedicated trails.	
2/1/22 13:06	Christina BITTNER	Readonread@verizon.net	welcome-survey			
2/1/22 13:18			welcome-survey			
2/1/22 13:49		Allison.Doyle@gmail.com	welcome-survey			
2/1/22 14:15			welcome-survey			
2/1/22 16:03	Kathleen Brennan	kb814@hotmail.com	point	1643749431945	This is a dangerous curve with no shoulder. I wish it could be made safer.	
2/1/22 16:09	Kathleen Brennan	kb814@hotmail.com	draw-response	1643468661781		
2/1/22 16:20	Kathleen Brennan	kb814@hotmail.com	draw-response	1643209866519		
2/1/22 16:23	Kathleen Brennan	kb814@hotmail.com	draw-response	1643492672556		
2/1/22 16:50			welcome-survey			
2/1/22 17:12			welcome-survey			
2/1/22 19:00	Lisa	Lisagrube@gmail.com	welcome-survey			

2/1/22 21:38	Erin P Holiday	epholiday@verizon.net	welcome-survey			
2/1/22 21:48	Erin P Holiday	epholiday@verizon.net	point	1643770123973	<p>This section of Patuxent Road and nearby Odenton Road are used by commuters to access the Odenton train station. There are no sidewalks on these road sections. Pedestrians are forced to walk on the road to access the train station. This is dangerous at any time of day due to traffic, but particularly dangerous in the evening hours when it is dark. There is a slight rise on Patuxent Road impeding driver's ability to see oncoming traffic. The situation is more dangerous in the winter when it is dark earlier in the evening and snow conditions force pedestrians to walk further out in the road. Frequently pedestrians walk in the road in the dark when returning home from the train station.</p> <p>One day, someone is going to get hit by a car. It is going to happen.</p> <p>There were some early attempts to build a multiuse path however the path ends in a front yard and has not been maintained.</p> <p>Would be happy to meet in person to review the area and point out the hazards.</p> <p>Thanks ! Erin Holiday 410-903-2167</p>	
2/2/22 6:17			welcome-survey			
2/2/22 6:49			welcome-survey			
2/2/22 12:17			welcome-survey			
2/2/22 12:22	Phil Reynolds	philreynolds60@hotmail.com	welcome-survey			
2/2/22 12:33			draw-response	1643157289784		
2/2/22 12:55			draw-response	1643157289784		
2/2/22 13:05			welcome-survey			
2/2/22 13:12			welcome-survey			
2/2/22 13:30			welcome-survey			
2/2/22 13:32			draw-response	1643142006621		
2/2/22 13:38			point	1643827114807	Extend the B&A Trail all the way into Annapolis.	
2/2/22 14:02	D.		welcome-survey			

2/2/22 14:05	D.		feature	1643828729003		This is potentially the most critical bikeway in the county. It is currently so dangerous, and used so often by bikers from the B&A coming south to Annapolis. Wide shoulders protect bikers north of the USNA bridge on 450, but south of the bridge the lanes narrow tightly. Biker groups often use this area, but a better, wider road - (there is ample room under Perry Circle- to expand this causeway would better serve both bikers and the greater annapolis community that uses this trailway.
2/2/22 14:07	D.		feature	1643828825851		This is potentially the most critical bikeway in the county. It is currently so dangerous, and used so often by bikers from the B&A coming south to Annapolis propert. Wide shoulders protect bikers north of the USNA bridge on 450, but south of the bridge the lanes narrow tightly. Biker groups often use this area, but a better, wider road - (there is ample room for expansion, especially with USNA building the alumni center- to expand this causeway would better serve both bikers and the greater Annapolis community that uses this trailway.
2/2/22 14:08	D.		feature	1643828893928		This road is so narrow, but so beautiful to get out towards Greenbury point. An oft-overlooked area that needs some form of development, widening, or shouldering for the protection of active participants in the county. Connect this trail now!
2/2/22 14:09	D.		feature	1643828949587		

2/2/22 14:11	D.		feature	1643829081053		This area is already highly congested, and gives no benefit to the greater commuting community. Better use of resources would be to pickup from the Poplar trail in Annapolis, and use the former rail right of way and utility lines that connect towards the back of the mall/Trader Joes section of town. Most bikers would not actively use a bike lane along this section of road.
2/2/22 14:12	D.		feature	1643829168333		This area is so congested, that creative designns for bikeways/pedestrians should be considered. Particularly, a walking bridge/archway over the East-West causeways would be more beneficial to the community and a safer use of space.
2/2/22 14:16	D.		point	1643829383882	A connection that crosses off the B&A to greater Arnold would make this exchange much safer	
2/2/22 14:41			welcome-survey			
2/2/22 15:00	Mildred (Millie) Bowers	millie_bowers2@yahoo.com	welcome-survey			
2/2/22 15:03			feature	1643832179291		I see people biking or walking along Rt. 50 overpass, between Cape St. Claire and St. Margarets area. It is not safe but is a potentially highly travelled route by cyclists and even walkers if infrastructure between Cape St. Claire and along St. Margaret's Rd. could be improved and made safe.
2/2/22 15:10	Mildred (Millie) Bowers	millie_bowers2@yahoo.com	point	1643832598566	This is Mitchell's Chance Road I am a senior and ride my bike on Mitchell's Chance Road. I don't feel safe. There is side walks and lots of people with children, dogs etc. are using the side walks (that's not a problem) we need bicycle lanes designated with bicycles symbol painted for our not Speaking English neighbors.	
2/2/22 15:14		BeckySwerdloff@gmail.com	welcome-survey			
2/2/22 15:16	Mildred (Millie) Bowers	millie_bowers2@yahoo.com	point	1643833015158	This is Stepneys Lane which has a bicycle lane but does not have the bicycle symbol painted and not English residents from Woodland Beach ride bicycle on the sidewalks.	
2/2/22 15:18	Joyce Edwards	jjedwards3@verizon.net	welcome-survey			

2/2/22 15:20		BeckySwerdloff@gmail.com	feature	1643833256593		Currently, route 424 is very dangerous for riding a bike. I would love to be able to ride along side of it separated from traffic.
2/2/22 15:26		BeckySwerdloff@gmail.com	feature	1643833582347		
2/2/22 15:32		BeckySwerdloff@gmail.com	feature	1643833968250		Route 424 is too dangerous to ride on since there are no shoulders, and in many cases no where to go but on the road. Cars travel way too fast and it is very busy. I can't see how putting a sign up will make it any safer. It would be much better to make a separate lane for biking. I understand this is a favorite route, but I've seen way too many close calls.
2/2/22 15:47			welcome-survey			
2/2/22 16:35			welcome-survey			
2/2/22 16:39	Sue King	sue.king11@gmail.com	welcome-survey			
2/2/22 16:53	Sue King	sue.king11@gmail.com	feature	1643838793957		Great connections from this route. Is it an oversight that there is not a connection between any of the Bacon Ridge trail head parking areas and proposed bike routes. Also, there is only one trailhead connection to Waterworks park, according to this map. Bike racks would lbe good at the trailheads too so one could ride a bike to it and hike if they aren't mountainbikers. That said, breakins and related crime is a problem at the Bacon trailheads so maybe bike racks wouldn't help.
2/2/22 17:00	Sue King	sue.king11@gmail.com	feature	1643839207454		a lane would be great here and continuing south on connected roads . This and the current connected roads are very hazardous to ride due to car speeds and lack of designated lanes. Some areas to have "share the road" type signs but the addition of sharrow would be helpful.
2/2/22 17:02	Janice Bird	docjlbird@yahoo.com	welcome-survey			
2/2/22 17:51	Ken	kmbjr0@gmail.com	welcome-survey			

2/2/22 17:54	Ken	kmbjr0@gmail.com	feature	1643842491930		creates viable bike connections between residential areas, retail, Kinder Farm Park, and the B&A Trail (which could serve as a larger conduit along the Rt. 2 corridor for bike and pedestrian traffic)
2/2/22 17:55	Ken	kmbjr0@gmail.com	feature	1643842500433		creates viable bike connections between residential areas, retail, Kinder Farm Park, and the B&A Trail (which could serve as a larger conduit along the Rt. 2 corridor for bike and pedestrian traffic)
2/2/22 17:59	Ken	kmbjr0@gmail.com	feature	1643842776185		Creating safe connections between areas of interest and the B&A Trail could initiate the creation of a viable pedestrian and bike transportation network.
2/2/22 18:04	Ken	kmbjr0@gmail.com	feature	1643843098529		Dedicated bike lanes along the length of Benfield Rd/Robinson Rd could greatly impact the amount of congestion along this route. Furthermore, it would create key connection between many residential neighborhoods and the B&A trail. Park and ride area at the intersection of Benfield/I-97 could serve as transition point for commuters.
2/2/22 18:31	Aldana Stanisci	Astanisci09@gmail.com	welcome-survey			
2/2/22 18:54	Mary K Leisure	ladylovage@verizon.net	welcome-survey			
2/2/22 19:08	Mary K Leisure	ladylovage@verizon.net	route	1643846924874	The community would benefit from a safe walking and biking area along this stretch of road. Currently there are no sidewalks and the shoulder is narrow. It is not safe to walk or bike to Lake Shore, CVS the medical facilities or restaurants in the area.	
2/2/22 19:28			welcome-survey			
2/2/22 19:33			welcome-survey			
2/2/22 19:34			welcome-survey			
2/2/22 19:38			welcome-survey			
2/2/22 19:53	Kathleen Nuccetelli	knuccetelli@gmail.com	welcome-survey			
2/2/22 19:55	Kathleen Nuccetelli	knuccetelli@gmail.com	draw-response	1643492672556		

2/2/22 19:55	Kathleen Nuccetelli	knuccetelli@gmail.com	draw-response	1643738192164		
2/2/22 19:56	Kathleen Nuccetelli	knuccetelli@gmail.com	draw-response	1643729827135		
2/2/22 19:57	Kathleen Nuccetelli	knuccetelli@gmail.com	draw-response	1643749431945		
2/2/22 19:57	Kathleen Nuccetelli	knuccetelli@gmail.com	draw-response	1643223935851		
2/2/22 21:18	Sydney Cuccaro	Sydney.cuccaro@gmail.com	welcome-survey			
2/2/22 21:19			welcome-survey			
2/3/22 2:32			welcome-survey			
2/3/22 8:35	Alice	carrpeggy@comcast.net	welcome-survey			
2/3/22 8:43			welcome-survey			
2/3/22 9:29			welcome-survey			
2/3/22 9:32			welcome-survey			
2/3/22 9:33			route	1643898836710		
2/3/22 9:34	Scott Harman	asharman1@verizon.net	welcome-survey			
2/3/22 9:37			route	1643899068654		
2/3/22 9:41			route	1643899281012		
2/3/22 9:42			route	1643899333620		
2/3/22 9:42	Scott Harman	asharman1@verizon.net	point	1643899341432	it's a resident	
2/3/22 10:57	Patrick J McMahon	pmcmahon@mta.maryland.gov	welcome-survey			
2/3/22 10:59	Patrick J McMahon	pmcmahon@mta.maryland.gov	route	1643903986953	Long term goal of a bike/ped bridge over the Patapsco once the BWI Trail has been extended up to Nursery Road and the Patapsco Regional Greenway connects from Patapsco Avenue to the Baltimore Highlands Light Rail Station.	
2/3/22 11:01	Patrick J McMahon	pmcmahon@mta.maryland.gov	feature	1643904090301		Really excited about the effort to extend the BWI Trail up to the North Linthicum and Nursery Road Light Rail Stations. This is a really important regional project and should eventually connect to trail projects in Baltimore County and Baltimore City.
2/3/22 11:02	Patrick J McMahon	pmcmahon@mta.maryland.gov	feature	1643904163689		Really excited about making it safer to ride on B&A Boulevard, particularly the bridge over the Patapsco, and to address the challenging intersection with Camp Meade Road.
2/3/22 11:06	Patrick J McMahon	pmcmahon@mta.maryland.gov	route	1643904364077	Need a good trail connection from the BWI Trail to the WB&A Trail at Odenton. My line on the map didn't go all the way to the WB&A (but was intended to) and I don't know what's the best alignment once you get close to Route 32, but I hope the County will follow up on it's study and make this connection happen.	

2/3/22 11:09	Patrick J McMahon	pmcmahon@mta.maryland.gov	point	1643904570145	The Odenton MARC Station is an important transit hub and also should be a hub for bicycle/walking trail connections. The proximity to the WB&A Trail, South Shore Trail, bike lanes on Town Center Blvd., future WB&A Road Trail, and connections to Fort Meade make it essential that the connections in this area are done well. This site currently has lots of good bike parking, but deserves more truly transit oriented development, with a limited amount of parking provided.	
2/3/22 11:13	Patrick J McMahon	pmcmahon@mta.maryland.gov	point	1643904811063	The Glen Burnie Light Rail stop is a big potential asset for Anne Arundel County, but it is currently underutilized. The connection to the B&A Trail is poor, crossing B&A Blvd. is treacherous, and it is surrounded by a sea of surface parking instead of a dense, walkable neighborhood. I hope that Anne Arundel County will work to make this area and this transit station more walkable and bring some life to this area. Also, I hope that Transit Station will be added to the types of places in the survey.	
2/3/22 11:15	Patrick J McMahon	pmcmahon@mta.maryland.gov	point	1643904955293	Walking along this section of Ritchie Highway is incredibly challenging and more should be done to make it easier, particularly for people who get off at bus stops along Ritchie Highway so they can safely walk along and cross the street, not just at the MVA but along the length of Ritchie Highway.	
2/3/22 11:19	Patrick J McMahon	pmcmahon@mta.maryland.gov	route	1643905186624	Love the B&A Trail but hope changes can be made so that a wider range of bicycle riders will feel comfortable riding from the end of the trail into Annapolis. It's so close and yet so far because of how challenging that last two miles is. Hope that someday I'll feel comfortable riding this with my son.	
2/3/22 11:21	Patrick J McMahon	pmcmahon@mta.maryland.gov	feature	1643905270950		Riva Road is such an important connection and I hope that the County will continue it's efforts to make it safer for walking and bicycling.
2/3/22 11:22	Patrick J McMahon	pmcmahon@mta.maryland.gov	feature	1643905366071		Looking forward to the day when this bridge is finally complete and you can comfortably bike from Anne Arundel County to Prince George's County and down to D.C. It's been a long time coming and hope it's done soon.
2/3/22 11:25	Patrick J McMahon	pmcmahon@mta.maryland.gov	point	1643905519174	Hope that there can be safe routes to bike to the Dorsey MARC Station from Anne Arundel County in the future, although it's not the highest priority.	
2/3/22 11:27	Patrick J McMahon	pmcmahon@mta.maryland.gov	point	1643905655808	Hope that there can be better bicycle and walking connections to the Savage MARC Station in the future. It's location at the county line makes it challenging as does the surrounding industrial property, but it could be a connection to jobs and other destinations.	
2/3/22 12:06	Ken	kmbjr0@gmail.com	draw-response	1643652911079		
2/3/22 12:09	Ken	kmbjr0@gmail.com	draw-response	1643551011713		
2/3/22 12:10	Ken	kmbjr0@gmail.com	draw-response	1639667949027		
2/3/22 12:13	Ken	kmbjr0@gmail.com	draw-response	1643131102972		

2/3/22 15:51	James J AMBROZE WICZ	jjbroze@hotmail.com	welcome-survey			
2/3/22 18:20	Ken	kmbjr0@gmail.com	feature	1643930437556		Connecting BWI Trail to Patapsco Valley State Park would be huge.
2/3/22 18:26	Ken	kmbjr0@gmail.com	draw-response	1643656487948		
2/3/22 20:59			welcome-survey			
2/3/22 21:02			welcome-survey			
2/3/22 21:38	Ken	kmbjr0@gmail.com	feature	1643942309641		dedicated pedestrian/bike paths connecting residents to amenities down Mountain Rd corridor, from B&A trail to Downs Park
2/3/22 21:38	Ken	kmbjr0@gmail.com	feature	1643942336564		dedicated pedestrian/bike paths connecting residents to amenities down Mountain Rd corridor, from B&A trail to Downs Park
2/3/22 21:39	Ken	kmbjr0@gmail.com	feature	1643942341805		dedicated pedestrian/bike paths connecting residents to amenities down Mountain Rd corridor, from B&A trail to Downs Park
2/3/22 21:39	Ken	kmbjr0@gmail.com	feature	1643942346712		dedicated pedestrian/bike paths connecting residents to amenities down Mountain Rd corridor, from B&A trail to Downs Park
2/3/22 21:39	Ken	kmbjr0@gmail.com	feature	1643942353257		dedicated pedestrian/bike paths connecting residents to amenities down Mountain Rd corridor, from B&A trail to Downs Park
2/3/22 21:39	Ken	kmbjr0@gmail.com	feature	1643942368250		dedicated pedestrian/bike paths connecting residents to amenities down Mountain Rd corridor, from B&A trail to Downs Park
2/3/22 21:42			draw-response	1643722725676		

2/3/22 21:43			feature	1643942588261		A dedicated bike lane along Benfield/Robinson and connecting to B&A trail would be a huge benefit to this area. Residents would have safe immediate connections to schools, parks, retail, restaurants, grocery store, convenience store--in addition to the areas served by the B&A trail system. With the addition of the Jumpers Hole bike lane, connection to Kinder Farm park.
2/3/22 21:50			feature	1643943009598		Finish South Shore Trail as planned! Impactful connections among Ft Meade, Millersville, MARC, Crofton, Odenton, PG County. Connect with BWI Trail for greater transportation reach. Extend out towards Elkridge, Patapsco SVP for connections into Howard and Baltimore Counties.
2/3/22 22:10			feature	1643944214212		Connecting two travel paths increases utility of both
2/3/22 22:10			feature	1643944248347		Finish Broadneck Trail as planned!
2/3/22 22:19			draw-response	1643156528518		
2/3/22 23:23	Cheryl Gray	cagray61@verizon.net	welcome-survey			
2/4/22 4:22			welcome-survey			
2/4/22 7:34			welcome-survey			
2/4/22 9:10			welcome-survey			
2/4/22 9:14			route	1643984047396		
2/4/22 9:15			welcome-survey			
2/4/22 9:16			route	1643984202237		
2/4/22 12:38			feature	1643996292678		Adding sidewalk would be ideal here.
2/4/22 12:58			draw-response	1643157289784		
2/4/22 13:15	Jeff Dustin	Jeffdustin1@yahoo.com	welcome-survey			
2/4/22 13:18			route	1643998690444	Is this really what we consider a bike lane? With respect to running motor vehicles, people either want to be in one or far enough away from them to not be continuously agitated by or fearful of their noise, pollution, and momentum.	

2/4/22 13:28			feature	1643999332076		Should be upgraded to "Bike Trail" if "Bike Lane" is synonymous with essentially useless paint and signs in existing shoulder. In all seriousness, the County could probably save money by asking local advocacy groups/residents to go out and paint their own local road shoulders, if that's all "Bike Lane" is going to mean. This model has worked for off-road trail maintenance.
2/4/22 13:47			draw-response	1643209866519		
2/4/22 13:49			welcome-survey			
2/4/22 14:09			welcome-survey			
2/4/22 14:10			welcome-survey			
2/4/22 14:11			welcome-survey			
2/4/22 14:33	B Lang	briegerman@yahoo.com	welcome-survey			
2/4/22 14:34	B Lang	briegerman@yahoo.com	welcome-survey			
2/4/22 14:36	Sheldon Pitterman	nspitterman@yahoo.fr	welcome-survey			
2/5/22 6:13	Cat Miller	catm0620@gmail.com	welcome-survey			
2/5/22 6:37	Cat Miller	catm0620@gmail.com	route	1644061042475	When I moved from DC to Annapolis in 2019 I was really taken back by the lack of bike lanes near our home. My fiance and I love to walk and ride our bikes and prefer to get to places within 2-5 miles from our home walking and biking. We live so close to many shopping centers, parks, etc. now but the existing sidewalks are cracked in many places and not great for bikes, and the roads aren't safe for biking either as the main access points (especially down Rt. 2 from the Whole Foods / Target to Admiral Cochrane) are unsafe to ride on due to heavy and fast car traffic. The longer I've lived here the more I see people who work in the shopping centers who live off of Forest Dr. walk and bike to work down Forest / Rt. 2. They often walk in the grass along the road, or on the street next to cars going 40-50 miles per hour. I am genuinely concerned for the safety and wellbeing of these people who have to commute by walking or biking down Rt. 2 each day. Currently, when we bike / walk to the shopping centers we have to go down Admiral Cochrane, hang a right at Riva, and there is a crosswalk there but after you cross you have to share the sidewalk with pedestrians and there are no bike lanes with guard reflectors to get to safely to the shopping centers. There is no cut through between the shopping center Giant is in and the	
2/5/22 6:39	Cat Miller	catm0620@gmail.com	route	1644061181859	Would love to be able to bike or walk across the road safely to get to the park. We would walk our kayaks across the road to get there if we could. Others from the community who have kids would also walk or bike there if they could, as we've discussed at BBQs and get togethers.	

2/5/22 6:43	Cat Miller	catm0620@gmail.com	route	1644061386426	Would like to see bike lanes with reflectors / guards. Basically a safe way to get down West and under 50 to link the Whole Foods shopping center to the mall.	
2/5/22 6:44	Cat Miller	catm0620@gmail.com	point	1644061495861	Have seen many people cross here on foot despite no crosswalk / sidewalk.	
2/5/22 6:46	Cat Miller	catm0620@gmail.com	route	1644061569037	Would love to see designated bike lane.	
2/5/22 6:47	Cat Miller	catm0620@gmail.com	route	1644061625271	Would love to see designated bike lane / sidewalk expansion or improvement.	
2/5/22 8:20			welcome-survey			
2/5/22 11:43			welcome-survey			
2/5/22 12:06	Micah	micah.strube@gmail.com	welcome-survey			
2/5/22 12:08	Micah	micah.strube@gmail.com	welcome-survey			
2/5/22 12:09	Micah	micah.strube@gmail.com	welcome-survey			
2/5/22 12:10	Micah	micah.strube@gmail.com	point	1644081042911	Easier access from B&A trail to cross Ritchie and reach businesses, plus bike parking at shopping centers.	
2/5/22 12:10	Micah	micah.strube@gmail.com	welcome-survey			
2/5/22 12:46	Michael David Golden	md55golden@yahoo.com	welcome-survey			
2/5/22 12:50	Michael David Golden	md55golden@yahoo.com	point	1644083423179	Elvaton Road does not have shoulders. Very scary to ride a bike on. I use this road to connect from Old Mill and the B&A Tril.	
2/5/22 16:04			welcome-survey			
2/5/22 17:50			welcome-survey			
2/5/22 22:14	Luis Lucchini	lrlucchini07@gmail.com	welcome-survey			
2/5/22 22:16	Luis Lucchini	lrlucchini07@gmail.com	feature	1644117395115		Protect this bike lane! we have the space, especially if we go the Traffic Calming route and narrow the car lanes. This entire corridor can and should have protected bike lanes from Telegraph Road to Crain Hwy.
2/5/22 22:19	Luis Lucchini	lrlucchini07@gmail.com	route	1644117564404	Add a protected bike lane to this corridor. This all those residential neighborhoods from Quarterfield Rd. to the town center, especially now that we're revitalizing it.	
2/5/22 22:20	Luis Lucchini	lrlucchini07@gmail.com	point	1644117638307	Protect this crossing better, people from the neighborhoods on the east will be better connected to the town center	
2/5/22 22:21	Luis Lucchini	lrlucchini07@gmail.com	point	1644117696547	Protect this crossing better, people from the neighborhoods on the east will be better connected to the town center	
2/5/22 22:28	Luis Lucchini	lrlucchini07@gmail.com	feature	1644118111278		Boooo signed bike lanes are useless. This is a really wide road, it can certainly accomodate a dedicated and protected bike lane
2/6/22 13:01			welcome-survey			
2/6/22 13:15			welcome-survey			
2/6/22 18:32			welcome-survey			
2/6/22 19:46	Stephan Burton	stephantop@gmail.com	welcome-survey			

2/6/22 19:49	Stephan Burton	stephantop@gmail.com	point	1644194981380	There is a traffic calming barrier here that is a safety hazard to cyclists. It should be removed.	
2/6/22 19:49	Stephan Burton	stephantop@gmail.com	welcome-survey			
2/6/22 22:17	David Lampen	dal6x@virginia.edu	welcome-survey			
2/6/22 22:37	David Lampen	dal6x@virginia.edu	route	1644205027903	I used this route to get to work several times before I acquired a Rock Road/Pepper Gate pass. Charter Oaks Boulevard provides direct access to the only ACP serving the eastern edge of Fort Meade. A shared lane or MUP along Charter Oaks would encourage bicycle access to Fort Meade from the east and south. I have used the MUP along 175 from Blue Water, but the MUP forces me to make a left turn into the ACP in front of a right turn lane when I'm riding into Fort Meade from the south. A separated bike lane or MUP along the length of Mapes Road would allow cyclists to cross the base safely, and access the services and facilities in the base's core, without slowing traffic on its busiest thoroughfare.	
2/7/22 1:02			welcome-survey			
2/7/22 4:33			welcome-survey			
2/7/22 6:34			welcome-survey			
2/7/22 7:02			feature	1644235333346		This trail is badly needed. It would be great to connect Greenbury Point with other running routes. This road should be widened and allow for running/jogging lanes.
2/7/22 7:03			feature	1644235435255		PLEASE connect this route with the college parkway trail and B&A trail! This would be so fantastic to be able to reach Sandy Point with Annapolis via trails!
2/7/22 9:57			route	1644245828435	it would be great to connect defense Highway to Harry s Truman with a bike lane. It would connect 2 major routes and would allow residents of my community (North River Forest) to bike safely to The Farmers Market and walking trails behind county Buildings.	
2/7/22 10:43			welcome-survey			
2/7/22 13:59	Peter Goldsmith	peterzgoldsmith@gmail.com	welcome-survey			

2/7/22 14:12	Peter Goldsmith	peterzgoldsmith@gmail.com	point	1644261120122	Woodland Road is the only through road from Obrecht to the trail on East West Blvd, which ultimately connects to the B&A Trail. Cyclists turning from Woodland onto Obrecht have difficulty seeing oncoming traffic. There is no sidewalk or designated bike lane from Obrecht to Jumpers Hole, leading to the B&A Trail. There is also no sidewalk or designated bike lane along Woodland Road leading to the East West Trail. Hundreds of new homes were recently built on Obrecht/Brightview; however, there is no safe bicycle infrastructure leading to the B&A Trail. In the Walk & Roll plan, I would like to see a plan for safely connecting communities on Obrecht to both the B&A Trail and the East West Trail. Currently, runners and walkers travel east along the side walk and cut through Larbo Road to get to the East West trail; however, that is not necessarily legal or safe.	
2/7/22 14:17			welcome-survey			
2/7/22 15:41			welcome-survey			
2/7/22 21:59			welcome-survey			
2/8/22 8:23			welcome-survey			
2/8/22 10:00			welcome-survey			
2/8/22 11:47	John Menocal	johnmenoca@aol.com	welcome-survey			
2/8/22 11:48	C		welcome-survey			
2/8/22 12:00	Beth Paquette	bekiscoo@gmail.com	welcome-survey			
2/8/22 12:04			welcome-survey			
2/8/22 12:05			point	1644339954009	no safe bike lane or shoulder supported by the state	
2/8/22 12:07			route	1644340066125	road next to elementary school and park with playgrounds has high speed car traffic, no shoulder, no sidewalk and blind corners.	
2/8/22 12:09			route	1644340145883	Would be great to have a bike lane and and/or sidewalk on Londontowne Rd. I do not feel safe walking or biking on this road as-is	
2/8/22 12:10			welcome-survey			
2/8/22 12:13			welcome-survey			
2/8/22 12:15			point	1644340515250	This road is used by school aged children, there is an elementary school, a middle school, a high school, and a preschool along this road, along with a post office, library, police station, shopping center and restaurants nearby so there is constantly pedestrian traffic crossing it. The little sidewalk that is there is poorly maintained and completely ignored and often very dangerous, and there is heavy car traffic there without proper sight lines and there is little to no traffic enforcement. If you improved this area and made it more accessible to humans it would become a super desirable place to be around instead just a dirty crowded road that is constantly clogged with people trying to cut their commute distance to 214.	

2/8/22 12:21			point	1644340906636	near this intersection there are brand new homes on one side with sidewalks that don't connect to anything. There is a county park across the street, and a church, and roads that lead to downtown Annapolis. There is no safe intersection to cross or sidewalks or bike lanes that lead anywhere interesting. Nobody is safe on this road.	
2/8/22 13:51	Joan McCuiston	joanmccuiston@gmail.com	welcome-survey			
2/8/22 14:42	Nicole Murphy	Nicole@murphysmail.com	welcome-survey			
2/8/22 14:43	Nicole Murphy	Nicole@murphysmail.com	welcome-survey			
2/8/22 15:21			welcome-survey			
2/8/22 16:38			welcome-survey			
2/8/22 21:01		courtney73110@gmail.com	welcome-survey			
2/8/22 23:08	Bob Watters	robert.watters@gmail.com	welcome-survey			
2/8/22 23:11	Bob Watters	robert.watters@gmail.com	welcome-survey			
2/8/22 23:18	Bob Watters	robert.watters@gmail.com	feature	1644380323132		Connecting this part of the county between Odenton and Annapolis would be a major win for a safe bicycle route for both commuters and recreation seekers alike, and a major win for the county. Due to the size of the potential populations served, connecting dense residential areas with services, businesses and areas where people work, along a congested vehicle route with scenic byway restrictions that needs a creative solution like isolated trails, this stretch of the South Shore Trail should be *the priority* for AA Co in the near term. The Cost/Benefit is clear!
2/9/22 7:56			welcome-survey			
2/9/22 9:39			welcome-survey			
2/9/22 9:40	Scott Muchow	scott.muchow@gmail.com	welcome-survey			
2/9/22 10:12			welcome-survey			
2/9/22 15:16			welcome-survey			
2/9/22 15:32			welcome-survey			
2/9/22 15:36			welcome-survey			
2/9/22 15:36			welcome-survey			
2/9/22 15:54	Colleen Prosser	colleenprosser@yahoo.com	welcome-survey			
2/9/22 16:52	Gabriel Maier	gabrielmaier@gmail.com	welcome-survey			

2/9/22 16:56	Gabriel Maier	gabrielmaier@gmail.com	point	1644443747921	Linthicum Library. Crosswalk isn't graded for accessibility at entrance. Traffic is too fast on Hammonds Ferry Road to safely cross, no sidewalk connection light post obstruction on opposite side. Needs Street mounted "pedestrian crossing" crossing panel to slow traffic.	
2/9/22 16:58	Gabriel Maier	gabrielmaier@gmail.com	point	1644443866953	School Crossing to get to Linthicum Ave. Intersection is too busy with traffic to fast (and unyielding) to cross safely. Should have active crosswalk signal, sidewalks are poor and needs needs Street mounted "pedestrian crossing" crossing panel to slow traffic.	
2/9/22 17:02	Gabriel Maier	gabrielmaier@gmail.com	route	1644444113008	The entire "Camp Meade commercial corridor" from Maple Road to Music Lane is a travesty for commuting, pedestrian access. Sidewalks are crumbling, not graded for accessibility, obstructed by telephone poles, interrupted and poorly implemented. It needs an overhaul and a new master plan desperately to match the current needs of the Linthicum Community	
2/9/22 17:03	Gabriel Maier	gabrielmaier@gmail.com	point	1644444186431	No Crosswalk exists at this trail exit. It's unsafe to cross.	
2/9/22 17:05	Gabriel Maier	gabrielmaier@gmail.com	point	1644444290118	This light rail crossing should be grade separated for safety. Traffic on Maple is too frequent to remain at-grade.	
2/9/22 17:05	Gabriel Maier	gabrielmaier@gmail.com	point	1644444323976	This light rail crossing should be grade separated for safety. Traffic on camp meade is too frequent to remain at-grade.	
2/9/22 17:13			welcome-survey			
2/9/22 17:24			welcome-survey			
2/9/22 17:34			point	1644446054142	I think there is a real need for a bike/walk path from the WB&A trail here, east to the B&A trail. Any current routes require much added distance north to the Airport loop and back south down the B&A, which is a lot if the destination is Annapolis. A big issue is getting across Rt3. Such a path could allow reaching the Bacon Ridge Trails as well. Two Rivers residents would love to be able to bike to the east, over to Rt2. Any progress on a Pedestrian bridge over Rt3 would be welcome.	
2/9/22 17:53	Elizabeth Bowers	lizbowers1928@gmail.com	welcome-survey			
2/9/22 17:54	Elizabeth Bowers	lizbowers1928@gmail.com	feature	1644447261996		
2/9/22 19:09			welcome-survey			
2/9/22 20:04			welcome-survey			
2/9/22 21:50			welcome-survey			
2/9/22 22:08			welcome-survey			
2/9/22 22:59			welcome-survey			
2/9/22 23:16			welcome-survey			
2/10/22 5:48			welcome-survey			
2/10/22 6:38	Jody Thomas	Jodypappasthomas@gmail.com	welcome-survey			
2/10/22 6:40	Jody Thomas	Jodypappasthomas@gmail.com	welcome-survey			
2/10/22 7:08			welcome-survey			
2/10/22 7:38	Danielle	lucid.danielle@gmail.com	welcome-survey			

2/10/22 7:55	Danielle	lucid.danielle@gmail.com	welcome-survey			
2/10/22 8:07	Danielle	lucid.danielle@gmail.com	point	1644498459712	People headed north on Locust come to a T-bone at Cedar Park Road. This is a somewhat blind intersection. A curb cut for stroller and bike traffic on the north side of Cedar Park Road would help with the "dart to cross" method that most people utilize here to get to the stadium. Side walks on the south side of Cedar Park Road would also fix the problem. Adding a side walk would make the road narrower and calm the traffic. Double win.	
2/10/22 9:22			welcome-survey			
2/10/22 9:32	Edgar Mercado	edgar_mercado@yahoo.com	welcome-survey			
2/10/22 9:38			welcome-survey			
2/10/22 13:47	Bill Martin	rpmart22@aacounty.org	welcome-survey			
2/10/22 13:49	Bill Martin	rpmart22@aacounty.org	feature	1644518957602		Very much needed.
2/10/22 13:52	Bill Martin	rpmart22@aacounty.org	feature	1644519173228		This is a critical route for accessing Parole Town Center by bike and foot!
2/10/22 15:10	Zoe Johnson	zoe.johnson1@navy.mil	welcome-survey			
2/10/22 15:12	Zoe Johnson	zoe.johnson1@navy.mil	point	1644523947021	No safe shoulder for biking or walking on either side of street	
2/10/22 17:57	eric crawford	ecraw4d@gmail.com	welcome-survey			
2/11/22 9:59	Liam Kelly	liamkelly17@gmail.com	welcome-survey			
2/11/22 10:02	Liam Kelly	liamkelly17@gmail.com	route	1644591765633	This is the only way for the top half of the community to (upper loop) to get to the bottom half (lower loop). There is currently a shoulder but without any barrier it is unnerving to use. No one really feels comfortable letting their younger kids ride this section but it is the main way for the kids to get to the lower loop with thier bikes. Same goes for stroller.	
2/11/22 10:04	Mark Yerkes	mark.yerkes@gmail.com	welcome-survey			
2/11/22 10:08	Mark Yerkes	mark.yerkes@gmail.com	feature	1644592257969		Any improvement to the shoulder of this route would be great.
2/11/22 10:24	Joe	Joseph.j.lion@gmail.com	welcome-survey			
2/11/22 10:25			welcome-survey			
2/11/22 10:28			point	1644593316790	our neighborhood used this path regularly with bikes and strollers but its not safe due to high traffic and speed on W Benfield road. Would like a side walk with barrier installed so we can safely get around our neighborhood	
2/11/22 12:01	Robert Petrole	Rpetrole@gmail.com	welcome-survey			
2/11/22 12:01			welcome-survey			
2/11/22 12:02			welcome-survey			
2/11/22 12:03			welcome-survey			
2/11/22 12:19	Ryan diez	Rcdiez14@gamil.com	welcome-survey			
2/11/22 12:20	Ryan diez	Rcdiez14@gmail.com	welcome-survey			
2/11/22 12:23	Ryan Diez	rcdiez14@gmail.com	welcome-survey			

2/11/22 12:28	Ryan Diez	rcdiez14@gmail.com	route	1644600516736	Please construct a SAFE passageway (sidewalk) that would allow bicycles, strollers, etc. to safely get from Ben Oaks Dr. W to Severnside Dr. It is currently unsafe due to bend in road and cars traveling at high rate of speed. Cones erected by the county are regularly destroyed by cars.	
2/11/22 12:39			welcome-survey			
2/11/22 13:06	Lynne Rockenbach	lyndyrock@gmail.com	welcome-survey			
2/11/22 13:12	Lynne Rockenbach	lyndyrock@gmail.com	route	1644603160199	The Ben Oaks neighborhood has an upper and lower loop, but no good way to get strollers or bicycles between them. A sidewalk/bike path here seems most economical. Another alternative would be some sort of elevator near one of the stairways, but that would incur more maintenance costs.	
2/11/22 13:18	Lynne Rockenbach	lyndyrock@gmail.com	feature	1644603492592		
2/11/22 13:41			welcome-survey			
2/11/22 13:42			route	1644604956918	Traffic is bad on W Benfield, having a clear sidewalk or biking lane would be beneficial for travelers	
2/11/22 14:50	Ashley		welcome-survey			
2/11/22 14:56			welcome-survey			
2/11/22 15:00			point	1644609632930	a path to the woods	
2/11/22 15:02			route	1644609743848	A path for mountain bikers or walkers. NOT PAVED PLEASE. all trails are paved and it takes away from the natural beauty of the woods I would like this trail not paved and litter-free, please.	
2/11/22 17:03			welcome-survey			
2/11/22 19:08			welcome-survey			
2/11/22 19:31	John Kuriawa	johnkuriawa@gmail.com	welcome-survey			
2/11/22 19:39	John Kuriawa	johnkuriawa@gmail.com	draw-response	1643729049353		
2/11/22 19:42	John Kuriawa	johnkuriawa@gmail.com	draw-response	1643738192164		
2/11/22 19:46	Linda Wallace	Lindaewallace28@gmail.com	welcome-survey			
2/11/22 19:49	Linda Wallace	Lindaewallace28@gmail.com	point	1644626954349	I'd love to see a walking/ biking trail along Queenstown Rd and also up WB&A Rd.	
2/11/22 20:32			welcome-survey			
2/12/22 8:41	Rebecca	Rea96789@gmail.com	welcome-survey			
2/12/22 8:42	Rebecca	Rea96789@gmail.com	welcome-survey			
2/12/22 8:44	Rebecca	Rea96789@gmail.com	point	1644673468173	There is not a safe bike lane or walking lane to connect it to the other part of Linthicum where it is safer to bike and walk.	
2/12/22 10:04			welcome-survey			
2/12/22 10:05			welcome-survey			
2/12/22 10:07			welcome-survey			
2/12/22 10:09			welcome-survey			
2/12/22 10:09			welcome-survey			

2/12/22 10:10			welcome-survey			
2/12/22 10:11			welcome-survey			
2/12/22 10:12			welcome-survey			
2/12/22 10:13			welcome-survey			
2/12/22 10:13			welcome-survey			
2/12/22 10:14			route	1644678849803	Traffic speed makes using shoulder risky.	
2/12/22 10:15			route	1644678902910	Stroller use	
2/12/22 11:20			welcome-survey			
2/12/22 11:24			route	1644683079805	Annapolis Road starting at the Odenton library to Sappington station Road, to Burns Crossing Road, to WB& A road, to Friendship park if it had a side walk this would be great route for bike riding, walking! I would like to see a paved bike trail!	
2/12/22 11:26			route	1644683213407	this would gain access for lots of residential area to access business on Annapolis road. Walking or Biking, we need sidewalks!	
2/12/22 14:19	Elizabeth Nelson	Elizdinh@gmail.com	welcome-survey			
2/13/22 12:46	Sue Lyn Potter	montecarlosuelyn@msn.com	welcome-survey			
2/13/22 15:03			welcome-survey			
2/13/22 15:22			welcome-survey			
2/13/22 15:24			route	1644783889620		
2/13/22 15:46			welcome-survey			
2/13/22 15:48			welcome-survey			
2/13/22 15:49			welcome-survey			
2/14/22 11:39	Layne Tretow	layne98@verizon.net	welcome-survey			
2/14/22 12:02			welcome-survey			
2/14/22 12:06			route	1644858400810	Taylor Ave needs a consistently wider shoulder, on both sides, to get to the Grauls shopping center.	
2/14/22 13:02			welcome-survey			
2/14/22 13:02			feature	1644861758415		This is so needed!
2/14/22 13:05			route	1644861910032	Need a safer route along W Benfield for people on bikes, walking or with strollers.	
2/14/22 14:29			welcome-survey			
2/14/22 14:51			welcome-survey			
2/14/22 14:56			point	1644868560895	hiking and mountain biking trail	
2/14/22 14:59			point	1644868742981	It is currently very difficult to safely walk from Parole to the shopping areas on Jennifer Road.	
2/14/22 14:59			point	1644868767831	It is currently very difficult to safely walk from Parole to the shopping areas on Jennifer Road.	
2/14/22 17:11	Heather Nye	hetzeck@yahoo.com	welcome-survey			

2/14/22 17:15	Heather Nye	hetzeck@yahoo.com	point	1644877036910	This intersection prevents me from biking to work - it's so busy and intimidating. I wish there was a clear path to getting from the Bay Ridge area into Hillsmere without having to go through this intersection. Once can cut through the parking lot, but that's also taking your life into your own hands. Maybe there could be a better cut through behind the Eastport Library so people could avoid this intersection altogether to get to Quiet Waters and Hillsmere?	
2/15/22 10:06			welcome-survey			
2/15/22 12:28			welcome-survey			
2/15/22 12:33	Kate	Kaitlyntaylor93@protonmail.com	welcome-survey			
2/15/22 12:36	Kate	Kaitlyntaylor93@protonmail.com	welcome-survey			
2/15/22 12:38	Kate	Kaitlyntaylor93@protonmail.com	route	1644946696121	I would love to see a side bike lane so that I could make it to route 2, then cross and make it to the bike trail, Thank you!	
2/15/22 12:58			welcome-survey			
2/15/22 13:35			welcome-survey			
2/15/22 14:19			welcome-survey			
2/15/22 14:23			welcome-survey			
2/15/22 14:28			welcome-survey			
2/15/22 20:07			welcome-survey			
2/15/22 21:48			welcome-survey			
2/15/22 22:27			welcome-survey			
2/16/22 0:33			welcome-survey			
2/16/22 6:18			welcome-survey			
2/16/22 6:29			draw-response	1643228979785		
2/16/22 6:32			route	1645011169199	Providing access for active transportation between communities on Cypress Creek and shopping center that is more direct than driving would encourage more walking/biking and reduce traffic on Rt. 2	
2/16/22 6:39			point	1645011579152	The southbound right turn lane on Rt. 2 is dangerous to individuals in the crosswalk. Cars regularly ignore the No Turn On Red sign and roll straight through crosswalk to get closer to turn and see past the other 4 lanes of traffic. Those 4 lanes of traffic make pedestrians and cyclists hidden from right turning traffic until too late and the traffic in those same lanes makes it difficult for people in crosswalk to see on-coming traffic in the right turn lane until they've cleared the 4 lanes of waiting cars. ELIMINATE RIGHT TURN LANE! Not needed here.	
2/16/22 7:01			point	1645012883829	This is a pinch point on Manhattan Beach Road. At the crest of a hill with limited visibility in both directions, there is no shoulder on the inbound side of the road and there are holes along the edge of the pavement. Bikes must move into traffic lanes to pass safely, but are not visible to cars traveling in same direction due to hill. Need shoulder space here - a continuation of shoulder available before the hill and after the intersection with Buckingham Cove Rd.	

2/16/22 7:43			route	1645015380846	Adding walking/biking paths between communities would provide Safer Routes to Schools for more students.	
2/16/22 7:43			route	1645015402338	Adding walking/biking paths between communities would provide Safer Routes to Schools for more students.	
2/16/22 7:46			route	1645015590328	Adding walking/biking paths between communities would provide Safer Routes to Schools for more students.	
2/16/22 7:48			route	1645015735194	Adding walking/biking paths between communities would provide Safer Routes to Schools for more students.	
2/16/22 8:11	Kevin C Wikar	wikark@yahoo.com	welcome-survey			
2/16/22 8:24	Kevin C Wikar	wikark@yahoo.com	draw-response	1639667772559		
2/16/22 11:20	Randy Kliewer	rkliewer3@gmail.com	welcome-survey			
2/16/22 11:24	Randy Kliewer	rkliewer3@gmail.com	point	1645028646463	It would be great to add sharrows from AOTB to Bay Ridge RD, and to develop a safe route through the Bay Ridge/Forest Drive corridors	
2/16/22 13:03			welcome-survey			
2/16/22 13:13	Adam Pegues	pegues@usna.edu	welcome-survey			
2/16/22 13:16		mgehrdes@gmail.com	welcome-survey			
2/16/22 13:19	Adam Pegues	pegues@usna.edu	route	1645035550836	It's a high-speed "cut-through" reliever route that is heavily used to connect Route 2 to Route 648/B&A Blvd. Because of the water, all of the traffic must come down B&A Blvd. and use Asbury Drive. There is currently zero shoulder width for parts of Asbury, making it EXTREMELY unsafe for bikers or pedestrians. There is also no traffic calming, so autos routinely speed along Asbury at 40-60 mph. There should be traffic calming installed, and some of the ample lane width should be reallocated from autos to foot/bike traffic. This is true all the way up to and including the circle at Arundel Beach Rd. & Leelyn Drive. Two schools and lots of residential neighborhoods in the vicinity, but absolutely zero infrastructure for anything but auto traffic.	
2/16/22 13:23	Adam Pegues	pegues@usna.edu	route	1645035775033	There is a crossing signal on Route 2 at Whites Path, but nowhere to go once you cross to east side of divided highway. Need to provide safe crosswalks across B&A Blvd. intersection and also reallocate some of the space given to autos (THREE left turn lanes!?) to allow for foot/bike traffic to proceed safely up B&A Blvd. Hundreds of homes in the vicinity, an awesome bike trail on west side of Route 2, and ... no safe way for anyone from the east side of Route 2 to access the B&A bike trail.	
2/16/22 13:25	Adam Pegues	pegues@usna.edu	feature	1645035932765		This road needs attention ASAP - no safe options for foot/bike traffic to cross from neighborhoods on east side of Route 2 to the B&A Trail on the west side. There needs to be space allocated along B&A Blvd for non-auto users. It is unsafe currently.

2/16/22 13:27	Adam Pegues	pegues@usna.edu	feature	1645036044949		Benfield has a ton of through traffic, and space needs to be provided for bike and foot traffic. This is especially true as the road passes close to SPMS, SPHS, and local elementary schools as well. People have no way to safely get around unless in a automotive vehicle.
2/16/22 13:28		mgehrdes@gmail.com	draw-response	1644061042475		
2/16/22 14:50			welcome-survey			
2/16/22 14:54		kennlois@verizon.net	welcome-survey			
2/16/22 15:06			welcome-survey			
2/16/22 19:24			welcome-survey			
2/16/22 19:27			feature	1645057678093		Heavy traffic, it is unsafe to walk/bike on this road. Separate biking/walking trail needed.
2/16/22 21:38			welcome-survey			
2/16/22 21:45			draw-response	1643555341592		
2/16/22 21:56			draw-response	1639683997421		
2/16/22 22:52	Earl Bradley	earlbradley@verizon.net	welcome-survey			
2/17/22 11:46			welcome-survey			
2/18/22 6:57			welcome-survey			
2/18/22 7:05	Kj campbell	jason4hd@gmail.com	welcome-survey			
2/18/22 7:11	Kj campbell	jason4hd@gmail.com	point	1645186314493	Traffic regularly speeds through this area by the school. It is the crest of a hill on a blind corner making speed a serious issue. It has sufficient space to construct a traffic circle, which would slow vehicles and allow for better congestion management when school is beginning and at dismissal time. Just a thought	
2/18/22 11:38			welcome-survey			
2/18/22 12:48	Laurie	ljd8082@gmail.com	welcome-survey			
2/18/22 12:51	Laurie	ljd8082@gmail.com	route	1645206661085	It would be nice if there was a safe way to walk along Pike Ridge Road. My son and I walked there yesterday but won't do it again unless it is made safer	
2/18/22 12:54	Laurie	ljd8082@gmail.com	point	1645206853514	This area needs a crosswalk	
2/18/22 13:35			welcome-survey			
2/18/22 13:40			point	1645209650111	This road leads from the Naval Academy (HWY 450) bridge to the North Severn portion of NSA Annapolis. The road is narrow, windy and without shoulder. I frequently see people walking, running or biking on this road, to include Academy Midshipmen. With the ongoing traffic and lack of path and adequate lighting, this is a recipe for an accident or worse. In addition to correcting the hazard, I personally would use a bike/pedestrian path to transit between the North Severn portion of the base to the U.S. Naval Academy portion of the base.	

2/18/22 14:21	Steve Vahsen	steve.vahsen@gmail.com	welcome-survey			
2/18/22 15:17			welcome-survey			
2/18/22 15:27			welcome-survey			
2/18/22 15:35			welcome-survey			
2/18/22 15:37			welcome-survey			
2/18/22 16:47	Deborah K Fontaine	FONTAINEREDSOX@VERIZON.NET	welcome-survey			
2/18/22 18:36			welcome-survey			
2/19/22 7:57	Chip Walsh	Chip@chipwalsh.org	welcome-survey			
2/19/22 8:11	Chip Walsh	Chip@chipwalsh.org	feature	1645276286781		This is already viable route but dedicated lanes will help. Important to figure out a passage across Rt 3.
2/19/22 8:18	Chip Walsh	Chip@chipwalsh.org	feature	1645276703161		This is an important route. Will help Croftonites get to the MARC Station. To continue east onto Riegel, one must now turn right on 3, move three lanes left and enter a turn about, then cross three northbound lanes and turn right onto Reidel. Auto traffic is usually heavy, making this an uncomfortable situation on a bike. Please come up with a better bicycling solution.
2/19/22 8:26	Chip Walsh	Chip@chipwalsh.org	point	1645277168469	A bridge to PG Co would open up cycling to Bowie, Latham, Seabrook and New Carrollton, where I used to work. I used to ride to jobs in Latham and New Carrollton, across route 450. I was injured one morning on 450 when forced off the road by a vehicle. A bridge connecting to trails in PGCo would have made a dramatically more pleasant and safer ride.	
2/19/22 8:31	Chip Walsh	Chip@chipwalsh.org	route	1645277477714	Bike lanes on 424 from WBA trail to Reidel Rd. Traffic on 424 is heavy and portions of this route have no shoulder, forcing bikes to share the single lane with auto traffic.	
2/19/22 8:33	Chip Walsh	Chip@chipwalsh.org	feature	1645277614011		
2/19/22 8:39	Chip Walsh	Chip@chipwalsh.org	feature	1645277985720		The right of way is there. The whole South Shore trail should be built out. I used to bike commute to the MARC station and met other bike commuters from as far as Annapolis. This segment would make commuting safe and enjoyable, plus attract recreational users from surrounding neighborhoods.

2/19/22 8:50	Chip Walsh	Chip@chipwalsh.org	feature	1645278618109		Current wonderful bike lanes eastward on 175 abruptly end. There is a sign: bike lane ends, that the rider passes, after which the biker must share lanes with auto, including through the high speed interchange with Md 32. Would love to see improved access into Odenton.
2/19/22 8:54	Chip Walsh	Chip@chipwalsh.org	feature	1645278897464		Looks like a great route! Build it!
2/19/22 9:03	Chip Walsh	Chip@chipwalsh.org	feature	1645279416696		Great idea, especially given the new high school location. However, to the south, heavily trafficked New Cut Rd has no shoulders, so riders must share the single lane, and to the north Crain highway offers no passage through the intersection with I97 and across Veterans Hwy, and riders must share the road with auto traffic. His route will be minimally useful without connections north and south.
2/19/22 9:10	Chip Walsh	Chip@chipwalsh.org	route	1645279827054	Road shoulders along Benfield are big enough for a bike lane. I'd really like to see improved access on West Benfield, but there's not much physical space on that route.	
2/19/22 9:19	Chip Walsh	Chip@chipwalsh.org	route	1645280349232	Along Veterans Hwy north of 178, wide shoulders disappear at West Benfield. I'd like to see marked bike lanes from West Benfield to beyond Benfield to Maxwell Fry Rd. Riders can turn right on Maxwell Fry to access routes north and east in Severna Park and beyond.	
2/19/22 9:27			welcome-survey			
2/19/22 9:29			point	1645280941279		
2/19/22 10:26			welcome-survey			
2/19/22 10:28			welcome-survey			
2/19/22 13:36		hperoune@gmail.com	welcome-survey			
2/19/22 14:23			welcome-survey			
2/19/22 16:01			welcome-survey			
2/20/22 7:35	Kristin	kdavi03@yahoo.com	welcome-survey			
2/20/22 7:36	Bryan Burkhardt	Baburkhardt06@gmail.com	welcome-survey			
2/20/22 7:37	Bryan Burkhardt	Baburkhardt06@gmail.com	welcome-survey			
2/20/22 10:32			welcome-survey			
2/20/22 10:45			welcome-survey			

2/20/22 11:36			welcome-survey			
2/20/22 11:36			welcome-survey			
2/20/22 11:53			welcome-survey			
2/20/22 12:25			welcome-survey			
2/20/22 12:26	Ed	Ehdietrich@gmail.com	welcome-survey			
2/20/22 13:39	Christina Dove	christina.dove@ymail.com	welcome-survey			
2/20/22 13:40	Christina Dove	christina.dove@ymail.com	feature	1645382445544		area also needs sidewalks, would love to walk my child to school but the area has heavy traffic and no sidewalks
2/20/22 13:43	Christina Dove	christina.dove@ymail.com	route	1645382608017	There are no sidewalks to get to Sunset Elementary. The traffic on Ft Smallwood is very heavy, and it would be wonderful to safely walk kids to school	
2/20/22 13:50			welcome-survey			
2/20/22 14:17	Sarah DelSignore	sarahdelsignore@gmail.com	welcome-survey			
2/20/22 15:08			welcome-survey			
2/20/22 16:14			welcome-survey			
2/20/22 16:17			welcome-survey			
2/20/22 22:23	Stoney Simons	stoney.simons@gmail.com	welcome-survey			
2/20/22 22:31	Stoney Simons	stoney.simons@gmail.com	route	1645414308602	Would be great to have a bike lane on all of Sands Road, especially by the gravel pits where construction trucks make it uncomfortable to ride a bicycle.	
2/20/22 22:33	Stoney Simons	stoney.simons@gmail.com	welcome-survey			
2/21/22 2:57	Shelly Campbell	Smcampbell@gmail.com	welcome-survey			
2/21/22 3:18			welcome-survey			
2/21/22 7:17			welcome-survey			
2/21/22 10:13			welcome-survey			
2/21/22 10:14			feature	1645456469444		
2/21/22 10:17			feature	1645456637576		
2/21/22 12:50	Robert Schmick	markt@erols.com	welcome-survey			
2/21/22 12:57	Sarah Zsak	sarahzsak@gmail.com	welcome-survey			
2/21/22 13:07	Sarah Zsak	sarahzsak@gmail.com	route	1645466874082	Not sure what you think, but what about a trail along the power lines to Riva Area Park? I am certain many residents of the local neighborhoods (Wynnewood, Ashley, etc.) would like a safe and green way to get to the park. And it would raise property values for all. I have heard of utility trails in other states (PGE, etc.). Would BGE be able to work with us on something like this?	

2/21/22 13:10			point	1645467030262	Indian Landing Road really needs one raised sidewalk from Workman Drive down toward the Boat Club at the river. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:11			point	1645467114222	Indian Landing Road really needs one raised sidewalk from Workman Drive down toward the Boat Club at the river. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:13			point	1645467188508	Indian Landing Road really needs one raised sidewalk. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:13	Robert Schmick	markt@erols.com	draw-response	1644868767831		
2/21/22 13:14	Sarah Zsak	sarahzsak@gmail.com	point	1645467240215	This is a dangerous intersection for pedestrians, bicyclists, and drivers	
2/21/22 13:14			point	1645467280059	Indian Landing Road really needs one raised sidewalk. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:15			point	1645467309402	Indian Landing Road really needs one raised sidewalk. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:15			point	1645467337716	Indian Landing Road really needs one raised sidewalk. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:16			point	1645467359495	Indian Landing Road really needs one raised sidewalk. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:16			point	1645467384409	Indian Landing Road really needs one raised sidewalk. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	

2/21/22 13:16			point	1645467418568	Indian Landing Road really needs one raised sidewalk. It is currently unsafe to walk, jog, walk dogs, or bicycle on Indian Landing Road, due to vehicular traffic that often speed above the posted speed limit. Neighbors of the Historic Indian Landing Community need to be able to walk safely to visit each other, without using cars. Thank you.	
2/21/22 13:18	Sarah Zsak	sarahzsak@gmail.com	route	1645467500001	Bicyclists take this route at times but appears dangerous. My family of 6 would love to decrease vehicle traffic and also be able to travel here in a healthier/greener way, but it is too risky for us.	
2/21/22 13:19			welcome-survey			
2/21/22 13:26			point	1645467962914	A public pathway that is easy to be traversed is needed somewhere at the end of Indian Landing Road, so that Historic Indian Landing Community residents can easily and legally launch their kayaks into the river, even if they haven't been able to become members of the Boat Club. A walking pathway, big enough to carry a kayak, is needed. Thank you.	
2/21/22 13:29			point	1645468192727	Extremely dangerous intersections in this general area for both pedestrians and vehicles. Something needs to be done to make it safer. Thank you.	
2/21/22 14:09			welcome-survey			
2/21/22 14:14			welcome-survey			
2/21/22 14:16			point	1645471017100	The trail dead ends here	
2/21/22 14:20			route	1645471253694	I work by the airport and would like to bike to work. However, there is no safe route. Instead, i bike to the Odenton Marc, take the train 1 stop to bwi, then bike. This is good exercise and environmentally friendly, bit more expensive than driving. After realizing this does not make sense financially and puts me at the mercy of the train schedule, I have started driving more but would love to bike instead.	
2/21/22 15:01			route	1645473664096	Bridge needed	
2/21/22 15:51	Dawn Baskin	dawn@thebaskins.com	welcome-survey			
2/21/22 22:32			welcome-survey			
2/21/22 23:42	Joe Golden	golden2288@msn.com	welcome-survey			
2/22/22 14:44			welcome-survey			
2/22/22 15:30			welcome-survey			
2/22/22 20:24	Louise McKernan	louiseg98@hotmail.com	welcome-survey			
2/22/22 20:26	Louise McKernan	louiseg98@hotmail.com	feature	1645579600677		
2/22/22 20:28	Louise McKernan	louiseg98@hotmail.com	feature	1645579705778		The sidewalk is narrow and doesn't lend itself to walking more than one or two people wide.
2/22/22 20:32			route	1645579957842	This is a narrow road that is used frequently for both biking and running. Runners are often forced off the road and into the brush, and with the turns in the road bikers sometimes aren't seen until the last minute. The number of bikers and runners on this road begs for a bike/walk path.	

2/22/22 23:25			welcome-survey			
2/22/22 23:26			welcome-survey			
2/22/22 23:27			welcome-survey			
2/23/22 10:35			welcome-survey			
2/23/22 11:27			welcome-survey			
2/23/22 14:17	Frank meyers	Frboy.fm@gmail.com	welcome-survey			
2/23/22 14:20	Frank meyers	Frboy.fm@gmail.com	welcome-survey			
2/23/22 14:22	Frank meyers	Frboy.fm@gmail.com	feature	1645644159992		We use this route to comute to work.
2/23/22 14:24	Frank meyers	Frboy.fm@gmail.com	feature	1645644264168		Wat to dangerous now.
2/23/22 14:31			welcome-survey			
2/23/22 14:54			welcome-survey			
2/23/22 14:55			welcome-survey			
2/23/22 15:00			draw-response	1643832598566		
2/23/22 15:02			feature	1645646566902		
2/23/22 15:31			welcome-survey			
2/23/22 15:33			welcome-survey			
2/23/22 15:33	Colleen Prosser	colleenprosser@yahoo.com	welcome-survey			
2/23/22 15:37	Colleen Prosser	colleenprosser@yahoo.com	point	1645648620629	I would like to see access from Gingerville to Harry S. Truman Parkway. The county owns the property at the end of Cardamon Lane, and can provide access. If that is done I can access shopping, drs, etc from my home without using a car.	
2/23/22 15:48			welcome-survey			
2/23/22 15:50	Cathrine Yeager	cathryeager@gmail.com	welcome-survey			
2/23/22 15:56			draw-response	1644340906636		
2/23/22 16:20			welcome-survey			
2/23/22 16:39	Jeremy	j3r3myl33@gmail.com	welcome-survey			
2/23/22 16:44	Melissa Chatham	mchat76@aim.com	welcome-survey			
2/23/22 16:45	Melissa Chatham	mchat76@aim.com	route	1645652736123	There is no sidewalk and I use this route to go to the library from home. Otherwise I have to go down into the school area and back up.	
2/23/22 16:47	Melissa Chatham	mchat76@aim.com	point	1645652865491	At this location there is a brick pillar with a big evergreen bush both of which block the vision of the drivers making a right hand turn to pedestrian traffic waiting to cross the street toward the library.	
2/23/22 16:50	Melissa Chatham	mchat76@aim.com	route	1645652999389	There is no crosswalk across Aquahart at this location so you have to play frogger or walk all the way down to the B&A trail just to come back to this light. The intersection of Westway and Aquahart with the green arrow from Ritchie is also scary.	
2/23/22 16:51	Melissa Chatham	mchat76@aim.com	point	1645653078886	For whatever reason, the audio cues for this crossing is barely heard. They are there but you can barely hear them. Not as load as others along the trail.	
2/23/22 16:53	Melissa Chatham	mchat76@aim.com	point	1645653179063	The audio cues at this light are barely audible. They are there but not anywhere near as loud as the other lights/crossings on the B&A trail.	

2/23/22 17:16	David	tomahawkdavid@gmail.com	welcome-survey			
2/23/22 17:19	David	tomahawkdavid@gmail.com	route	1645654750452	DEADLY, as Trish proved in 2013.	
2/23/22 17:22	David	tomahawkdavid@gmail.com	route	1645654927065	Very dangerous traffic pattern and exceptionally aggressive drivers; have been deliberately run off road seven times in this section.	
2/23/22 17:24			welcome-survey			
2/23/22 17:26	David	tomahawkdavid@gmail.com	route	1645655191489	State of the shoulder is exceptionally dangerous, especially in low light. There are numerous areas where riders are forced into traffic due to debris on the shoulder; much of it has been there for more than a year.	
2/23/22 17:29			route	1645655346256	better shoulder (remove debris and in certain places increase should width	
2/23/22 17:29	David	tomahawkdavid@gmail.com	route	1645655368480	No shoulders and very aggressive drivers traveling at excessive speed with no enforcement evident.	
2/23/22 17:31			feature	1645655491999		Need a pedestrian/cyclist activated traffic signal for safe crossing
2/23/22 17:32	David	tomahawkdavid@gmail.com	route	1645655557864	Shoulders are consistently awash in foul water due to lack of culverts to control runoff. Many times the condition of the shoulders (from water or other debris) forces riders into traffic.	
2/23/22 17:34			feature	1645655672997		need a signal near fire station to cross in when traveling south and turning east from 424 to Bell Branch
2/23/22 18:26			welcome-survey			
2/23/22 18:55	Tina	Dicingsmiths@gmail.com	welcome-survey			
2/23/22 18:55			welcome-survey			
2/23/22 19:41			welcome-survey			
2/23/22 20:02			welcome-survey			
2/23/22 21:45			welcome-survey			
2/23/22 23:15			welcome-survey			
2/23/22 23:37			point	1645677453114	There is a blind hill on this street with no sidewalk, and kids are walking to a bus stop and biking to see friends; also neighbors walk their dogs in the dark after work and with no sidewalk and limited lighting, it is just an accident waiting to happen.	
2/24/22 4:46			welcome-survey			
2/24/22 8:00	Aaron Todd	aaron.w.todd@gmail.com	welcome-survey			
2/24/22 8:24			welcome-survey			
2/24/22 8:45	Marla Rodgers	ougirl_1@msn.com	welcome-survey			
2/24/22 8:48	Marla Rodgers	ougirl_1@msn.com	feature	1645710582265		I would walk and ride my bike regularly on this road if the sidewalks were maintained and if there was a bike lane

2/24/22 8:49	Marla Rodgers	ougirl_1@msn.com	feature	1645710661583		Currently, this is a scary stretch to ride a bike but the only way to get to the peninsula. Desperately needs a bike lane!
2/24/22 8:50	Marla Rodgers	ougirl_1@msn.com	feature	1645710731403		Since there's no shoulder or sidewalk, you currently have to ride in the road which is 2-lane. It needs a safe shoulder at the very least
2/24/22 9:25			welcome-survey			
2/24/22 9:26			route	1645712802925		
2/24/22 10:58			welcome-survey			
2/24/22 11:03		Donna.carrington@comcast.net	welcome-survey			
2/24/22 11:23			welcome-survey			
2/24/22 12:53	Mike	Cornerstoneinc@gmail.com	welcome-survey			
2/24/22 14:48	Randy Williams	stressrn33@gmail.com	welcome-survey			
2/24/22 15:14	April Walker	awalker2077@aol.com	welcome-survey			
2/24/22 15:29	April Walker	awalker2077@aol.com	welcome-survey			
2/24/22 16:55			welcome-survey			
2/24/22 17:11			welcome-survey			
2/24/22 17:17	Stewart Potter	spotter@celtic-links.com	welcome-survey			
2/24/22 17:21	Stewart Potter	spotter@celtic-links.com	draw-response	1639675099989		
2/24/22 17:24	Stewart Potter	spotter@celtic-links.com	route	1645741463823	There is no crossing markings or signals and there is no sidewalk going from the sidewalks at Ridge Forest Way to the walking path to Stoney Run. Many of the communities are NOT tied together with sidewalks, they simply have their paths end at roads.	
2/24/22 17:25	Stewart Potter	spotter@celtic-links.com	route	1645741544761	There is no shoulder or sidewalk connecting the communities on the east side of Ridge Road to Stoney Run and their paths to Arundel Mills shopping.	
2/24/22 17:27	Stewart Potter	spotter@celtic-links.com	route	1645741629960	Make a sidewalk here to finish the connection between Stoney Run and Arundel Mills.	
2/24/22 17:28			welcome-survey			
2/24/22 17:32			route	1645741944152	Need to add a bike/walking lane from Beverly Beach to the intersection with Triton Beach Road. It is not right to cut off Beverly Beach from the bike lane planned down Rt214. And don't say 'you can go through the park to get to the bike lane'. That is not practical. People would use the bike lane to get to the Stop Shop and the Post Office at the very least. It would also be great to get to the Old Stein Inn and Bayside Inn from Beverly Beach or from other communities to Bayside.	
2/24/22 17:36			welcome-survey			
2/24/22 18:54	Lorraine Faris	lor.f50@gmail.com	welcome-survey			
2/24/22 20:29	Janellen Frantz	jfrantz50@gmail.com	welcome-survey			

2/24/22 20:35	Janellen Frantz	jfrantz50@gmail.com	feature	1645752943547		It is difficult to walk or bicycle from St. Margaret's to the Cape St. Claire Shopping Center because one must cross an exit ramp (Route 50 E to Cape St. Claire Rd.).
2/24/22 20:40	Janellen Frantz	jfrantz50@gmail.com	welcome-survey			
2/24/22 20:56	Bob Frantz	Rfrantz50@gmail.com	welcome-survey			
2/24/22 21:02	Bob Frantz	Rfrantz50@gmail.com	feature	1645754535222		This is a "great" area for biking (beautiful country road) but currently is extremely dangerous due to fast moving traffic, no shoulder and a narrow road.
2/24/22 21:06	Bob Frantz	Rfrantz50@gmail.com	route	1645754795790	I have occasionally used 179 for biking, but it doesn't have shoulders in all areas and can be dangerous with traffic. I would suggest putting a bike lane along St. Margarets road (Rt 179).	
2/24/22 21:10	Bob Frantz	Rfrantz50@gmail.com	route	1645755000068	East College Parkway and Whitehall are also good candidates for bike trails. Limited shoulder and high speed traffic make it dangerous to bike.	
2/24/22 21:12			welcome-survey			
2/24/22 21:13	Bob Frantz	Rfrantz50@gmail.com	feature	1645755215048		Good idea. I've biked this once, but it's risky given the parked cars and, unless you ride on the sidewalk, which I believe is illegal, you basically have to ride with the traffic. If possible, it would be good to have the bike lane extend all the way to the City Dock.
2/24/22 21:14	Bob Frantz	Rfrantz50@gmail.com	feature	1645755280065		Great idea
2/24/22 21:28			welcome-survey			
2/24/22 21:30			welcome-survey			
2/24/22 21:31			welcome-survey			
2/24/22 21:32			welcome-survey			
2/24/22 21:32			welcome-survey			
2/24/22 21:36			point	1645756558649	Whitehall Cove is an area that many people walk and bike. There are no street lights, cars and trucks often drive fast, and a space for pedestrians, bicyclists, and equestrians is needed.	
2/24/22 23:53	Mark Smith	687 Winding Stream Way, Unit 301, Odenton, Md. 21113	welcome-survey			
2/24/22 23:56	Mark Smith	687 Winding Stream Way, Unit 301, Odenton, Md. 21113	point	1645764997192	Hope it connects to Bowie soon!	
2/25/22 7:47	Tricia Baldwin	Pbaldwin@reliablecontracting.com	welcome-survey			

2/25/22 7:56	Tricia Baldwin	Pbaldwin@reliablecontracting.com	feature	1645793792860		pats of this route will be difficult due to lack of shoulders and winding roads with blind curves. i also don't think riding along 197 is a good idea unless its a separate trail. And even then, how do you safely cross highway off and on ramps?
2/25/22 7:59	Tricia Baldwin	Pbaldwin@reliablecontracting.com	feature	1645793948588		i don't think bikes belong on highways. Too dangerous.
2/25/22 8:02	Tricia Baldwin	Pbaldwin@reliablecontracting.com	feature	1645794177485		this may be a good route if access to it is provided in an area not adjacent to residential homes and some fencing or natural barriers are created to protect the community from trash and unwanted parking.
2/25/22 10:26			welcome-survey			
2/25/22 11:12	Geneva M Lankford-Blanks	glankfordblanks.18@gmail.com	welcome-survey			
2/25/22 13:13			welcome-survey			
2/25/22 14:30			welcome-survey			
2/25/22 14:30			welcome-survey			
2/26/22 7:00			welcome-survey			
2/26/22 7:26			feature	1645878390495		I support the decision to add signage but agree with my fellow cyclists that R424/Davidsonville Rd needs proper cycling shoulders similar to R450/Defense Hwy. Since R424/Davidsonville Rd is the main link between Crofton and Davidsonville, there are numerous trucks and oversized vehicles. And due to impatient drivers, there is aggression against cyclists that are seemingly in the way of speeding cars. It's one of the most dangerous roads I have cycled (and this includes overseas experience in Africa and the UK) but it is a necessary evil in order to travel between Crofton and Davidsonville. I'll also note there's no extension
2/27/22 1:31			welcome-survey			
2/27/22 8:15			welcome-survey			
2/27/22 8:17			welcome-survey			

2/27/22 8:20			point	1645968056188	This intersection crossing route 2 can be very intimidating for families with small kids. It cuts off communities on the eastern side from using the b and a trail to it's fullest. A pedestrian walkway would also benefit the tens of elementary school students who cross the road in the afternoon.	
2/27/22 20:08	james ONeil	atdmetal@gmail.com	welcome-survey			
2/27/22 20:23	james ONeil	atdmetal@gmail.com	point	1646011429065	This section of 214 could desperately use a sidewalk. We currently walk our children (4 and 6 years old) to and from school along here, and we also use this road to walk to Beverly Triton Nature Park. Many other pedestrians walk along this corridor as well. Traffic is increasing along here, and the speed limit is frequently ignored. It is becoming increasingly unsafe for our family to walk here, especially during the weekends when the traffic is heavier. It has reached the point that we have to time our walks around expected traffic, and I am concerned that a fatal accident will occur if safety measures aren't put in place immediately.	
2/27/22 20:29	james ONeil	atdmetal@gmail.com	feature	1646011790037		I would like to see a sidewalk or walking trail along this road to Beverly Triton Nature Park. It is extremely unsafe for pedestrians to travel this road on the weekends when traffic is heavier, making the Nature Park inaccessible to the people that live in this area. Before we make improvements to the park to get more people to visit, it is imperative that you ensure the safety of those who live here first.
2/27/22 20:44	james ONeil	atdmetal@gmail.com	point	1646012677283	Many people walk and run along this road. An extended shoulder along here would significantly improve pedestrian safety. A simple solution would be to re-line the existing road, shifting both traffic lanes closer to one side to create a wider shoulder on the other side. A walking trail parallel to the road in the Nature Park would be even better. As the weather warms up, traffic will only get worse, further impacting pedestrian safety. Priority must be placed on keeping the members of these communities safe as we continue to get more and more out of town traffic.	
2/27/22 20:46	james ONeil	atdmetal@gmail.com	route	1646012783563	Along this route I would like to see sidewalk or walking trail.	
2/27/22 20:59			welcome-survey			
2/28/22 8:06	rosemary byrd	rbyrd3@comcast.net	welcome-survey			

2/28/22 8:19	rosemary byrd	rbyrd3@comcast.net	route	1646054369564	This route has no shoulder or bike lane, but is very popular with walkers and bike riders. Moderate vehicle traffic makes it dangerous when walkers/bikers and cars encounter each other. I'd like to see a new path built that will allow residents of Rose Haven and slipholders of Herrington Harbour to walk/bike safely between North Beach and Rose Haven. Assuming the environmental impacts would not prevent it, I'd like to see an elevated boardwalk built through the marsh; alternatively, a formal trail along Holland Point would be helpful.	
2/28/22 9:47	Karen Eagan	eagankaren23@yahoo.com	welcome-survey			
2/28/22 9:47	Karen Eagan	eagankaren23@yahoo.com	welcome-survey			
2/28/22 10:12	Colin Gotimer	c.gotimer@gmail.com	welcome-survey			
2/28/22 10:34			welcome-survey			
2/28/22 11:58	Bill Schmidt	baschmidt2@yahoo.com	welcome-survey			
2/28/22 12:08	Bill Schmidt	baschmidt2@yahoo.com	feature	1646068081896		I am not sure about the proposed route but more trees are needed on the existing Broadneck Trail (on the side of the trail closest to the road) - there is no shade for most of the day and in the summer it is uncomfortable to walk or bike (especially walk) in the heat with no shade. Recently grass seed was put down in that area and it was a missed opportunity to not plant shade trees like willow oaks and northern red oaks. This can be corrected of course and would coincide with the new state law to plant 5 million native trees by 2031.
2/28/22 12:15	Bill Schmidt	baschmidt2@yahoo.com	feature	1646068537441		any markings that would make bikers more visible to traffic would be an improvement, even on more rural roads
2/28/22 12:19	Bill Schmidt	baschmidt2@yahoo.com	feature	1646068797123		it would be great to have a safe, protected way to bike to Baltimore
2/28/22 12:21	Bill Schmidt	baschmidt2@yahoo.com	feature	1646068883649		it would be great to have a safe, protected way to bike to Baltimore and connect to its parks and greenspaces, including to Patapsco state park

2/28/22 12:33	Bill Schmidt	baschmidt2@yahoo.com	feature	1646069584889		it would be great to have a more protected bike lane on this stretch of road and continuing into Annapolis. It would also be great if bike lanes were physically separated from the vehicular traffic but, short of this, I think the bike lanes in the county (I am specifically thinking about the stretch along route 450 but it would be good for any bike lane) should be painted to make it clear for riders but moreso for drivers so that they are fully aware. There are road signs but they are small and even if they were significantly larger drivers (including myself) just don't pay attention. If there was a colored bike lane along the road (I would suggest royal blue - bright but not
2/28/22 12:48	Laura Niewenhous	Lniewenhous@hotmail.com	welcome-survey			
2/28/22 13:31	Paul Kratchman	paulkratchman@yahoo.com	welcome-survey			
2/28/22 13:35	Paul Kratchman	paulkratchman@yahoo.com	welcome-survey			
2/28/22 13:35	Rosemary Lally	rosemary@cii.org	welcome-survey			
2/28/22 13:36	Cornelia	c_kwikar@verizon.net	welcome-survey			
2/28/22 13:43	Cornelia	c_kwikar@verizon.net	point	1646073779119	B&A trail functionally disappears at end of Navy Bridge. Protected bike lane should extend from bridge to King George Dr	
2/28/22 13:50	Cornelia	c_kwikar@verizon.net	draw-response	1646073779119		
2/28/22 15:13			welcome-survey			
2/28/22 15:32			welcome-survey			
2/28/22 15:54	Michael Mingus	darthmingi@gmail.com	welcome-survey			
2/28/22 15:59	Michael Mingus	darthmingi@gmail.com	draw-response	1643230397997		
2/28/22 16:38			welcome-survey			
2/28/22 16:49			point	1646084975420	It would be great if a crosswalk with a curbcut could be installed from the residential community side to the Bay Head Park side. People frequently speed down this road so, while it wont fix the speeding, adding a crosswalk will help as a visual aid to drivers on where to expect crossing kids.	
2/28/22 18:13			welcome-survey			
2/28/22 18:14			point	1646090061117	residential	
2/28/22 20:24	Sean Michael	wildcatsml92@icloud.com	welcome-survey			
2/28/22 20:26	Sean Michael	wildcatsml92@icloud.com	welcome-survey			
2/28/22 20:29	Sean Michael	wildcatsml92@icloud.com	point	1646098145678		
2/28/22 21:08			welcome-survey			

2/28/22 21:11			welcome-survey			
2/28/22 21:34			welcome-survey			
3/1/22 8:09			welcome-survey			
3/1/22 11:44			welcome-survey			
3/1/22 13:52			welcome-survey			
3/1/22 17:05			welcome-survey			
3/1/22 18:05			feature	1646175916262		This is where our new (bwi granted) "Welcome to North Linthicum" sign sits. This sign replaced the old Welcome sign. There was a survey crew there marking the ground and they have carelessly spray painted our sign, wall, and rosebush. Can someone please contact us to discuss this matter.
3/1/22 18:21	Charles Morgan	Cj_morgan76@yahoo.com	welcome-survey			
3/1/22 18:24	Charles Morgan	Cj_morgan76@yahoo.com	welcome-survey			
3/1/22 18:25	Charles Morgan	Cj_morgan76@yahoo.com	welcome-survey			
3/1/22 19:56			welcome-survey			
3/1/22 19:57			welcome-survey			
3/1/22 19:57			welcome-survey			
3/1/22 19:58	John Jasen	jjasen@gmail.com	welcome-survey			
3/1/22 20:02	John Jasen	jjasen@gmail.com	route	1646182937878	Could be useful	
3/1/22 20:03	John Jasen	jjasen@gmail.com	point	1646182996151	need dedicated bike trail to Magothy	
3/1/22 20:19			welcome-survey			
3/2/22 6:59			welcome-survey			
3/2/22 8:13			welcome-survey			
3/2/22 8:32			welcome-survey			
3/2/22 8:37			feature	1646228241687		Very important to safely allow cyclists to access route 468 and route 2. Shoulders are non-existent in many sections of 214 making cycling to work or for pleasure very dangerous. Bike lane much more important than additional lanes for cars.
3/2/22 8:42			route	1646228572506	a short biking/walking path connection between Dale St and Lee St would enable access of the communities on either side of Loch Haven Rd to safe walking/biking routes through each others neighborhoods, including access to parks.	

3/2/22 8:44			feature	1646228673317		important part of route 214 shared use plan.
3/2/22 8:45			feature	1646228724239		Critical for allowing access to Edgewater shopping district.
3/2/22 8:46			feature	1646228767759		critical for allowing bike/walking access to Edgewater shopping and to Annapolis
3/2/22 17:32			welcome-survey			
3/2/22 19:20			welcome-survey			
3/3/22 8:51	Nichole Meade	nsmeade@gmail.com	welcome-survey			
3/3/22 9:11	John W Spencer	JohnWSpencer5@gmail.com	welcome-survey			
3/3/22 11:45			welcome-survey			
3/3/22 18:52	Valeria Frances	valeriafrances13@hotmail.com	welcome-survey			
3/3/22 19:13	Valeria Frances	valeriafrances13@hotmail.com	feature	1646352785306		I would like to see a sidewalk that follows Town Center Blvd around the curve to Jacob's Road and then down Jacob's road - it is very very unsafe to walk around that curve so there is no way those of us down Jacobs can ever walk to the small shopping district, Train station without a sidewalk on this curve or a sidewalk through Still Meadow to Seven Oaks. This needs to be addressed
3/4/22 12:44			welcome-survey			
3/4/22 20:11			welcome-survey			
3/5/22 16:21			welcome-survey			
3/5/22 16:32	Samantha Hall	Snhall24@aol.com	welcome-survey			
3/7/22 12:28			welcome-survey			
3/7/22 14:23			welcome-survey			
3/7/22 14:26			route	1646681168793	A simple connector path here would eliminate bikes trying to share space with cars or pedestrians at the intersection.	
3/8/22 5:30	Randy Bradley	wrbradley@gmail.com	welcome-survey			
3/8/22 9:56			welcome-survey			
3/8/22 10:13			welcome-survey			
3/8/22 10:48	Kim Pollock Hudyma	khudyma@gmail.com	welcome-survey			
3/8/22 11:09			welcome-survey			
3/8/22 13:54			welcome-survey			
3/9/22 12:21	Mary Pratt	marypratt923@gmail.com	welcome-survey			
3/9/22 12:34			welcome-survey			

3/9/22 12:40			route	1646847656317	A completed sidewalk from Northeast High School/ & County Library (Duvall Hwy) to High Point Elementary School. AND one day all the way to the Middle school.	
3/9/22 12:48			route	1646848125096	Completion of sidewalk on Duvall Hwy to High point Elem. and continuation to Northeast Middle school on Outing Ave. to Mountain Road	
3/9/22 18:54			welcome-survey			
3/10/22 17:42			welcome-survey			
3/10/22 17:46			welcome-survey			
3/10/22 17:48			welcome-survey			
3/10/22 17:52			route	1646952753117	The bike lane abruptly ends and there is little to no shoulder at that point. Appears inadequate for vehicles and bikers to safely integrate into shared lane usage	
3/11/22 17:42			welcome-survey			
3/11/22 17:50			draw-response	1643218002134		
3/11/22 18:40			draw-response	1643229657015		
3/11/22 18:55			route	1647042914498	This route (mostly following power lines, like parts of the South Shore Trail) would provide low-stress walking and biking to help connect South Shore Trail (Annapolis & Odenton) with Severna Park, Millersville, & other communities to the north. The alternative is a signed bike route along part of Generalss Highway where there is little or no shoulder and lots of debris on the side of the road.	
3/11/22 19:09			draw-response	1645466874082		
3/11/22 19:12			route	1647043971230	Please finish the sidewalk on both sides of the bridge. People have to walk in the grass.	
3/11/22 19:25			draw-response	1643230161566		
3/11/22 20:31			draw-response	1639683997421		
3/11/22 20:44			feature	1647049490061		I would love to ride with my kids to Annapolis for a day out, or just take a ride after work, but riding along Generals Highway is uncomfortable for me and completely unsafe for my children. We can go to a park, but it involves packing up bikes on the car and driving. I would love a safe trail to ride on.
3/11/22 22:13	Jeffrey Rich	321j123@gmail.com	welcome-survey			
3/11/22 22:21	Jeffrey Rich	321j123@gmail.com	route	1647055259181	Quarterfield Road needs shoulders and speed enforcement to improve walking and biking safety. I often see pedestrians and cyclists use the road and it is not safe. Traffic and it's speeds are increasing along the road. It is surprising that there are not many incidents resulting in serious injuries or fatalities with the prevailing conditions.	
3/11/22 22:30	Jeffrey Rich	321j123@gmail.com	point	1647055855121	Danza Village does not have access to external roads or communities for walking or biking. It would be beneficial to get access to Donaldson Avenue which has wide shoulders, even though it has a higher speed limit. Quarterfield Road is not suitable for cycling or walking, lacking shoulders.	

3/12/22 10:56			feature	1647100574872		I see people cross West Street at Riva Road all of the time and it's very scary. There's no safe way to cross. Walking to the Jennifer Road intersection to cross is even worse. We need to make it safer for pedestrians in this area.
3/12/22 14:37			feature	1647113839483		This part of New Cut Rd also needs sidewalk. A lot of people walk here, you can see the trails worn into the grass, and they're building a high school here right now but they aren't providing sidewalk along New Cut Rd. Student's should be provided with a safe way to walk to the school.
3/12/22 15:27			welcome-survey			
3/12/22 20:42			welcome-survey			
3/13/22 10:22			welcome-survey			
3/13/22 11:56			feature	1647186994736		This would be a game-changer for a lot of people who live north of Annapolis. There's so much within walking and biking distance but just no safe way to get there right now. We need this trail!
3/13/22 12:10			route	1647187857007	This would be a great way to connect Piney Orchard with the shopping center. I hate having to drive such a short distance just to go to a shop or restaurant. Biking on Waugh Chapel Road just isn't safe, and a path from the sports park would be a lot shorter for most homeowners.	
3/13/22 15:28			route	1647199684769	This connection would make it a lot easier and safer for students to get to school or get to jobs at the mall and other shops on the other side of route 100.	
3/13/22 23:38			welcome-survey			
3/13/22 23:43			route	1647229390041	Very narrow, hilly dangerous section of Bay Head Road that often has walkers or bikers in roadway. Area on east side is partially state owned, part of Sandy Point State Park. Could very much use a trail or wide shoulder for pedestrians and bicycles.	
3/13/22 23:48			point	1647229734232	Often people (including kids) biking and walking here on narrow road with poor sight lines. Needs trail, sidewalk, or shoulder.	
3/14/22 11:16			welcome-survey			
3/14/22 15:39			welcome-survey			
3/15/22 9:48	George Percivall	george.percivall@verizon.net	welcome-survey			
3/15/22 9:49	George Percivall	george.percivall@verizon.net	point	1647352178796	Home	

3/15/22 9:51	George Percivall	george.percivall@verizon.net	route	1647352275509	Seeking safer biking to South River Greenway	
3/15/22 9:51			welcome-survey			
3/15/22 9:53			point	1647352393159		
3/16/22 14:49			welcome-survey			
3/16/22 14:52			route	1647456717776	This road need speed bumps! Please consult with local residents here, who have been working really hard to make this road safer.	
3/16/22 14:59			welcome-survey			
3/16/22 15:18			welcome-survey			
3/16/22 15:19			welcome-survey			
3/16/22 15:21	Steve Rose	sb.rose@verizon.net	welcome-survey			
3/16/22 15:22			route	1647458522093	Great idea to connect three neighborhoods. Total number of homes >250. Many of use drive between/among these three communities.	
3/16/22 15:22	Julie McCabe	danjulesp@gmail.com	welcome-survey			
3/16/22 15:22			welcome-survey			
3/16/22 15:23			welcome-survey			
3/16/22 15:24			welcome-survey			
3/16/22 15:24	Richard Larabee	catboat@protonmail.com	welcome-survey			
3/16/22 15:24			welcome-survey			
3/16/22 15:25			welcome-survey			
3/16/22 15:25			welcome-survey			
3/16/22 15:26			welcome-survey			
3/16/22 15:26			route	1647458815150		
3/16/22 15:27	Richard Larabee	catboat@protonmail.com	route	1647458836555	I would love to walk / bike to the post office & convenience store from 253	
3/16/22 15:27	Julie McCabe	danjulesp@gmail.com	feature	1647458837710		I live on this road and many cyclists ride this section of the road and it is not safe to ride or walk. Vehicles and commercial trucks constantly fly down this road and there is no shoulder and several blind curves that make is completely unsafe for both the driver and cyclists/walkers. We have tried on numerous fronts to reduce the amount of commercial trucks going to and fro the gravel pits and excavation sites located on Sands Road.
3/16/22 15:27	Richard Larabee	catboat@protonmail.com	welcome-survey			

3/16/22 15:27			feature	1647458854678		PLEASE PLEASE PLEASE connect College Parkway to the B&A Trail!!! There are currently not even consistent sidewalks along this portion of the road making pedestrian access/use completely unsafe.
3/16/22 15:28			route	1647458932053	People walk along this all the time, but it can feel like walking along the side of a highway. Cars zipping by on the hot asphalt	
3/16/22 15:29			welcome-survey			
3/16/22 15:29			welcome-survey			
3/16/22 15:30			welcome-survey			
3/16/22 15:30			point	1647459028975	Intersection of Jumpers Hole and Earleigh Heights, it would be nice if it were more like the intersection of East West and Jumpers Hole.	
3/16/22 15:30	Richard Laribee	catboat@protonmail.com	feature	1647459042185		From Rt 2 to Galesville along 255 would be super helpful. Right now it's much too dangerous both for walking and biking.
3/16/22 15:30		rkbrafford@comcast.net	welcome-survey			
3/16/22 15:30	Allison Turner	ajturner1@aacps.org	welcome-survey			
3/16/22 15:30	Richard Laribee	catboat@protonmail.com	welcome-survey			
3/16/22 15:31	Julie McCabe	danjulesp@gmail.com	welcome-survey			
3/16/22 15:31			welcome-survey			
3/16/22 15:31	Richard Laribee	catboat@protonmail.com	point	1647459121124	It very much needs a street light at this intersection!	
3/16/22 15:32			welcome-survey			
3/16/22 15:32			welcome-survey			
3/16/22 15:33			welcome-survey			
3/16/22 15:33			point	1647459213280	There are no sidewalks along Davidsonville road from this point south. That's where sidewalks need to be built at least on the right side to Bell Branch park so people can walk from Corfton to the park. Now it's impossible to walk to the park from anywhere.	
3/16/22 15:34		rkbrafford@comcast.net	route	1647459282149	Very dangerous riding along 4 lanes of traffic on a very busy stretch of Forest Drive from Arundel on the Bay Rd to Bay Ridge Rd	
3/16/22 15:35	Mark Baldy	markbbaldy14@gmail.com	welcome-survey			
3/16/22 15:35			feature	1647459346882		Please finish this section of trail. It is currently too dangerous to walk/bike on this section of College Pkwy.
3/16/22 15:35	Julie McCabe	danjulesp@gmail.com	route	1647459349583	Many cyclists park at Southern High School and ride the loop from Polling House Rd. to Bayard Rd and down South Polling House Rd back to the high school. There are no shoulders or room for people to walk or cycle. It is a heavily used loop with no safety measures.	

3/16/22 15:35			welcome-survey			
3/16/22 15:36	Julie McCabe	danjulesp@gmail.com	welcome-survey			
3/16/22 15:37	Shawn Squire	shawn@shawnsquire.me	welcome-survey			
3/16/22 15:37	Julie McCabe	danjulesp@gmail.com	welcome-survey			
3/16/22 15:37	Sarah Norton	sarahcnorton@gmail.com	welcome-survey			
3/16/22 15:38	Pam Marvel	pmarvel23@hotmail.com	welcome-survey			
3/16/22 15:39	Shawn Squire	shawn@shawnsquire.me	draw-response	1644946696121		
3/16/22 15:39	Russ Powell	rcpowell02@gmail.com	welcome-survey			
3/16/22 15:40			welcome-survey			
3/16/22 15:40			welcome-survey			
3/16/22 15:40	Richard Reynolds	rr.reynolds@verizon.net	welcome-survey			
3/16/22 15:41			welcome-survey			
3/16/22 15:41			welcome-survey			
3/16/22 15:41			welcome-survey			
3/16/22 15:41	Shawn Squire	shawn@shawnsquire.me	route	1647459710731	In addition to the proposed bike lane, I would also suggest that it includes a sidewalk! We love walking along Lake Waterford and would also like to walk to Ulman Road for that community (and Twain's + Kavern on nice summer nights), but it can be dangerous without any sidewalk or having to walk on a bike path. Better lighting and a place for pedestrains could let people walk along Lake Waterford and reach the businesses and communities down BA Blvd more easily. Thanks!	
3/16/22 15:41			welcome-survey			
3/16/22 15:41			welcome-survey			
3/16/22 15:42	Barbara Miller	cv67co@aol.com	welcome-survey			
3/16/22 15:42	Russ Powell	rcpowell02@gmail.com	draw-response	1643827114807		
3/16/22 15:42			welcome-survey			
3/16/22 15:43	Sara Phillips	usnasaraphillips@gmail.com	welcome-survey			
3/16/22 15:44			feature	1647459855121		There are no sidewalks or safe places to bike/walk along this road. This is a main road that connects several small towns to grocery stores, shops and restaurants and would be valuable to have a safe place to bike and walk when going to these establishments.

3/16/22 15:44			feature	1647459892594		There are no sidewalks or safe places to bike/walk along this road. This is a main road that connects several small towns to grocery stores, shops and restaurants and would be valuable to have a safe place to bike and walk when going to these establishments.
3/16/22 15:46			feature	1647459967412		There are no sidewalks or safe places to bike/walk along this road. This is a main road that connects several small towns to grocery stores, shops and restaurants and would be valuable to have a safe place to bike and walk when going to these establishments.
3/16/22 15:47	Sara Phillips	usnasaraphillips@gmail.com	route	1647460020708	It is very dangerous and would be extremely helpful to have a bike and walking path here.	
3/16/22 15:47			welcome-survey			
3/16/22 15:48	Michael Paszkiewicz	mikepaszkiewicz@gmail.com	welcome-survey			
3/16/22 15:48			welcome-survey			
3/16/22 15:49	Russ Powell	rcpowell02@gmail.com	draw-response	1639501871462		
3/16/22 15:49	D Gruber	dianneflutist@gmail.com	welcome-survey			
3/16/22 15:50	Richard Reynolds	rr.reynolds@verizon.net	point	1647460210481	Sidewalk distorted and dangerous. Need a bike lane !	
3/16/22 15:50	Barbara Miller	cv67co@aol.com	point	1647460231485	We need a sidewalk from the apartments on Halsey road to the intersection of Generals Highway. There is not one now so people walk on the grass and on the road. Not safe! Bicycle lanes would be nice too, lots of people would bike or walk to the mall if we had a side walk and bike lane. Need both. The mall should add fcovered bike racks too.	
3/16/22 15:50			route	1647460250810	There are basically no sidewalks in Woodland beach. This is fine for biking but you have to walk in the street and with cars parked and few stop signs it feels dangerous especially at night. People walking their dogs after dark often wear reflective vests.	
3/16/22 15:51			welcome-survey			
3/16/22 15:52			welcome-survey			
3/16/22 15:52			welcome-survey			
3/16/22 15:52	Elizabeth Marbut	red2251@icloud.com	welcome-survey			
3/16/22 15:53			welcome-survey			
3/16/22 15:54			draw-response	1645468192727		
3/16/22 15:54	D Gruber	dianneflutist@gmail.com	route	1647460453263	Dangerous to walk along Bembe Beach Road due to the fact that the road is narrow and it winds around.	
3/16/22 15:55			welcome-survey			
3/16/22 15:56			welcome-survey			

3/16/22 15:56	Elizabeth Marbut	red2251@icloud.com	route	1647460597276	Very busy area that should include a bike lane	
3/16/22 15:57			route	1647460647763	would like to be able to ride to Quiet Waters Park but current roads are too unsafe for bikes	
3/16/22 15:59	Jacqueline Dunn Kellum	jacquinedunn212@hotmail.com	welcome-survey			
3/16/22 16:01			welcome-survey			
3/16/22 16:03			welcome-survey			
3/16/22 16:03	Laura Towers	lauramtowers@gmail.com	welcome-survey			
3/16/22 16:05			welcome-survey			
3/16/22 16:12			welcome-survey			
3/16/22 16:12			welcome-survey			
3/16/22 16:14	Laura Towers	lauramtowers@gmail.com	route	1647461670186	A bike lane and/or sidewalk extending north all the way up Cape St. Claire Road would allow pedestrians and bikers from the interior portion of the Cape to access the shopping and recreation areas on Cape St. Claire Road without putting themselves in danger. In addition, a sidewalk and/or bike lane would allow residents and visitors south of Little Magothy View to easily access the main beach by foot or bike. Cape St. Claire Road is not currently safe for pedestrians or bikers because there is not space for cars to pass in both lanes with anyone on the side of the road and because it includes several blind spots where motorists might unexpectedly encounter pedestrians or bikers around bends or behind vegetation.	
3/16/22 16:16			welcome-survey			
3/16/22 16:18			welcome-survey			
3/16/22 16:25			welcome-survey			
3/16/22 16:25	Greg Davis	ArkhamAslm@aol.com	welcome-survey			
3/16/22 16:27			welcome-survey			
3/16/22 16:29			welcome-survey			
3/16/22 16:29	Greg Davis	ArkhamAslm@aol.com	point	1647462575178	Daffahlmere (estate)	
3/16/22 16:30			route	1647462604559	Glen Burnie to Fort Meade route to use for commuting.	
3/16/22 16:30			point	1647462628511	Two lane feeder road into several neighborhoods. Road has intermittent sidewalks, little to no shoulders where it would be safe to ride. Would make access to shopping at Quarterfield Shopping Centers safe if bike lanes were added.	
3/16/22 16:31			route	1647462669510	Glen Burnie to Fort Meade route I would like to use for commuting.	
3/16/22 16:32			welcome-survey			
3/16/22 16:33			route	1647462833874	WB&A road from Old Quarterfield to Dorsey Rd. Straight road leads directly to BWI bike trail from various neighborhoods. Road is narrow, without shoulders or sidewalks, traffic is high speed and dangerous with only the road to ride on. Same could be said of Burns Crossing Road to Patuxent Fwy.	
3/16/22 16:42	Joyce Moyer	jrmoyer@yahoo.com	welcome-survey			

3/16/22 16:49	Curtis Oliver	Francur07@aol.com	welcome-survey			
3/16/22 16:50	David L Hebb	dlh21261@gmail.com	welcome-survey			
3/16/22 16:51	robin turk	turkforpeace@gmail.com	welcome-survey			
3/16/22 16:51			welcome-survey			
3/16/22 16:52	curtis fatig	curtisfatig@gmail.com	welcome-survey			
3/16/22 16:55	William Steffens	willsteffens@hotmail.com	welcome-survey			
3/16/22 16:56	robin turk	turkforpeace@gmail.com	draw-response	1646011429065		
3/16/22 16:57	robin turk	turkforpeace@gmail.com	draw-response	1646012677283		
3/16/22 16:58	robin turk	turkforpeace@gmail.com	draw-response	1643158120573		
3/16/22 16:58	curtis fatig	curtisfatig@gmail.com	route	1647464293811	some has no shoulder and very dangerous to walk with a blind curve.	
3/16/22 17:00			welcome-survey			
3/16/22 17:07			welcome-survey			
3/16/22 17:11			point	1647465103662	It's just south of the HS on the east side of the street, where there is no sidewalk in front of 4 homes. Just connecting the sidewalks would go a LONG way to making walking safer.	
3/16/22 17:12			welcome-survey			
3/16/22 17:15			welcome-survey			
3/16/22 17:24			welcome-survey			
3/16/22 17:28	Val Alexander Berard	yankybrit@hotmail.com	welcome-survey			
3/16/22 17:32	Stephen Myers	shmyers@comcast.net	welcome-survey			
3/16/22 17:35	Stephen Myers	shmyers@comcast.net	route	1647466499432	bike path	
3/16/22 17:42	Mike Shay	Skipjack3244@aol.com	welcome-survey			
3/16/22 17:44	Mike Shay	Skipjack3244@aol.com	point	1647467040572	Deale area from Herrington Harbor to Deale Circle needs SIDEWALKS !	
3/16/22 17:44			welcome-survey			
3/16/22 17:45	Constance C Harold	connieharold@hotmail.com	welcome-survey			
3/16/22 17:49			welcome-survey			
3/16/22 17:51	Kathy n Hayes	kcahayes@yahoo.com	welcome-survey			
3/16/22 17:59			welcome-survey			
3/16/22 18:04			welcome-survey			
3/16/22 18:24	Sherri Calvo	sherri.calvo@gmail.com	welcome-survey			
3/16/22 18:53			welcome-survey			
3/16/22 18:58			welcome-survey			
3/16/22 19:16			welcome-survey			
3/16/22 19:35			welcome-survey			
3/16/22 19:51	lynne	lynnekz@aol.com	welcome-survey			
3/16/22 19:51	Michele	mmmosb@yahoo.com	welcome-survey			

3/16/22 19:51			welcome-survey			
3/16/22 19:58	lynne	lynnekz@aol.com	point	1647475130161	Community library. It's not that far from Seven Oaks or Piney Orchard. It would be great if there was a walking/riding path to connect it to those communities that was separated from traffic so kids could walk/ride to the library for programs on their own.	
3/16/22 20:45			welcome-survey			
3/16/22 20:50			feature	1647478215859		A bike lane is definitely needed here, but I would add that it is the intersections, especially turning left, that remain dangerous. Even at current pedestrian crossings, cars turning right on red often do not stop or see pedestrians walking in the crosswalks.
3/16/22 21:05			point	1647479143665	Crossing Bay Ridge Rd. or Georgetown Rd. at this intersection, even with the pedestrian crossing signs, is dangerous. The same is true of the nearby intersection at Edgewood Rd. Cars turning right on red often disregard walkers, bikers, and runners in the crosswalks. It is therefore difficult to walk to Quiet Waters Park, PAL Park, or restaurants from the neighborhoods off Georgetown and Edgewood roads. I would like to see the traffic signals changed to stop all traffic when pedestrians are present.	
3/16/22 21:05			welcome-survey			
3/16/22 21:06			welcome-survey			
3/16/22 21:21	Kathy Hays	modoc73@yahoo.com	welcome-survey			
3/16/22 21:28	Kathy Hays	modoc73@yahoo.com	point	1647480570136	Severals people have been hit by cars at this intersection. Please consider making it a safer intersection.	
3/16/22 21:36	Ann Conley	Arconley39@gmail.com	welcome-survey			
3/16/22 23:43	Dolores Dixon	ddixon1946@aol.com	welcome-survey			
3/17/22 1:20			welcome-survey			
3/17/22 1:35			point	1647495339267	There is a paved walking path along Johns Hopkins Road. The part of the paved path that is between the Johns Hopkins House and Duckett Farm Way is very close to the road, making the path unsafe to walk on because it is too close to cars driving by. A guardrail along this area of the road would make this section of the paved path safer for people walking on the path as well as children who use it for biking in addition to walking.	
3/17/22 2:37			welcome-survey			
3/17/22 4:03			welcome-survey			
3/17/22 7:12			welcome-survey			

3/17/22 7:17			feature	1647515850248		Support adding walkable routes physically separated from traffic, such as a traditional sidewalk, as long as impact on homeowner property is taken into account as well as community input from Chartridge residents.
3/17/22 7:23			welcome-survey			
3/17/22 7:39	Scott Davids	Davids1230@gmail.com	welcome-survey			
3/17/22 7:41			welcome-survey			
3/17/22 7:43	Mark Mulligan	Crew396@aol.com	welcome-survey			
3/17/22 8:37	Yvette Hamill-Graves	yhamill_graves@hotmail.com	welcome-survey			
3/17/22 9:14	Cecilia Richardson	cfnegri@hotmail.com	welcome-survey			
3/17/22 9:18	Cecilia Richardson	cfnegri@hotmail.com	draw-response	1643158120573		
3/17/22 9:20	Cecilia Richardson	cfnegri@hotmail.com	feature	1647523208952		
3/17/22 9:39	AUDIE L ROSE	alynnrose@msn.com	welcome-survey			
3/17/22 9:42	AUDIE L ROSE	alynnrose@msn.com	welcome-survey			
3/17/22 9:49	Mike Paszkiewicz	mikepaszkiewicz@gmail.com	welcome-survey			
3/17/22 9:53	Mike Paszkiewicz	mikepaszkiewicz@gmail.com	feature	1647525231865		Having a safe route from northwest Annapolis/Crownsville to the Greater Annapolis area would be fantastic. I have had incidents with very aggressive drivers on General's Highway and Bestgate Road that have discouraged me from continuing to use my bike to travel around town.
3/17/22 9:58	Mike Paszkiewicz	mikepaszkiewicz@gmail.com	feature	1647525506570		Having a protected trail instead of a bike lane would make travel in this area by bike/running a lot more realistic. General's Highway is too busy a thoroughfare to host cars and bikes/pedestrians.
3/17/22 10:19			welcome-survey			

3/17/22 10:21			feature	1647526894968		This route is so important. The bikelane on RT 178 is inadequate... It stops at the busy and unsafe intersections. I feel that having a safe off road bike/walking path is imperative. Many people are walking along that section of road -- there is no public transportation and people work at the senior center or live in rolling knolls/rolling hills who walk along the unsafe highway.
3/17/22 10:36			welcome-survey			
3/17/22 10:44			welcome-survey			
3/17/22 10:51	James Rumizen		welcome-survey			
3/17/22 10:54			welcome-survey			
3/17/22 11:34			welcome-survey			
3/17/22 11:39			feature	1647531549538		Maintain the existing sidewalks and roads. The sidewalks and medians are overgrown due to lack of regular maintenance creating unpassable areas and dangerous tripping hazards. The road is also poorly maintained. Speeds along the road create safety issues for pedestrians.
3/17/22 12:17	CURTIS ALLEN CHITTY	CPA.MARYLAND@OUTLOOK.COM	welcome-survey			
3/17/22 13:50			welcome-survey			
3/17/22 14:14	DAVID J DEMERS	david.demers@verizon.net	welcome-survey			
3/17/22 14:16	DAVID J DEMERS	david.demers@verizon.net	point	1647541006321	please build/design a safe way to cross Rt 3 so that the South Shore Trail can continue to Odenton and beyond.	
3/17/22 14:28			welcome-survey			
3/17/22 15:07			welcome-survey			
3/17/22 15:30			welcome-survey			
3/17/22 15:43	Madeline Dummert h	mwoods1121@gmail.com	welcome-survey			
3/17/22 15:47	Madeline Dummert h	mwoods1121@gmail.com	point	1647546436060	Would love to see a bike lane on this road. The sidewalks are well kept in this section so I'm comfortable running there but biking feels a lot less safe because of drivers flying down Disney.	
3/17/22 17:02			welcome-survey			
3/17/22 17:08			welcome-survey			

3/17/22 17:20			welcome-survey			
3/17/22 17:23			route	1647552193752	It's a nice quiet piece of road but has no sidewalks or space to walk or ride	
3/17/22 18:11	Reginald Madden	madden7604@comcast.net	welcome-survey			
3/17/22 18:13			welcome-survey			
3/17/22 18:18			welcome-survey			
3/17/22 18:24			draw-response	1639681662279		
3/17/22 19:36			welcome-survey			
3/17/22 19:39			route	1647560373663		
3/17/22 19:42			route	1647560531923	This route, West Benfield, gets heavy bike traffic on weekends, but is very dangerous and needs to have a bike lane.	
3/17/22 19:43			route	1647560637051	Would provide access to SPHS.	
3/17/22 21:34			welcome-survey			
3/17/22 21:35			welcome-survey			
3/17/22 22:43	Tihamer T Toth-Fejel	tihamertothfejel@gmail.com	welcome-survey			
3/17/22 22:47	Tihamer T Toth-Fejel	tihamertothfejel@gmail.com	feature	1647571672875		It would be more fun if this trail was along the Magothy River.
3/17/22 22:51	Tihamer T Toth-Fejel	tihamertothfejel@gmail.com	feature	1647571910867		I guess I'm spoiled rotten by the BW trail -- no cars at all! It would be nice if you could at least put some trees between the trail and the cars. We love our jeeps, but not when we're biking! Anyway, the route is great because it ties the BW trail to the Bay!
3/17/22 22:57	Tihamer T Toth-Fejel	tihamertothfejel@gmail.com	feature	1647572259936		Consider this support very weak. It's a great idea to tie Pasadena to Magothy Bridge Rd, but I *hate* bike paths. Do you really think that people behind the wheel aren't distracted by their phones?
3/17/22 23:04	Tihamer T Toth-Fejel	tihamertothfejel@gmail.com	feature	1647572641806		On one hand, being able to ride bikes to Down's Park from the BW trail sounds *GREAT*. However, I hate bike lanes. They are just too dangerous, especially for a fast and busy road like Mountain Road. I don't know how to fix the problem. :(
3/17/22 23:06	Chris	Crochet1962@gmail.com	welcome-survey			
3/17/22 23:09	Chris	Crochet1962@gmail.com	point	1647572988652		
3/18/22 4:30			welcome-survey			
3/18/22 7:16			welcome-survey			
3/18/22 7:37			welcome-survey			

3/18/22 9:04	Laurie Rollins Anderson	ira.laurie@gmail.com	welcome-survey			
3/18/22 9:04			welcome-survey			
3/18/22 9:24			welcome-survey			
3/18/22 10:16			welcome-survey			
3/18/22 11:00			welcome-survey			
3/18/22 12:07			welcome-survey			
3/18/22 12:35	Bill Thomas	DarkShadesBill@yahoo.com	welcome-survey			
3/18/22 12:40	Carol Cox Forbes	cac.grace@gmail.com	welcome-survey			
3/18/22 13:01			welcome-survey			
3/18/22 13:03			route	1647623029199	No crosswalks to local businesses.	
3/18/22 13:17			welcome-survey			
3/18/22 13:17			welcome-survey			
3/18/22 13:18			welcome-survey			
3/18/22 13:21			point	1647624104723	There is no crosswalk to local businesses in this area of the BWI Trail. There need to be more crosswalks to businesses around the county, as well as safer barrier bike lanes on local roads.	
3/18/22 13:33	Teri	Teristephens@msn.com	welcome-survey			
3/18/22 14:12	Jen king	Jen_o_king@yahoo.com	welcome-survey			
3/18/22 15:38			welcome-survey			
3/18/22 16:12			welcome-survey			
3/18/22 16:30			feature	1647635405187		Too much traffic and congestion on this road now. Sidewalks are available to walk or ride bike if necessary. I don't understand why bike routes are even being considered in such heavily traveled areas by commuters! Very dangerous.
3/18/22 19:25	Greg Cantori	gcantori@gmail.com	point	1647645909434	Public space. The entire city can be reengineered to along the lines of the hierarchy of vulnerability - prioritising wheelchairs, walkers, canes and children over all others then cyclists and scooters and finally cars and trucks and buses last.	
3/18/22 20:07			welcome-survey			
3/18/22 20:09			feature	1647648542760		Please please! Once got run over by a truck walking on gen hwy. Would like to be safer. Would also ride more often
3/18/22 21:01			welcome-survey			
3/18/22 21:03			welcome-survey			

3/18/22 21:09			route	1647652161397	Pedestrian use of Taylor St, to & from USNA Stadium and major transit pathways/roads such as Rowe Blvd and West St, is very dangerous. No sidewalks on major portions of this road, so especially dangerous in fall or whenever there are events after dark. Bad mix after stadium events of a glut of impatient drivers unfamiliar with local roads and pedestrians who don't have sidewalks. Improve sidewalks on Taylor!	
3/19/22 8:05	Carolina Zapata	c.andrea.zapata7@gmail.com	welcome-survey			
3/19/22 13:18			route	1647710286357	There needs to be a trail here so students can safely walk or bike to the high school and middle school. It would also reduce the need for buses in the area.	
3/19/22 13:40			feature	1647711599987		I support a separate bike trail along this route with a barrier to protect trail users from cars. I would prefer if the trail extend all the way to Waugh Chapel Road. I think this would be a great addition to the walking/biking network if done right (and would provide some great connections to the South Shore Trail), but an unprotected bike lane next to car traffic on Route 3 is just too dangerous for most people.
3/19/22 13:55			feature	1647712525283		All of South Shore Trail is very important, and serves as a major component of the overall biking/walking network. This section helps connect Annapolis to Crownsville, Millersville, Gambrills, and Odenton (a major transit hub). Please make this route a priority in the budget!
3/19/22 13:58			feature	1647712737938		South Shore Trail is a very important part of the overall biking/walking network. This section will connect a large residential area with businesses and shops in Parole and Annapolis, including the Annapolis Mall and Annapolis Town Center. Many of us live within easy walking or biking distance of Parole but can't get there because there is currently no safe route. This trail will be a big improvement to the area.

3/19/22 17:28			welcome-survey			
3/19/22 17:35			route	1647725733511	I do not attend high school. I see lot of HS students walking and biking under unsafe conditions due to lack of separate trails and adequate sidewalks through the community.	
3/19/22 18:00			welcome-survey			
3/19/22 18:24			welcome-survey			
3/19/22 18:27			point	1647728852931		
3/19/22 22:13	Jill	babyeley@bellsouth.net	welcome-survey			
3/19/22 22:22	Jill	babyeley@bellsouth.net	point	1647742938134	My child no longer attends St. Anne's, but it would have been nice if we could have ridden bikes to her school. Unfortunately, Arundel on the Bay has numerous segments without sidewalks and a few sharp curves that make it risky to walk and to bike along. Would definitely be interested in riding out beyond my neighborhood if Arundel on the Bay was more bike-friendly. I know there are seasoned bikers who trek this way, but its not safe for those who are less experienced or kids. Suspect there are LOTS of kids who could walk/ride to Hillsmere or St. Anne's if Arundel on the Bay had more sidewalks or bike lanes.	
3/20/22 8:54			welcome-survey			
3/20/22 8:55			welcome-survey			
3/20/22 11:06	Martin Egna	mregna@gmail.com	welcome-survey			
3/20/22 11:12	Martin Egna	mregna@gmail.com	draw-response	1643552534965		
3/20/22 14:40			welcome-survey			
3/20/22 14:45			point	1647801937242	This area has limited parking and the road to get here is heavily trafficked with no sidewalks, bike paths, or shoulder (vegetation has overtaken narrow shoulders).	
3/20/22 18:36			welcome-survey			
3/21/22 7:04			welcome-survey			
3/21/22 10:59			welcome-survey			
3/21/22 11:07			welcome-survey			
3/21/22 11:22			welcome-survey			
3/21/22 12:12	Jack Stewart Floyd	Jfloyd18@verizon.net	welcome-survey			
3/21/22 12:16	Jack Stewart Floyd	Jfloyd18@verizon.net	feature	1647879386897		Would be great to be able to ride to the State Park. I would suggest discount entrance fee for riders or hikers.
3/21/22 12:18	Jack Stewart Floyd	Jfloyd18@verizon.net	feature	1647879511867		It would be great to connect to Baltimore Annapolis trail without having to get into a car to drive there to get on trail. Gives lots of neighborhoods access to B&A trail.
3/21/22 14:53	Amy Doyle	amy.doyle@wellsfargo.com	welcome-survey			

3/21/22 14:57	Amy Doyle	amy.doyle@wellsfargo.com	draw-response	1644946696121		
3/21/22 14:57	Amy Doyle	amy.doyle@wellsfargo.com	welcome-survey			
3/21/22 14:58	Amy Doyle	amy.doyle@wellsfargo.com	welcome-survey			
3/21/22 15:03	Amy Doyle	amy.doyle@wellsfargo.com	feature	1647889396376		Yes, I support this. Great way to provide accessibility to both Lake Waterford Park and Kinder Park. Please make this improvement in concert with proposed bike lane 280 (from Solley Road to Pasadena Road).
3/21/22 15:07	Amy Doyle	amy.doyle@wellsfargo.com	feature	1647889638447		Very much needed. Putting a bike lane along 648 (connecting Solley Road to Pasadena Road) would create more accessibility to Lake Waterford Park, and by connecting with the sidewalks on Pasadena Road, more accessibility to the B&A bike trail is created as well. Would be a great improvement and a great way for county citizens to take more advantage of existing parks and trails.
3/21/22 15:09	Amy Doyle	amy.doyle@wellsfargo.com	welcome-survey			
3/21/22 15:11	Amy Doyle	amy.doyle@wellsfargo.com	feature	1647889863583		Much needed trail for this area to have better and safer access to the B&A bike trail.
3/21/22 15:16	Amy Doyle	amy.doyle@wellsfargo.com	feature	1647890202214		Any trail that can provide greater biking and walking access to Lake Waterford, the B&A trail, and Kinder Park for those who live west of Route 2 in the Pasadena area would be a very nice improvement to those county residents.
3/21/22 20:22	Mark J Leszcz	mjosefl@verizon.net	welcome-survey			
3/22/22 0:55	Ruth Potts	rlpent@yahoo.com	welcome-survey			
3/22/22 10:23			welcome-survey			
3/22/22 11:09			welcome-survey			

3/22/22 11:17			route	1647962239652	The nearest public parking lot to the Northern end of Waterworks trail head is at Generals Hwy Corridor Park. However, crossing the Crownsville Rd street is dangerous (cars drive fast!) and Honeysuckle Ln is also dangerous (narrow street with blind curves), not safe to walk or ride a bike. A lighted bike/pedestrian crosswalk is needed and a path to get between Generals Hwy Corridor Park and Waterworks trail (Northern end)	
3/22/22 11:20			route	1647962400122	A bike/walking path is needed to connect the bike path on Generals Hwy and Generals Hwy Corridor Park via Honeysuckle Ln. Honeysuckle Ln is a narrow, curvy road with blind curves that cars typically drive faster than the posted speed limit - making it dangerous to walk or ride a bike on.	
3/22/22 14:25			welcome-survey			
3/22/22 15:57			welcome-survey			
3/22/22 21:38			welcome-survey			
3/23/22 14:30			welcome-survey			

Appendix J: In-person Meeting Comments

April and May 2022 In-person Meeting Comments

Comments made on the "15 Corridors" Board
concerns about crime (Linthicum)
Nursery Road - worried about safety (women) due to panhandlers
(placed around 1&8) connect the dots
Jumpers Hole Road - bike/ped facilities study is a high demand area
Mountain Road - parking area provide space that could be used for bikes
show connections beyond map to Columbia, Laurel, Capital Trails, Baltimore Greenway etc
Muddy Creek Road (connecting #12 and #15) is recreational
short trip opportunities for Deale, Shady Side

Comments made on the Glen Burnie Town Center board

Parking exit cross walk on Crain Highway and in Town Center
MD 170/175 - SHA might be proposing something - developer can't do anything

Comments made on the Level of Traffic Stress map board

RT 3 thorough Crofton to Odenton TC is unsafe for cars let alone bike/ped w/ divided Highway
high stress left turn from route 2 N-bound shoulder to Harwood Road
route 2 408&Bayard Circle - route 2 shoulders disappear shortly before circle, very high stress
camp meade road bike lanes still feel too dangerous
recreation use - b&a trail (at least 1x/week), WBA Trail (1x/month), BWI Trail (2-3x/mo)

Roads Used - Dicks Mill Road, Hog Farm Road, Gambrills Road, Generals Highway, Davidsonville Road, Crownsville Road, Route 450, Bestgate Drive, Bay Ridge Road, Farragut Road, Mayo Road
Bike to Work: West Benfield to Annap Junction or West Benfield to Nursery Road - Unsafe Areas: Intersection 170 and 174, benfield and Gov. Stone, Route 175 and I-295 - Sidewalk/Road in Poor Condition - Veterans Highway from East/west Highway to Brightview
unsafe areas: Elkridge Landing Road (Heavy traffic, road in poor condition, parallel to road)
sidewalk/road poor condition: andover road (athletic facility to nursery road) and elkridge landing road
high demand for biking to odenton (placed in the northwest portion of county)

**Appendix K: Methodologies of Level of Traffic Stress
Analysis: A Comparison Between that of Toole
Design Group and the Maryland Department of Transportation**

Toole Design Group Level of Traffic Stress Analysis Methodology:

Maryland Department of Transportation (MDOT) Level of Traffic Stress (LTS) Analysis Methodology:

According to material available online, MDOT's LTS methodology¹ is as follows:

MDOT's LTS methodology is based on the metrics established by the Mineta Transportation Institute (MTI) Report 11-192 'Low-Stress Bicycling and Network Connectivity' (May 2012 additional criteria³ refined by Dr. Peter G. Furth (June 2017) below and Montgomery County's Revised Level of Traffic Stress.⁴

Mixed Traffic Criteria

Number of Lanes	Effective ADT	Prevailing Speed						
		<20mph	25mph	30mph	35mph	40mph	45mph	50+mph
Unlaned 2-way street (No centerline)	0-750	1	1	2	2	3	3	3
	751-1500	1	1	2	3	3	3	4
	1501-3000	2	2	2	3	4	4	4
	3000+	2	3	3	3	4	4	4
1 thru lane per direction (1-way, 1 lane street or 2-way street with centerline)	0-750	1	1	2	2	3	3	3
	751-1500	2	2	2	3	3	3	4
	1501-3000	2	3	3	3	4	4	4
	3000+	3	3	3	4	4	4	4
2 thru lanes per direction	0-8000	3	3	3	3	4	4	4
	8001+	3	3	4	4	4	4	4
3+ thru lanes per direction	Any	3	3	4	4	4	4	4

Effective ADT = ADT for two-way roads. Effective ADT – 1.5' ADT for one-way roads

Bike Lanes and Shoulders Not Adjacent to Parking

Number of Lanes	Bike Lane Width	25mph	30mph	35mph	40mph	45mph	50+mph
1-thru lane per direction or unlaned	6'+	1	2	2	3	3	3
	4'-5'	2	2	2	3	3	4
2 thru lanes per direction	6'+	2	2	2	3	3	3
	4'-5'	2	2	2	3	3	4
3+ thru lanes per direction	Any	3	3	3	4	4	4

Notes:

1. If bike lane/shoulder is frequently blocked, use mixed traffic criteria.
2. Qualifying bike lane/shoulder should extend at least 4' from a curb and at least 3.5' from a pavement edge or discontinuous gutter pan seam.
3. Bike lane width includes any marked buffer next to the bike lane.

1 https://www.mdot.maryland.gov/OPCP/MDOT_LTS_Metadata_Methodology_Full.pdf
 2 <https://transweb.sjsu.edu/sites/default/files/1005-low-stress-bicycling-network-connectivity.pdf>
 3 <https://cpb-us-w2.wpmucdn.com/sites.northeastern.edu/dist/e/618/files/2014/05/LTS-Tables-v2-June-1.pdf>
 4 <https://montgomeryplanning.org/wp-content/uploads/2017/11/Appendix-D.pdf>

Bike Lanes Adjacent to Parking

Number of Lanes	Bike Lane Reach = Bike + Parking Lane Width	25mph	30mph	35mph
1 lane per direction	15'+	1	2	3
	12'-14'	2	2	3
2 lanes per direction (2-way) 2-3 lanes per direction (1-way)	15'+	2	3	3
Other multilanes		3	3	3

Notes:

1. If bike lane/shoulder is frequently blocked, use mixed traffic criteria.
2. Qualifying bike lane reach must be at least 12'
3. Bike lane width includes any marked buffer next to the bike lane.

MDOT's LTS rating includes two additional scores: "0" to represent shared-use paths and similar facilities that are suitable for people of all ages and abilities to bicycle and "5" to represent roadways where bicycle access is prohibited. The below chart describes the LTS score in terms of facility types and the corresponding target bicycling audience that might be expected to use them.

LTS	Target Audience	Bicycle Facility Types
0	All ages and abilities	Rail-trails, shared-use paths
1	Almost everyone	Protected bikeways, sidepaths
2	Interested but concerned	Bike lanes, bike boulevards
3	Enthusied and confident	Bike lanes, shared lanes, shoulders
4	Strong and fearless	No bike facility or bike lane on a major roadway
5	Bicycle Access Prohibited	Bicycle access is prohibited by managing roadway agency

More detail is provided in MDOT's LTS memo.⁵

After MDOT ran the initial analysis described above and in the memo, the agency conducted a "network analysis" to calculate an LTS score that incorporates the impact bicycle facility design features have on LTS, namely the side of roadway with the bike lane, traffic flow, and vertical separation. Specifically, MDOT's memo states:

Bicycle Infrastructure Specifics

In addition to identifying protected bike lanes and standard bike lanes, additional metrics were collected with the initial analysis. These metrics were identified to enable network analysis once the initial analysis was performed. These metrics include:

- Side of roadway – Not all bike lanes are on both sides of the roadway, therefore the bike lane was assigned whether it is on the left, right or both sides of the street based on the cardinality of the route. This metric is applied to better calculate LTS in addition to bike lane miles per side of the roadway.
- Traffic flow – Each on-street bike facility was assigned whether it allows for one or two-way travel. This metric is applied to allow for contraflow bicycle lanes and bi-directional cycletracks in addition to allowing future network analysis and routing.
- Vertical separation – The presence of a vertical separator provides a physical barrier between bicycle and motor vehicle traffic. This separation reduces the likelihood and severity of crashes between motor vehicles and bikes, thus creating a more inviting bicycle experience. While Montgomery County's methodology distinguishes between

5 https://www.mdot.maryland.gov/OPCP/MDOT_LTS_Metadata_Methodology_Full.pdf

the types of barriers such as flex posts and parked cars, MDOT’s methodology only identifies if there is a vertical delineator within the buffer.

Several factors which help further define LTS were unavailable during the initial analysis. These factors include bike lane width, width of buffers between bike lanes and parking or travel lanes, bike lane reach and the presence of on-street parking. Due to the lack of data on existing bicycle facilities, all bike lanes are assumed to be at least 5’ wide and not adjacent to on-street parking. Shoulder width data is available on most roadway records which helped produce more accurate LTS results. For roadways with a bikeable shoulder, but not marked as a bike lane, the shoulder width was used to calculate the LTS. As MDOT continues to expand data collection for OMOC, these additional factors will be included to refine LTS scores. See the following charts for the bike facility LTS recommendations.

Standard Bike Lanes

Number of Lanes	Vertical Separation	≤25mph	30mph	35mph	40mph	45mph	50+mph
1-thru lane per direction or unlaned	No	1	2	3	3	4	4
2 thru lanes per direction	No	2	3	3	3	4	4
3+ thru lanes per direction	No	3	3	4	4	4	4

Protected Bike Lanes

Number of Lanes	Vertical Separation	≤25mph	30mph	35mph	40mph	45mph	50+mph
1	Yes	1	1	1	1	2	3
2	Yes	1	1	1	2	3	3
3+	Yes	1	1	2	2	3	4

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Roadways with Bikeable Shoulders

Number of Lanes	Shoulder Width	25mph	30mph	35mph	40mph	45mph	50+mph
1-thru lane per direction or unlaned	6'+	1	2	2	3	3	3
	4'-5'	2	2	2	3	3	4
2 thru lanes per direction	6'+	2	2	2	3	3	3
	4'-5'	2	2	2	3	3	4
3+ thru lanes per direction	Any	3	3	3	4	4	4

More detail is provided in MDOT’s LTS memo.6

6 https://www.mdot.maryland.gov/OPCP/MDOT_LTS_Metadata_Methodology_Full.pdf